

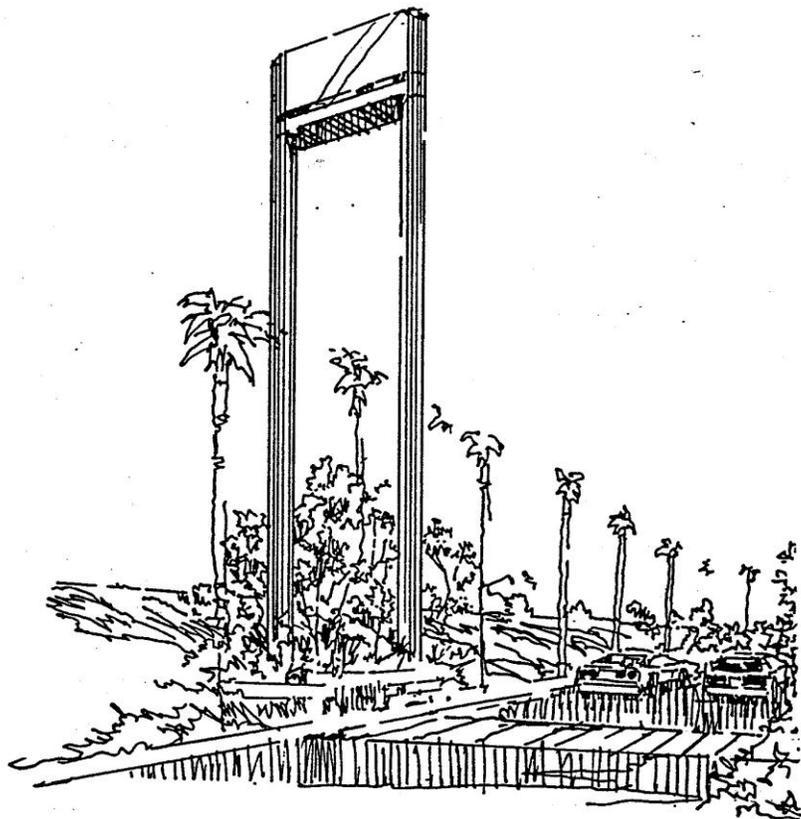
**Latest Revision:  
September 7, 2011**

**Corona Westgate  
Specific Plan  
(SP-87-1)**

**Adopted by  
City of Corona  
City Council Resolution 87-36  
on May 20, 1987**

**Prepared for  
Corona Redevelopment Agency  
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Corona, CA 91720**

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CORONA REDEVELOPMENT AGENCY

GRC

**CORONA WESTGATE SPECIFIC PLAN  
(SP-87-1) AMENDMENTS**

Application No.	Applicant/Amendment	Resolution No.	Date
SPA 89-19	Amendment to Section 402 (A) 4 expanding the permitted office and professional service uses in the ICDD. (Applicant Santa Fe Pacific)	89-50	12/6/89
SPA 89-22	Amendment to Section 403 (F) 1 to permit up to 200 square feet of enterprise signage above the first floor in the ICDD. (Applicant: Santa Fe Pacific)	90-32	2/21/90
SPA00-010	Amendment to Section 304 (C.4.b) to remove the 24" letter height provision for building mounted signs in the ACDD (Applicant: City Of Corona)	Ordinance No. 2779	9/6/00
SPA09-002	Amendment to allow automobile and recreational vehicle sales and similar uses within the Auto Center Development District (ACDD) (Applicant: City of Corona).	Ordinance No. 3013	9/16/2009
SPA11-003	Amendment to allow monument signs to be constructed up to six feet high in the front yard setback.	Ordinance No. 3101	9/07/2011



## TABLE OF CONTENTS

<b>Section</b>	<b>Page</b>
<b>CHAPTER 1: INTRODUCTION</b>	
100 Background	1
101 Purpose	2
102 Authority	2
103 Relationship to the General Plan	2
104 Specific Plan Goals	3
105 California Environmental Quality Compliance	3
106 Public Participation	4
107 Location and Setting	4
<b>CHAPTER 2: LAND USE PLAN</b>	
200 Introduction	7
201 Land Use Concept	7
202 Public Improvement Concept	11
<b>CHAPTER 3: AUTO CENTER DEVELOPMENT DISTRICT</b>	
300 Description of Boundaries	16
301 Purpose and Intent	16
302 Permitted and Conditional Uses	18
303 Minimum Site Development Standards	18
304 Signage	23
305 Other Governmental Codes	28
306 Design Review Requirements	28
307 Design Guidelines	28
<b>CHAPTER 4: INDUSTRIAL/COMMERCIAL DEVELOPMENT DISTRICT</b>	
400 Description of Boundaries	35
401 Purpose and Intent	35
402 Permitted and Conditional Uses	37
403 Minimum Site Development Standards	39
404 General Provisions	45
405 Other Governmental Codes	48
406 Design Review Requirements	48
407 Design Guidelines	48

## TABLE OF CONTENTS (CONTINUED)

Section	Page
<b>CHAPTER 5: DESIGN REVIEW PROCESS</b>	
500 Purpose	54
501 Development Project Reviews	54
502 Administration	55
503 Authority	57
<b>CHAPTER 6: ENFORCEMENT</b>	
600 Enforcement	59
601 Relationship to the Zoning Code	59
602 Interpretation	59
603 Compliance with Governmental Codes	59
604 Definitions	60

### ADDENDUM

1. Street, Driveway, Intersection and Parkway Planting Diagrams and Cross Sections	61
Wardlow Road, Pomona-Rincon Road, Research Drive	62
Bonnie Place, Bonnie Court	63
Entry Drive Trees	64
Intersection Trees	65
Serfas Club Drive (Auto Center Drive)	66

### LIST OF FIGURES

Figure	Page
1. Regional Location	5
2. Specific Plan Area & General Plan Designations	6
3. Existing Zoning	8
4. Specific Plan Land Use Map	9
5A. Development Concept Plan	13
5B. Serfas Club Drive Landscape Concept	14
6. Tentative Parcel Map No. 22687	15
7. Auto Center Development District	17
8. Industrial/Commercial Development District	36

## CHAPTER 1: INTRODUCTION

### Section 100-Background

The Corona Westgate Specific Plan is the result of the City of Corona's desire to create a commercial and industrial center near its western entrance. It is intended that the plan will permit the development of an auto-retail center at Serfas Club Drive, ringed by industrial/commercial uses that will be compatible with the auto center and provide a transition to the more standard industrial development that is planned for the area. Because the plan anticipates a unique mix of commercial and industrial land use, it also provides development and architectural review standards that are designed to meet the needs of the Specific Plan area.

The concept for an auto mall in the City of Corona has arisen out of the City's determination to retain local auto dealerships and the sales tax revenues generated by them, to attract new businesses that are not currently in Corona. The auto dealerships are an important component of the C economy and modern auto-retailing techniques dictate that franchises be consolidated in proximity to freeways.

The concept of a flexible commercial/industrial area is spawned by the prominent, location of the plan area adjacent to the 91 Freeway and the trend toward accommodating mixed commercial and industrial uses in locations with superior visibility and access. It is the intent of the Plan to encourage a mixture of land uses that will be able to take full advantage of the market potential and to encourage quality development to enhance the image of the Specific Plan area located at a major entryway to the City.

Auto dealerships and industrial/commercial development are consistent with the existing General Plan designations; however, the Specific Plan further restricts the range of uses to auto-related in the area designated for auto center development, and broadens the range of uses in the remaining Plan area. Therefore, the Specific Plan is designed to supplement existing goals and policies rather than replace them in their entirety.

Beyond refining the land use pattern, the Specific Plan incorporates standards and guidelines for controlling building design, landscaping, parking and signage. The Specific Plan establishes unique standards for the Westgate area; designed to be flexible, but requiring project proponents to be sensitive to surrounding projects and the general environment. This is accomplished within the Plan by establishing mandatory minimum development standards and broad development guidelines. The guidelines consist of design statements intended to give direction to designers and permit both the City of Corona and designers the opportunity to influence the quality of a proposal, while incorporating the individual needs of a specific project.

This flexibility is implemented through a design review process that permits the review of projects in the context of surrounding development to ensure compatibility between projects and between a project and its setting (i.e. freeway frontage). In this manner the Westgate area can serve as a focal point for the community and incorporate a mixture of land uses in a compatible fashion.

This plan is part of a two-fold effort to accomplish Corona's goals. The first part of the effort was accomplished in May 1986, when Amendment II to Project Area A' was adopted by City Council Ordinance No. 1801. The Amendment 11 boundaries include two possible auto center sites, one on West Sixth Street, which has since been abandoned in favor of the site at Serfas Club Drive. The purpose of the Redevelopment Plan is to provide financing for the auto center and to use the sales tax revenues from the center to fund rehabilitation efforts in the remainder of the Amendment area. - -

### **Section 101-Purpose**

The purpose of this Specific Plan is to promote the orderly development of a high-quality, economically successful auto center, encourages quality industrial/commercial development in the Specific Plan area, and ensure that the developments co-exist in a compatible and harmonious fashion. The Specific Plan is designed to implement the policies contained within the City's General Plan, the Redevelopment Plan and other applicable City planning documents.

### **Section 102-Authority**

The California Government Code authorizes cities to adopt specific plans either by resolution as policy, or by ordinance as regulation. This Specific Plan is regulatory in nature and serves as zoning law for the properties involved. Development plans, site plans and tentative tract and parcel maps in this area must be consistent with this Specific Plan. If a development agreement is sought, it must also be consistent with the intent of the General Plan and this Specific Plan. The scope of subjects covered in this Plan augments the General Plan to the extent that the subject under consideration involves the Corona Westgate Specific Plan portion of the community.

The Corona Westgate Specific Plan is established through the authority granted to the City of Corona, by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, and Sections 65450 through 65457. The Plan meets the requirements of the Government Code by providing land use and development standards for public and private facilities and containing provisions for the implementation of the Plan goals and objectives.

### **Section 103-Relationship to the General Plan**

Development within the City of Corona is governed by the General Plan. In conformance with this General Plan, the Corona Westgate Specific Plan is the document that will guide the development of the Westgate area. The major General Plan goals addressed by the Specific Plan include:

1. Land use planning consistent with infrastructure capacities and environmental resources.
2. An efficient circulation system designed to mitigate the impacts of mixed industrial/commercial development.
3. Attractive commercial environments for all economic segments of the community.

4. Design features that reinforce the identity of the City and enhance its appearance.
5. Increased employment opportunities.

#### **Section 104-Specific Plan Goals**

The Specific Plan and subsequent regulations implementing the Land Use Plan for the auto center and industrial/commercial development districts stem from and ensure the attainment of the following general goals:

- A. Provide a resource for maintaining certain existing Corona auto dealers within the City in an environment conducive to achieving maximum retail sales.
- B. Encourage the highest and best use of available industrial/commercial land consistent with good planning practices.
- C. Provide use flexibility to encourage the rapid development of available industrial/commercial land to promote new employment opportunities for Corona residents.
- D. Encourage the local and regional population to purchase vehicles within the City of Corona.
- E. Provide a framework for the engineering and construction of public infrastructure improvements designed to enhance the general environment and provide appropriate service levels for the citizens of Corona.
- F. Encourage an active partnership between property owners, businesses, the City and the Redevelopment Agency to accomplish the objectives of the Specific Plan.
- G. Encourage quality design standards within a consistent framework.
- H. Encourage the growth of the Corona tax base to minimize the need for future fees and taxes while maintaining a high quality of public services to the residents and businesses of the City.

#### **Section 105-California Environmental Quality Compliance**

In compliance with the requirements of the California Environmental Quality Act and the City of Corona environmental procedures, and in conjunction with the preparation of the draft Specific Plan, A Negative Declaration has been prepared for the Specific Plan, and circulated to the appropriate parties. The Negative Declaration addresses the potential environmental impacts which might result from the implementation of the proposed Plan, and augments the Environmental Impact Report prepared and certified as part of the Redevelopment Project Area adoption process.

## **Section 106-Public Participation**

The Corona Westgate Specific Plan has been developed through the cooperation of the Corona business community and the major property owners within the Plan boundaries. Adoption of the Specific Plan will entail a public hearing process to allow the general public residing within the City of Corona and others having an interest in the future of the Specific Plan area the opportunity to participate in the Specific Plan development process.

## **Section 107-Location and Setting**

The Corona Westgate Specific Plan area is located within the City of Corona, in western Riverside County (see Figure 1).

Corona is approximately 50 miles east of downtown Los Angeles, and 20 miles northeast of central Orange County. It adjoins the City of Riverside to the east, with unincorporated County territory to its west.

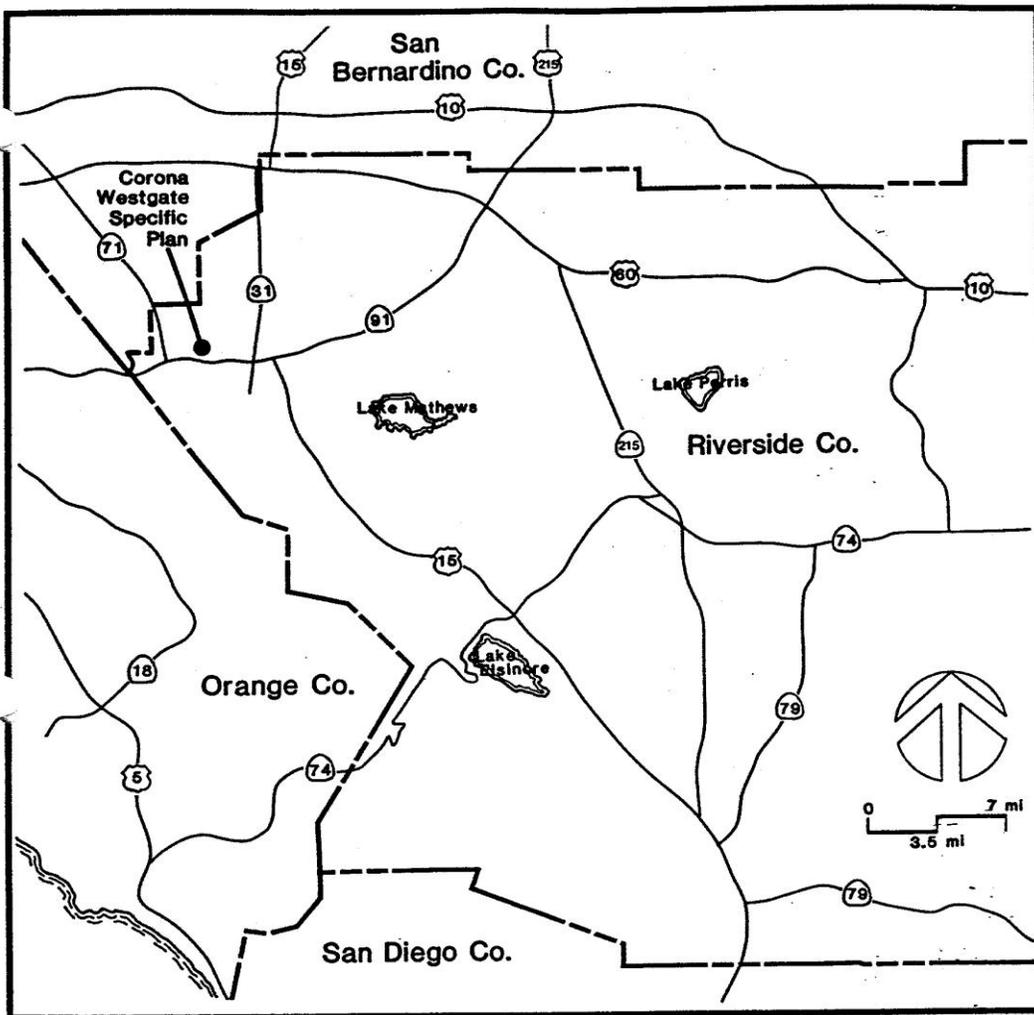
The project is located on Corona's west side, north of and adjacent to the 91- Freeway, and south of the A.T. & S.F. railroad right-of-way. Access to the 91 Freeway is available at Serfas Club Drive and Maple Street.

The City of Corona and surrounding areas in Riverside and Orange counties have been growing at a rapid pace for the past decade. Commercial and industrial development is increasing in proportion to the population escalation occurring in the Southern California region. The City of Corona is experiencing its own internal growth as well as development pressures from adjacent and nearby communities. This growth has contributed to the City's rapid transition from an agricultural to a suburban community. The transition will continue to be the major factor influencing the future direction of the community. The City of Corona must contend with the economic growth trends of the region as well as strive to retain its unique identity and attain its own established goals.

The Westgate Specific Plan area is situated in a location once considered the undeveloped western periphery of the City, but which is now becoming a highly desirable area due to the recent upswing in development in Corona, and Riverside County as a whole. In addition, other changes occurring include the annexation of unincorporated County land along the southwestern City boundary, the development of the Sierra Del Oro 3,200-unit single-family residential community, and the construction of the 1-15 interchange at the 91 Freeway.

The Specific Plan area is ideal for uses requiring high freeway visibility and convenient access. Higher intensity uses than those allowed by the current industrial zoning districts are required in order to take maximum advantage of the area's location and favorable development trends. At the same time, the visual impact of the area from the Freeway will make it an important focal point in the City, and will influence the establishment of a City "image".

The Specific Plan boundaries and the General Plan designations are illustrated on Figure 2. The plan boundaries include the property located within Subdivision Tract Maps Nos. 20137 and 19150-1 and Parcel Map 19420.



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# CORONA WESTGATE SPECIFIC PLAN

FIGURE 1  
Regional Location



## **CHAPTER 2: LAND USE PLAN**

### **Section 200-Introduction**

The Corona Westgate Specific Plan is designed to: a) facilitate the development of an integrated auto sales center; and b) promote the harmonious and orderly development of an industrial/commercial area. As the “policy” section of the Plan, the major function of the Land Use Plan is to establish the overall goals and to establish the land use pattern which will serve as the foundation for development standards and regulations set forth in subsequent chapters.

The Land Use Plan divides the property lying within the Specific Plan boundaries into two development districts. The first, designed to accommodate the auto center, shall henceforth be known as the Auto Center Development District or “ACDD”, and the second district, permitting a mix of industrial and commercial uses, shall be referred to as the Industrial/Commercial Development District or “ICDD”.

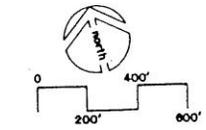
The land use pattern is the result, in part, of performing design studies related to the auto center use, negotiating with a large number of auto dealers and attempting to coordinate the City’s efforts with the two large existing property owners within the Plan’s boundaries. Additionally, the Plan integrates the auto center in with existing development and attempts to avoid the creation of non-conforming uses by selectively excluding previously developed land. The areas designated for industrial/commercial development are designed to permit a greater range of uses than traditional industrial zoning in order to take advantage of the area’s superior visibility and access and to promote quality development at a major entryway into the City.

Existing uses within the Specific Plan area consist of industrial buildings for warehousing and distribution facilities in the ICDD. One facility located on the 91 Freeway is secondarily used for retail sales to the public which is consistent with the goal of the Specific Plan to take advantage of the prominent location. Within the ACDD one restaurant currently exists which is compatible with the regulations of this Plan and, in fact, the Plan is designed to add an additional food use in the same vicinity as a convenience to those who frequent the area.

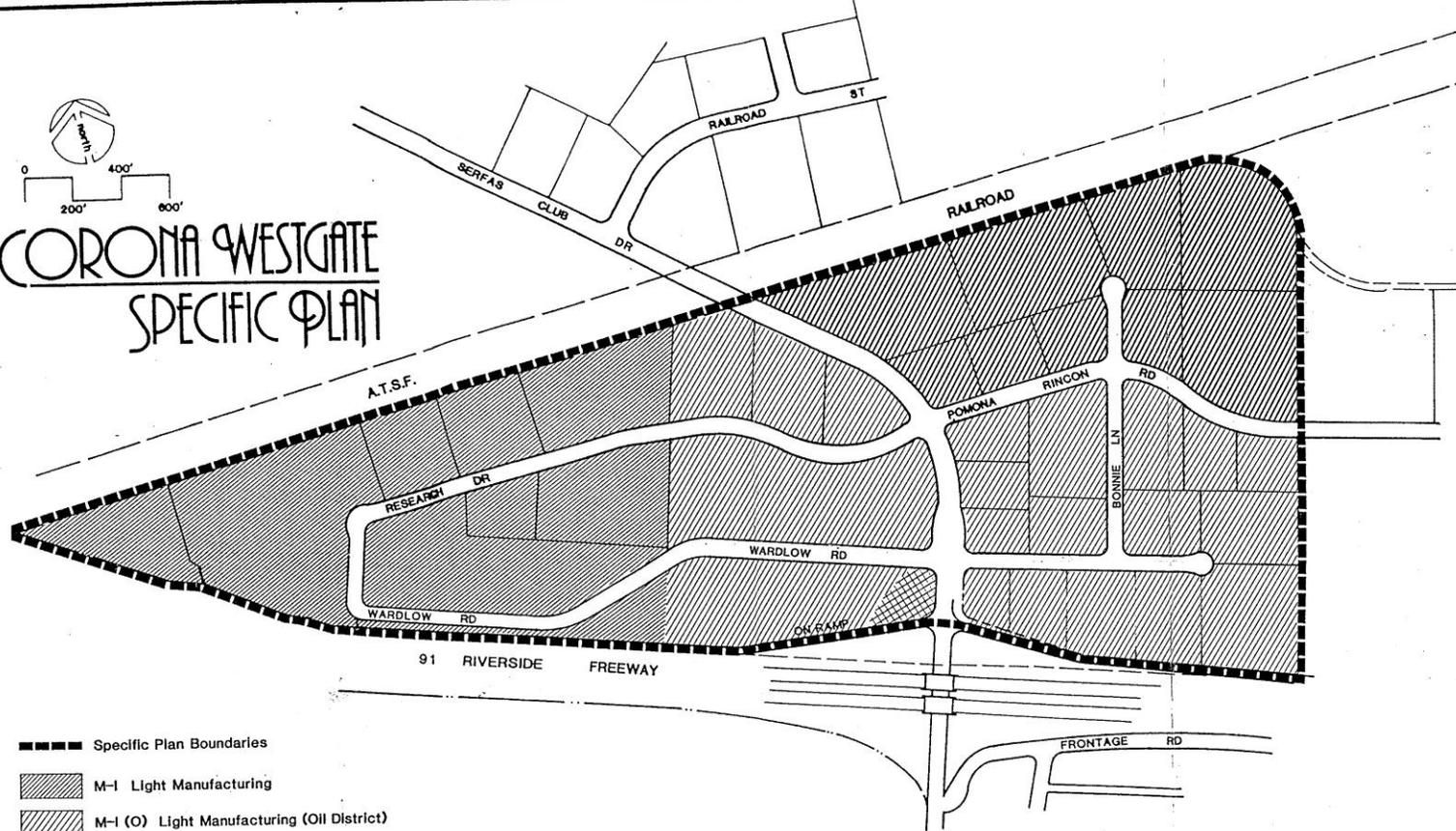
### **Section 201-Land Use Concept**

The Corona Westgate Specific Plan represents a very straightforward concept. Principally, the freeway frontage west of Serfas Club Drive is devoted to auto-retailing, while the remaining property is devoted to industrial/commercial development.

The Auto Center Development District boundaries encompass approximately 27.2 acres and the Industrial/Commercial Development District encompasses 99.1 acres. Another 21.1 acres are devoted to public streets. The existing zoning pattern is illustrated in Figure 3 and the Corona Westgate Specific Plan Land Use Map is illustrated in Figure 4.



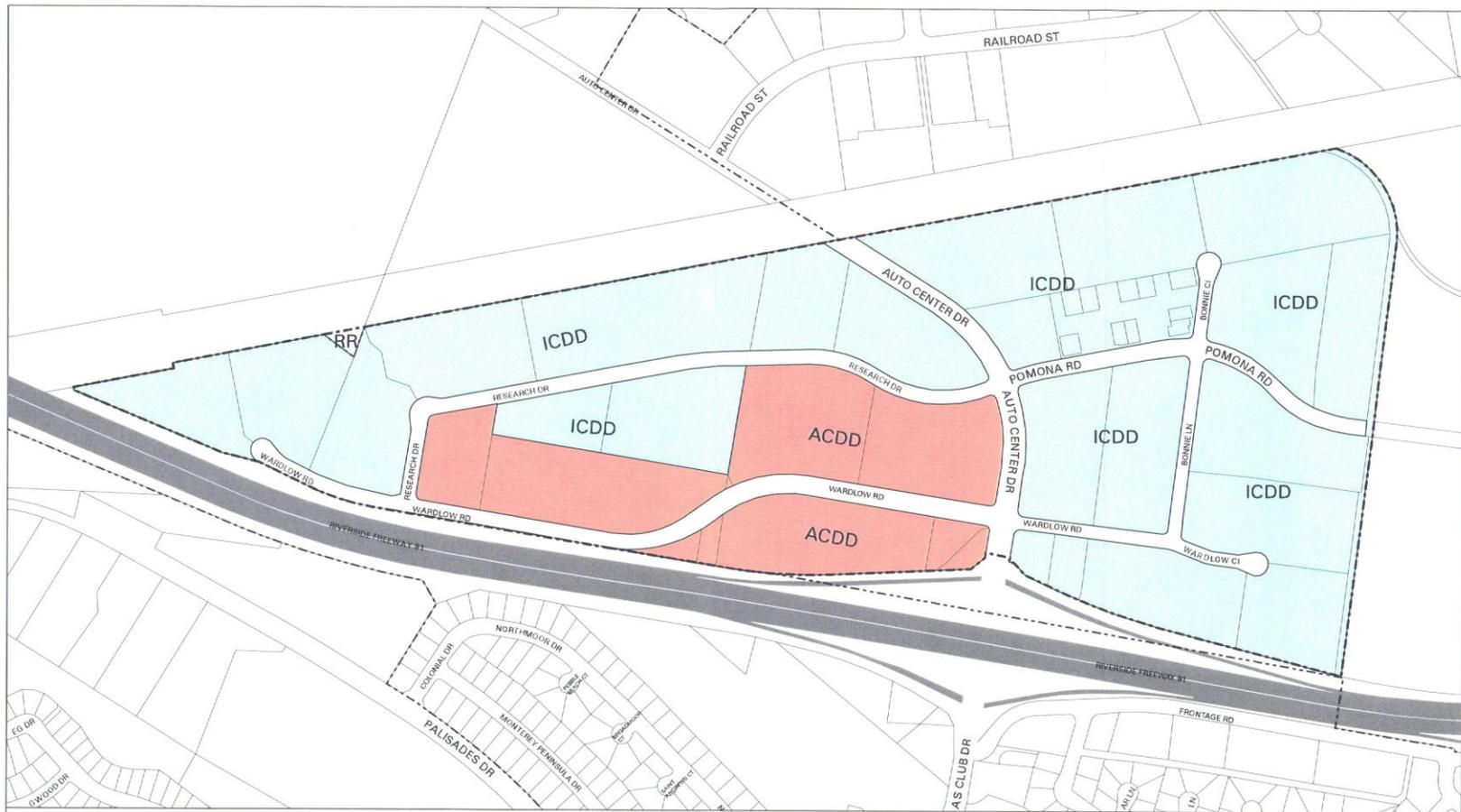
# CORONA WESTGATE SPECIFIC PLAN



- Specific Plan Boundaries
- ▨ M-1 Light Manufacturing
- ▩ M-1 (O) Light Manufacturing (Oil District)
- ▧ C-F Freeway Commercial
- ▨ M-3 (O) Heavy Manufacturing (Oil District)

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FIGURE 3  
Existing Zoning



ACDD Automotive Commercial Development District  
 ICDD Industrial Commercial Development District

### City of Corona 87-1 Westgate Specific Plan

### LAND USE PLAN

This map is intended for general information and planning purposes. The City of Corona is not responsible for any errors or omissions.

City of Corona  
 Geographic Information Services  
 Last Updated On: 11/26/2012  
 Update Through: 3/4/2012

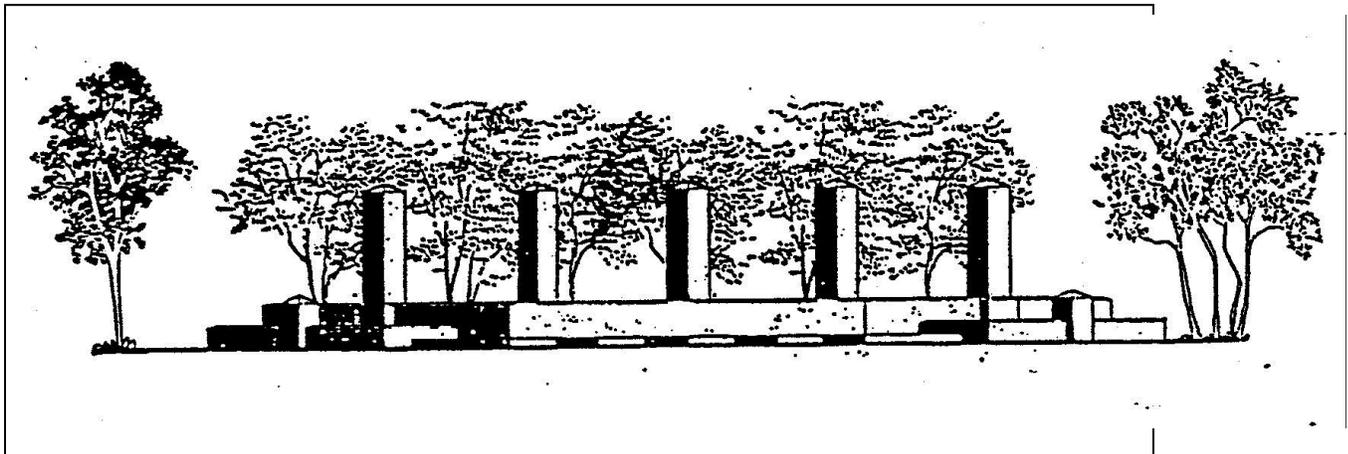
The internal circulation pattern is designed for a convenient, aesthetically pleasing environment in the ACDD to create a “retailing” atmosphere, while the circulation system within the ICDD is more oriented to providing functionally efficient circulation.

Consequently, the environment within the ACDD is oriented toward slow vehicular movement and pedestrian traffic to encourage shoppers to take advantage of the auto-related shopping center. On the other hand the ICDD will consist of uses oriented toward free-standing design rather than a center approach.

The key to the successful implementation of the Specific Plan is the design review-process addressed in Chapter 5 of this Plan. This process permits the applicant and -the City to review proposed development and, in particular, review the issues of site design and architectural compatibility. Through this process, the City has the opportunity to achieve the Plan’s goals by individually assessing the proposed design of each project to prevent, for example, truck loading areas from facing retail or pedestrian areas. This process is flexible to accommodate individual project needs, but thorough enough to - insure compatibility between projects.

The Corona General Plan designations for the Specific Plan area are light industry in proximity to the 91 Freeway with general community-commercial in the vicinity of the Serfas Club Drive off-and on-ramps. To the north of the area designated for light industry (generally north of Pomona Rincon Road), the General Plan permits general industry. The specific Plan is consistent with the General Plan; however, it further defines the existing zoning designations by permitting a larger range of uses in the industrial areas. Additionally, the Plan focuses on taking full advantage of freeway frontage property, which is becoming a very scarce resource within the City (this is particularly true for freeway land in proximity to freeway on-and off-ramps). The increased flexibility of the ICDD relative to existing zoning will permit property to be marketed to a much larger business community, including commercial businesses, and thus create greater value. Additionally, the ICDD will naturally restrict, to some degree, pure warehousing and the truck traffic related to this use, by permitting a broader range of land uses. Designation of property for an auto center will reserve property for this particular use.

Public improvements within the ICDD and those public improvement matters not addressed within this Plan, such as sewer, water, etc., will be considered to be governed by the approved subdivision tract maps, the Municipal Zoning Code or other codes of the City, such as building and fire codes, etc.



## **Section 202-Public Improvement Concept**

The development concept for the auto center, including the general alignment of the public street, is illustrated on Figure SA, entitled the Development Concept Plan.

Parcel Map No. 22687, Figure 6, shows the precise street alignment, which includes a proposed cross section that includes an eighty (80) foot wide street right-of-way. The median width will be fourteen (14) feet, with the travel lanes being twenty-five (25) feet wide on each side of the median. The travel lanes will include one lane in each direction, along with one row of parking on each side. The parkway widths will be eight (8) feet wide and will include a seven (7) foot wide sidewalk. The proposed travel lanes are particularly wide for the planned traffic volume, and provide for a smooth transition to the existing improvements of Wardlow Road, at the intersections of Research Drive and Serfas Club Drive.

The raised median islands are of sufficient width to accommodate fourteen (14) feet of landscaping. The median islands will have left turn pockets to provide left turn access and will provide safe turning movements into the auto dealerships. Additional entrances into each of the proposed auto sites will be provided as each individual site develops. The median landscaping will be similar in design to the parkways in terms of plant materials; however, median islands present the opportunity for additional concentration of trees to accent the vertical element of the auto center without detracting from the visibility to the dealerships or display areas.

Public improvements within the auto center will include concrete curbs and gutters, along with asphaltic concrete pavement. Sidewalks will be provided on both sides of Wardlow Road where medians will be constructed. Wardlow Road, in front of Parcels 1, 2, 3 and 4 will have sidewalks only on the north side of the street. Storm drainage facilities will be provided to protect the proposed improvements and will join existing facilities adjacent to the Project Area. The proposed sewer and water facilities in Serfas Club Drive and Research Drive.

The Redevelopment Agency of the City of Corona will prepare a master landscape plan for Wardlow Road and portions of Serfas Club Drive adjacent to the auto center and construct the improvements for the public right-of-way and all publicly financed signage areas. This master landscape plan; for Wardlow Road, will be based on a turf and palm tree design with textured paving utilized on portions of the median islands. The specified median and parkway trees shall be the Washington Filifera (California Fan Palm). Shrubs and bushes will be used sparingly to offset a particular feature such as entryway signs. To visually link the area together, this landscape treatment will be consistent with what will be required of private owners and, thus, the common feature throughout the District will be the landscape design as opposed to architectural uniformity. Pre-existing municipal standards identifying the quantity of parkway and median island trees shall be adhered to, although alterations in spacing or clustering of trees shall be permitted under this Plan.

The landscape plan for Serfas Club Drive will involve monument signing at the northwest corner of Wardlow Road and Serfas Club Drive to identify the auto center. This signing will be incorporated into a major entryway feature consisting of concrete planters and thirteen (13) foot high decorative columns. This entryway feature will be duplicated on the northeast corner to identify the Westgate Corporate Center property. Similarly, the landscape treatment along Serfas Club Drive will consist of a double row of street trees at forty (40) feet on center and bollards located in a turf area twenty-nine (29) feet in width (approximately half of which is outside the right-of-way line but within a permanent landscape easement area). A fifteen (15) foot wide landscaped easement is to be located along Serfas Club Drive [Center Drive]. A row of shrubs will be planted in a three (3) foot area at the rear of the turfed area. The landscape and hardscape treatment on the west side of Serfas Club Drive shall be provided by the Corona Redevelopment Agency and maintained by the Auto Merchant's Association. Site treatment along the east edge of Serfas Club Drive shall be provided and maintained by the property owner or its successors (See Figure 5B). The following trees and shrubs will be utilized on Serfas Club Drive:

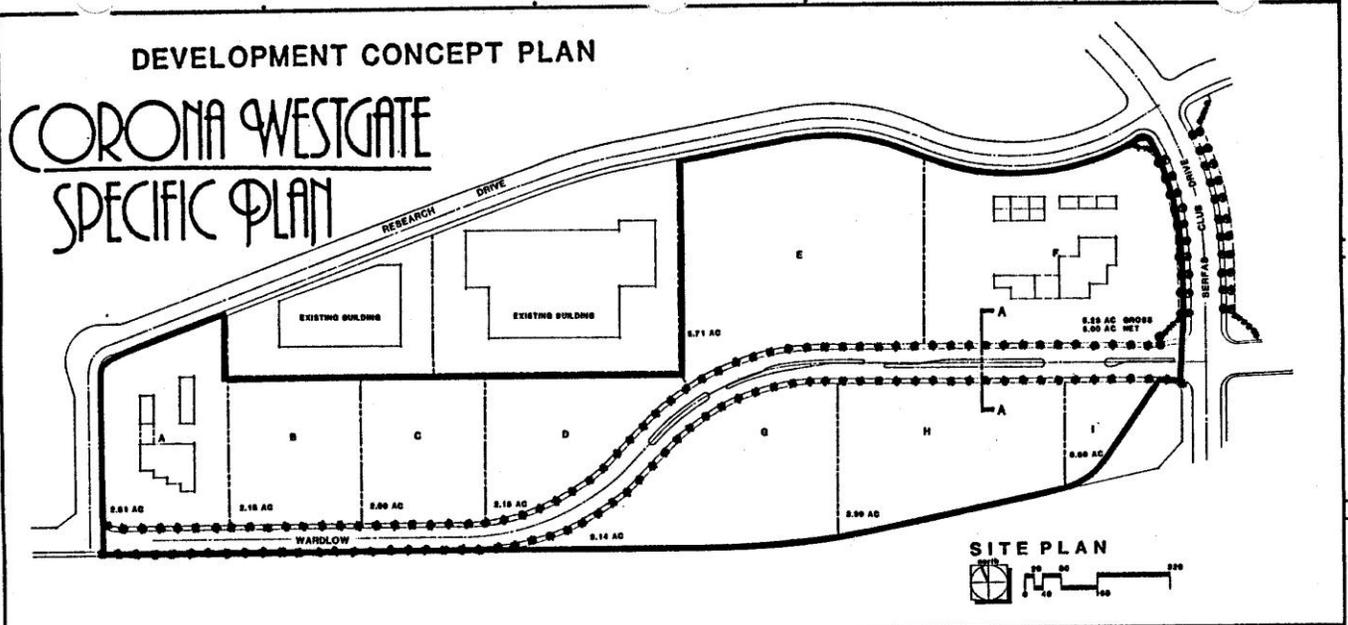
A. **Trees**

Brachychiton Populnelim  
Liriodendron Tulipifera  
Prunus Cerasifera Atropurpurea

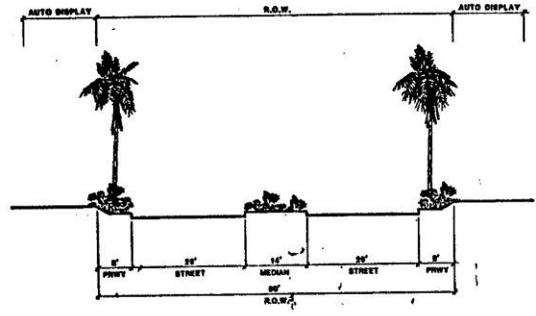
B. **Shrubs**

Rapphiolepis F.  
'Jack Evans'  
Xylosma Congestum  
Photinia Fraseri  
Trachelospermum  
Jasminoides  
Ligustrum Texanum  
Myrsine Africana

All landscaping and signage within the public right-of-way and landscape easements shall be maintained by the respective property owners associations.



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**STATISTICAL SUMMARY**

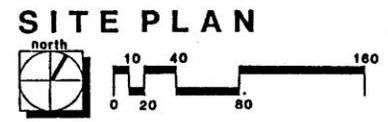
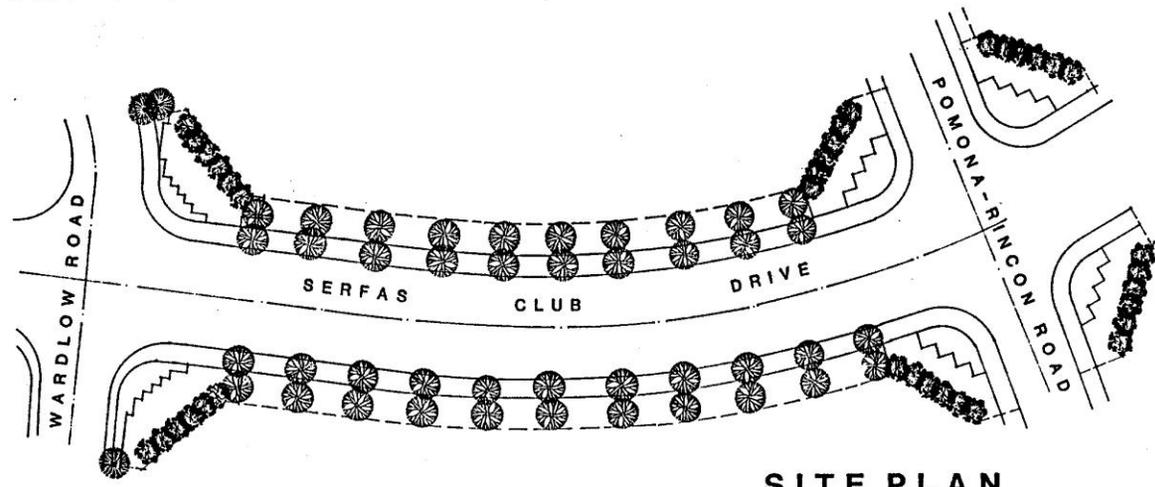
PARCEL	SIZE
A	2.51
B	2.15
C	2.00
D	2.15
E	5.71
F	5.00
G	3.14
H	3.30
I	.50
<b>TOTAL</b>	<b>25.09 AC</b>

- NOTES:**
1. LANDSCAPE SCREEN BUFFER TO BE PROVIDED BETWEEN PARCELS "H" AND "I".
  2. BUILDINGS FRONTING FREEWAY RIGHT OF WAY IN PARCELS "G" AND "H" TO BE PROVIDED WITH SPECIAL ARCHITECTURAL TREATMENT.

**FIGURE 5A**

LANDSCAPE CONCEPT PLAN  
**CORONA WESTGATE**  
SPECIFIC PLAN

14



**FIGURE 5B**

# CORONA WESTGATE SPECIFIC PLAN

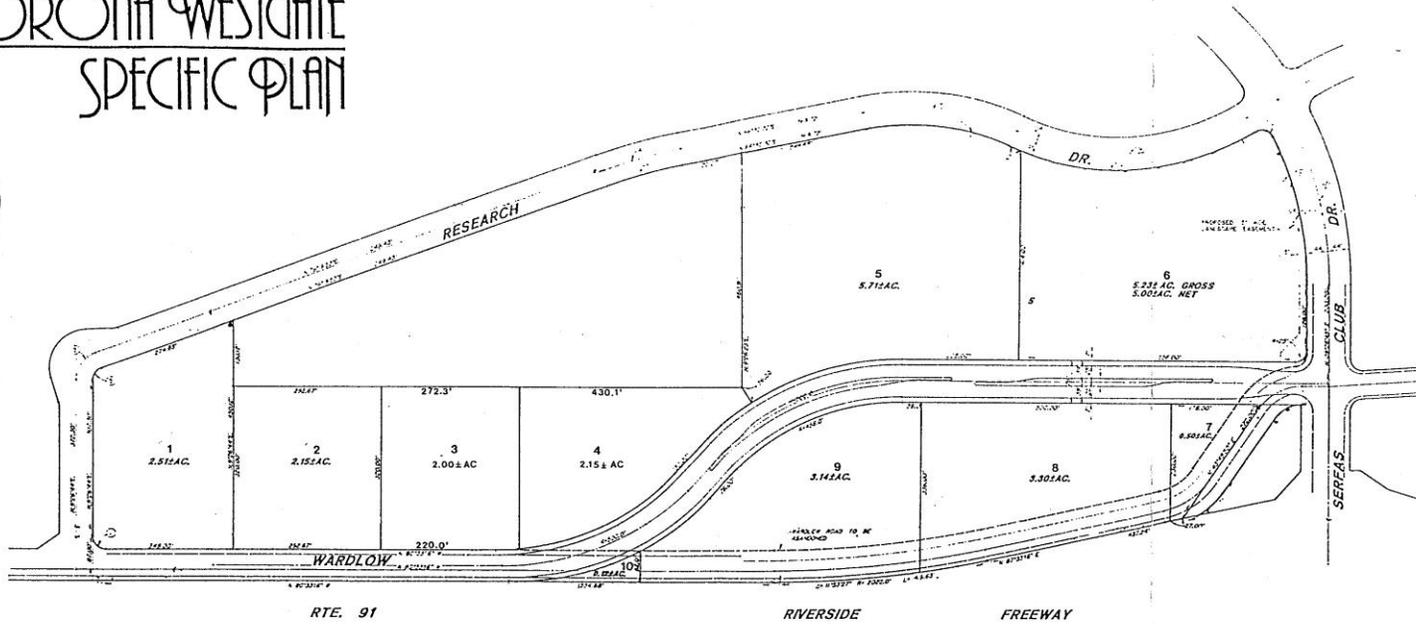


FIGURE-6

**Tentative Parcel Map No. 22687**

IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA

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## **CHAPTER 3: AUTO CENTER DEVELOPMENT DISTRICT**

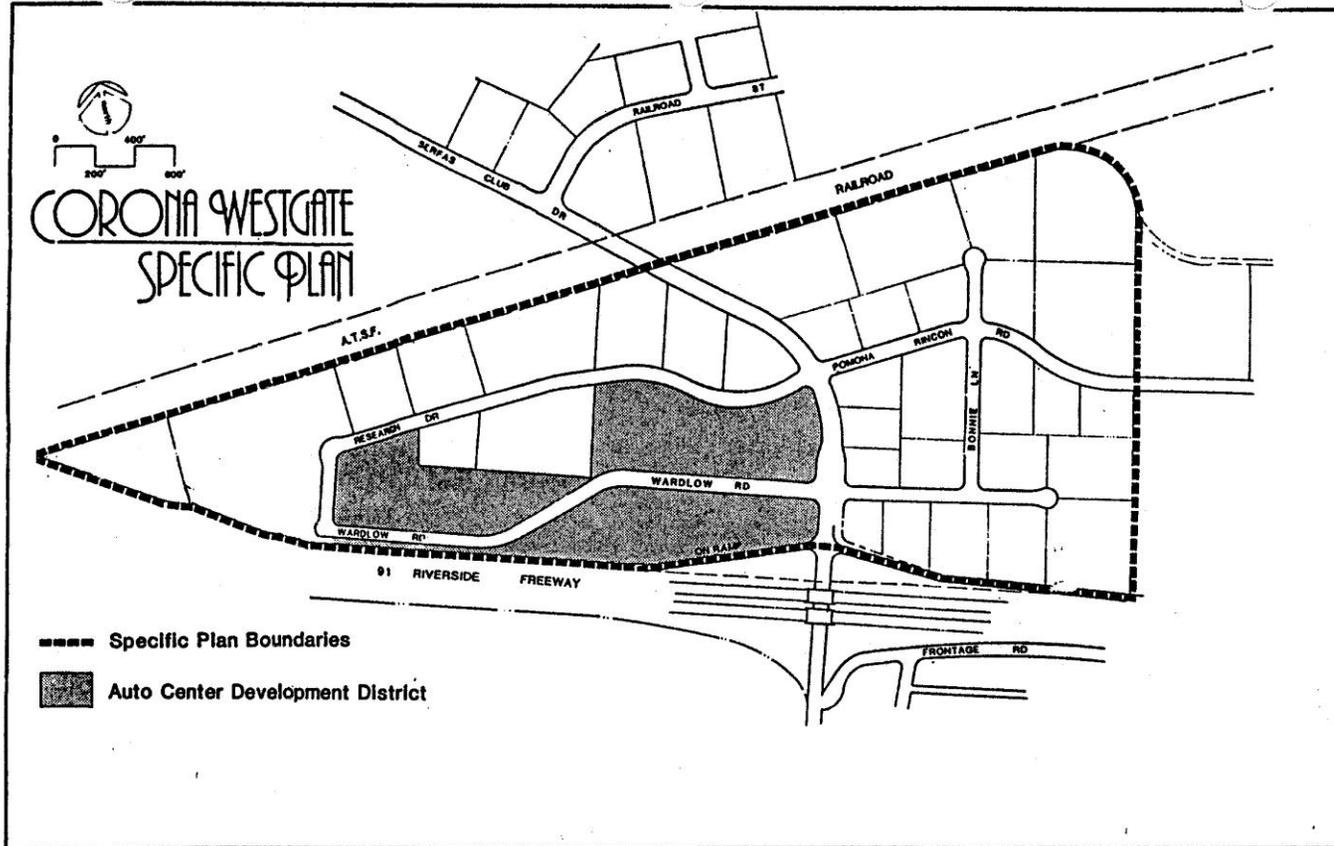
### **Section 300-Description of Boundaries**

The Auto Center Development District is defined by those boundaries identified in the Land Use Map. The ACDD is generally located on the north side of the 91 Freeway, west of Serfas Club Drive, and south Sand east of Research Drive. See Figure 7, Auto Center Development District.

### **Section 301-Purpose and Intent**

The ACDD is intended to reserve certain property for automobile retail sales and support uses and to promote the development of a center consisting of a new and used car, and truck, and recreational vehicle sales facilities with consistent landscaping, signage and compatible architecture. The use of the regulations and development standards contained herein is intended to fulfill the following objectives:

- A. Encourage the consolidation of automobile sales franchises in a shopping center concept.
- B. Provide appropriate signage designed to meet the needs of automobile retailing while remaining aesthetically pleasing to the public.
- C. Provide appropriate circulation with a proper transition to the adjoining development district.
- D. Provide flexible design standards to encourage compatible architecture and site design while encouraging a range of styles to create a sense of excitement and vitality.
- E. Provide minimum development standards to which all development within the District must adhere.



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FIGURE 7  
 Auto Center Development District

## **Section 302-Permitted and Conditional Uses**

Permitted uses within the ACDD are as follows:

### **A. Permitted Uses**

1. New vehicle dealerships oriented principally toward the sale of new passenger vehicles and light - to medium-weight trucks (less than 35,000 lb gvwr).
2. Used vehicle dealerships
3. Vehicle repair service
4. Vehicle part and supply sales
5. Vehicle leasing
6. Recreational vehicle sales
7. Specialty vehicle sales, including motorcycles

### **B. Conditional Uses**

The following uses are permitted only after a valid conditional use permit (CUP) or minor conditional use permit (MCUP) has been approved by the City.

1. Auto body, paint and upholstery shops as an integral but secondary part of operating a new car franchise (CUP)
2. Restaurants, including drive-through, provided they are located within three hundred (300) feet of Serfas Club Drive (CUP)
3. Car wash— self service and full service, subject to the provisions of Chapter 17.72 of the CMC (CUP)
4. Public and private parking lots as an ancillary use to any of the permitted uses listed above (CUP)
5. Auto museum as an ancillary use to a new or used vehicle dealership (MCUP)

### **C. Similar Uses**

Other uses permitted by Commission determination pursuant to Chapter 17.88 of the Corona Municipal Code.

### **D. Prohibited Uses**

1. Any outdoor uses not fully enclosed within a building other than vehicle sales and vehicle storage.
2. Dismantling of vehicles or the storage of vehicles for parts.

## **Section 303-Minimum Site Development Standards**

In addition to those requirements contained within the Municipal Code, the applicable minimum site development standards listed below shall apply. If there is a conflict between the Municipal Code and the following standards, the standards— contained herein shall take precedence (except restaurants shall comply with the applicable Municipal Code standards).

## A. Building Setbacks

1. No structure of any kind, and no part thereof, shall be placed on any site closer to a property line than herein provided. The following structures and improvements are specifically excluded from these setback provisions:

- a. Roof overhangs
- b. Steps and walks.
- c. Paving and associated curbing in relation to landscaped areas.
- d. Walls, except that no wall exceeding three (3) feet in height shall be - placed within landscaped or street setback areas.
- e. Landscaping and security devices such as bollards located within landscaped areas.
- f. Signs and displays identifying the owner, lessee or occupant constructed pursuant to Section 304.
- g. Lighting

2. Setback from interior property lines:

No building setback is required from interior property lines, providing that the building wall does not exceed twenty-four (24) feet in height, or fifty (50%) percent of the length of the property line. Buildings not in conformance with the aforementioned criteria shall set back ten (10) feet from interior property lines.

3. Setback from street property lines:

- a. Sales or display buildings shall be setback a minimum of sixty (60) feet from the Wardlow Road right-of-way and a minimum of ten (10) feet from Serfas Club Drive and Research Drive.
- b. Service and storage buildings shall be setback a minimum of sixty (60) feet from the front property lines and minimum of five (5) feet from Serfas Club Drive and Research Drive.
- c. The Planning Director may alter the aforementioned building setbacks to allow the effective utilization of Lots 4, 8 and 9 on Parcel Map 22687, which are irregularly shaped.
- d. Vehicle display and on-site parking shall be setback a minimum of five (5) feet from street property lines.

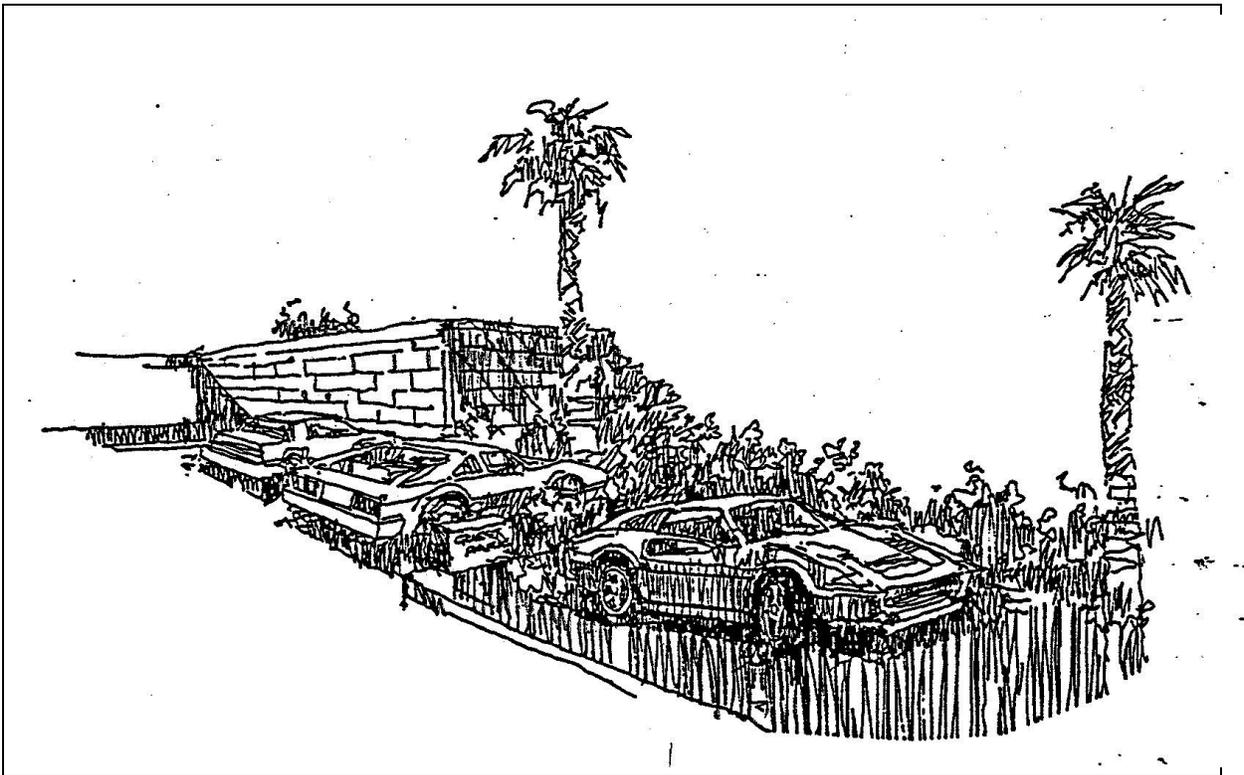
B. Building Heights

1. Buildings shall be a maximum of forty (40) feet in height to top of ridge line.
2. Automobile display and signage shall not be allowed on top of any building.
3. Automobile storage in the second level of a building shall be allowed - only if fully screened from view, including the line of sight from the second story of any structures which may be built across the interior road.

C. Parking

Off-street spaces for vehicle dealerships shall be provided for employees and customers on the dealership site or on a common parking facility especially established for such purposes as follows:

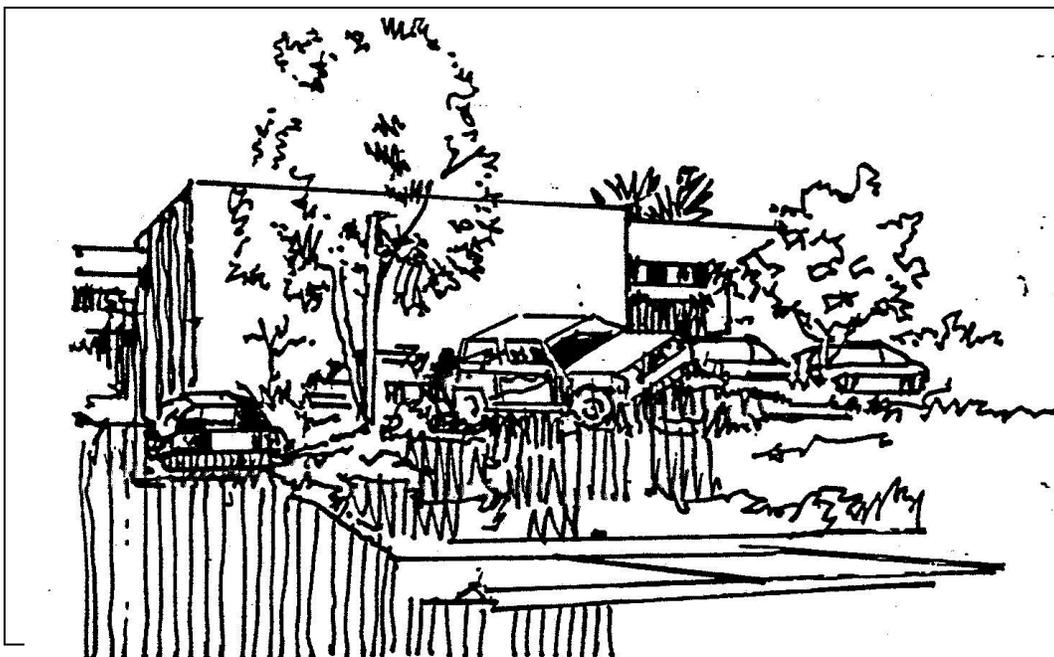
1. Each auto dealer shall provide a minimum of six (6) parking spaces reserved exclusively for customers of new vehicle purchases. These parking spaces shall be readily available to the public and shall be parallel to the street frontage where possible.



2. Employee and additional customer parking shall be provided at a ratio of one space for a) 1000 square feet of display floor area; b) 400 square feet for buildings used for repair, service, parts, supplies and vehicle storage; and, c) 250 square feet for buildings used for office and administration.
3. If employee parking is provided in a common facility shared by one or more merchants, a recorded document shall be filed with the Planning Department and shall be signed by the legal owner of the common site, stipulating the permanent reservation of use of the site for employee parking purposes.
4. On-site handicapped parking shall be provided in a ratio of one (1-) space per forty (40) required parking spaces or one (1) space per parking lot whichever is greater.
5. Parking of display and sales vehicles is exempt from Municipal Code standards for striping, stall size aisle width.
6. Trucks and recreational vehicles in excess o seven (7) feet in height may not be displayed within thirty-five (35) feet of the front yard property line.

D. Landscaping

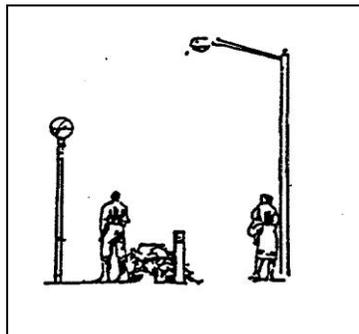
1. Where a parcel abuts a public Street, other than the 91 Freeway, a five (5) foot landscaped setback is required. Properties adjacent to Serfas Club Drive shall have a landscaped setback of fifteen (15) feet.
2. In addition to the landscaped Street setback, 2.5 square feet of landscaping shall be required in the sixty (60) foot building setback area” for each lineal foot of street frontage on Wardlow Road.
3. On-site landscaping shall be compatible with the Design Guidelines which are contained herein, to coordinate the species of plant materials and provide continuity of landscaping within the auto center.



4. Undeveloped areas reserved for future expansion, such as any parcel not promptly built out, shall be maintained in a weed-free condition but need not be landscaped. Such sites shall be maintained in such a manner to prevent dust from blowing onto adjacent properties.

E. Lighting

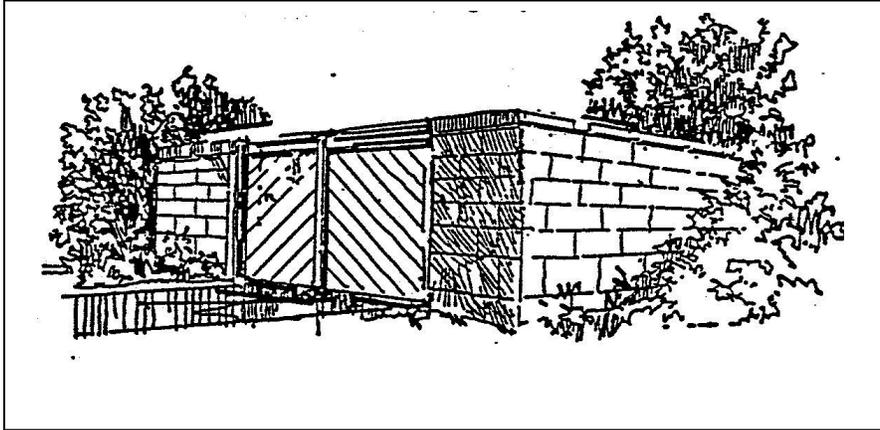
1. Off-site street lighting will be provided by the Redevelopment Agency of the City of Corona.
2. On-site exterior lighting shall comply with the Plan's lighting criteria, as set forth in this Section.
3. On-site lighting plans shall be submitted showing the design layout and exact fixture mounting and wattage proposed by each auto dealer.
4. Areas of display shall be lighted by sharp cut-off fixtures to direct lighting downward only, with particular sensitivity to lighting spillover to Serfas Club Drive, Research Drive and the 91 Freeway.
5. 'Front-row on-site lighting standards adjacent to public streets shall not exceed twenty (20) feet in height and shall have no more than three fixtures per standard at a minimum of thirty (30) feet on center. The interior of the display area may be illuminated by fixtures not closer than sixty (60) feet to the front line of display lights. These fixtures shall be on standards no higher than twenty (20) feet, shall be spaced no closer than sixty (60) feet on center, and shall have no more than four (4) fixtures per pole.
6. Average wattage for the entire display area shall not exceed 1.75 watts per square foot.
7. Service, storage and employee parking areas shall be lighted by standards not higher than twenty-four (24) feet. Average wattage for the entire storage area shall not exceed 0.3 watts per square foot.
8. Creative lighting design and reinforcement of lighting intensity to provide varying degrees of light intensity for merchandising and highlighting purpose are encouraged as long as the overall average wattage is not exceeded.
9. Spot fixtures shall be directed downward only, except at spot display locations along the project's perimeter walls.
10. Strings of incandescent fixtures shall not be allowed in any exterior area.



## F. Walls

Walls constructed on an interior lot line or at the rear of a required landscaped setback of the auto center shall be in keeping with the regulations contained herein.

1. Walls shall not exceed six (6) feet in height, provided that Security fences and walls up to eight (8) feet in height may be permitted subject to Architectural Review, except within building setback areas where walls may not exceed three (3) feet in height.
2. All service, storage and trash areas be screened from view from any public street by a wall. Trash enclosures shall be constructed to the City of Corona enclosure standards on file in the Planning Department.



3. All walls shall be constructed of slump stone masonry block, wrought iron, or a combination thereof.

## G. Sound Attenuation

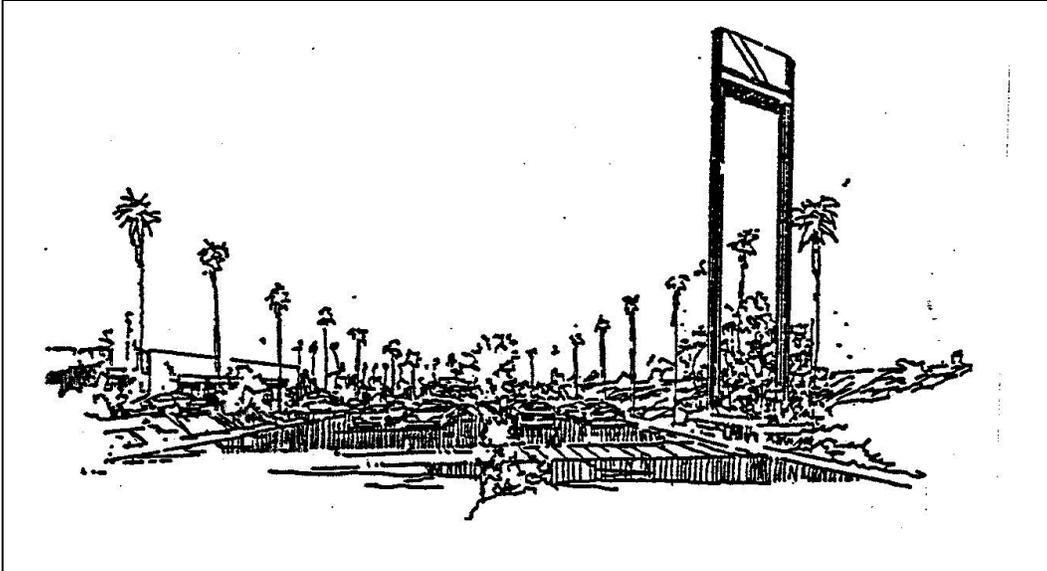
1. All body repair work and all compressor work shall be performed in a fully enclosed area only. Access doors to such enclosures shall be screened or face away from all public rights-of-way.
2. Air compressor exhaust stacks shall contain a muffling device.
3. Exterior loudspeakers shall not be mounted more than ten (10) feet above finished grade and shall be oriented toward the interior of each parcel.
4. Air conditioning units and mechanical equipment may not be mounted on top of any building unless screened from the view of all adjacent properties by a screening technique involving an integrated architectural design element.

## **Section 304-Signage**

### A. Introduction

The purpose of this Section is to guide automobile dealers in the selection and placement of their signage. It is the intent to provide a reasonable number and size of signs.

It is the further intent to limit the number and size to that required for proper conduct of business and to control the design and placement of all signage. It is desired that artistic flexibility be allowed while maintaining continuity and appropriate scale to the center as a whole. The information contained in this program as adopted by the City of Corona establishes mandatory criteria to which each sign must conform. Each business will be responsible for the construction, installation and maintenance of its signage, and must submit for design review approval to the City of Corona pursuant to Chapter 5 of this Plan. Submitted drawings must indicate location, materials, finishes, height, square footage and method of installation for all proposed signage.



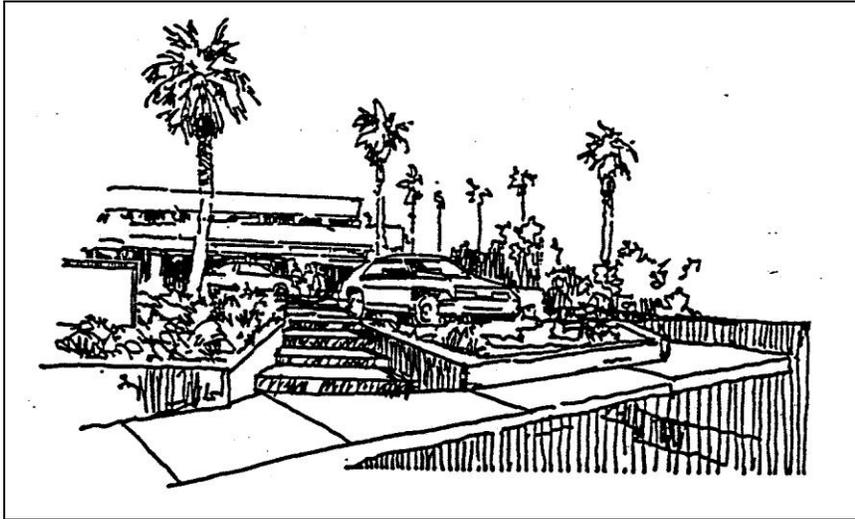
#### B. Identification Signs for the Auto Center

Identification signs for the auto center will be provided by the Redevelopment Agency of the City of Corona to attract customers into the auto center. Such identification signage shall include a monument sign at the Serf as Club Drive entrance to the auto center not to exceed one hundred twenty (120) square feet per sign face, major freeway signage not to exceed seventy-five (75) feet in height and six hundred (600) square feet per sign face, and directional street signage not to exceed eight (8): square feet per sign face.

#### C. Automobile Dealers' Signs

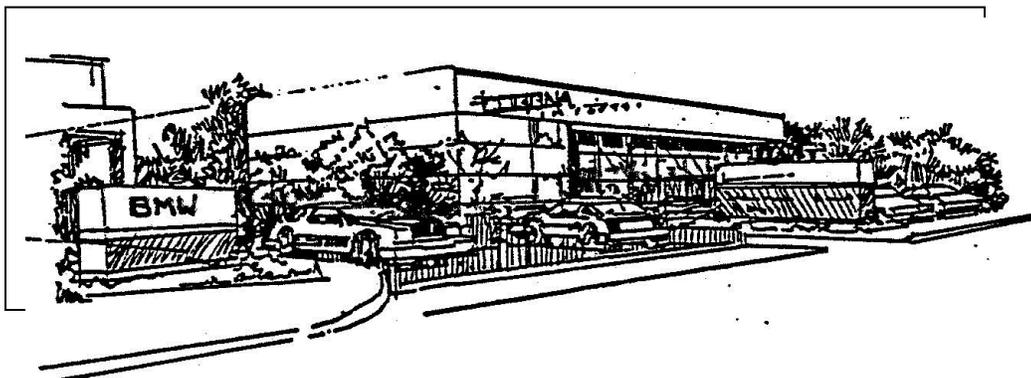
##### 1. General Criteria

- a. There shall be no pennants, display flags, spinners, exposed lamp or other attention-getting devices, except as may be approved by the Planning Director (created through the CC&R's), for periods not to exceed thirty (30) days for promotional purposes for the entire center.
- b. Banners, paper signs or painted signs on windows or similar temporary advertising, will be permitted for a period, not to exceed thirty (30) days, for special promotional activities, subject to the approval of the Planning Director.



- c. No exposed raceways, ballast boxes or transformers will be permitted.
  - d. illuminated signs shall be internally lighted by fluorescent or neon tubes. No luminous or reflective background or script may be attached or applied.
  - e. Sign company names or stamps shall be concealed.
  - f. Locations, type and size of all permanent signs shall be indicated on the drawings submitted during the Design Review Process for approval by the City.
  - g. The maximum height of all signs shall be measured from the nearest curb grade or grade of the nearest ground level (non-elevated) paving area, whichever is higher.
  - h. No roof-mounted signs will be permitted.
  - i. The color of signs should coordinate with the overall building color scheme. Signs should reflect high-quality design and not be “gimmicky” in use of materials, color or form.
2. Street Identification Signs
- a. One street-front signs shall be permitted per franchise.
  - b. Street signs may contain the following information
    - i) Brand(s) of vehicle sold
    - ii) Manufacturer’s logo
    - iii) Dealer’s name
    - iv) Identification of used cars and/or trucks

- v) Secondary manufacturer's lines of similar identification
  - c. Each sign face may not exceed sixty (60) square feet in area not exceed a height of twenty-two (22) feet.
  - d. Each sign may be double sided.
  - e. Signs shall be located not closer than seventy—five (75) feet from a common lot line between dealerships and not closer than one hundred and fifty (150) feet between signs. Location and spacing of signs is subject to review and approval through the design review process pursuant to Chapter 5 herein.
  - f. Signs shall be monument or pylon signs and shall be constructed of solid masonry or metal panels. Signs shall be trimmed out with a material which is used in the main showroom building in order to promote an architecturally coordinated look for each site.
  - g. Signs shall be lighted internally. No external lighting shall be allowed, except for monument signs constructed of natural materials.
  - h. Signs shall be contained within a planting area consisting of grass or selective ground covers equal in area to the area of a single sign face (a sixty (60) square foot monument sign should be located within a landscaped area of sixty (60) square feet). Monument signs, except for structures described in CMC Section 17.70.040(B), shall be located outside corner cut-off areas set forth in CMC Section 17.70.050 and shall not exceed a height of six (6) feet.
3. Street Directional Signs
- a. Signage information may include directions to Entrance, Service, Parts, Customer Parking, etc.
  - b. Directional signs shall not exceed six (6) square feet per sign face, nor have a sign face dimension in excess of four (4) feet.
  - c. All signs may be double sided.
  - d. Signs shall be located at driveway areas.
  - e. Materials used shall be compatible with the dealership's building design and the overall sign program.
  - f. Signs shall be internally lighted only.
  - g. Signs shall be set back three (3) feet from the front property line.





4. Building-mounted Signs

- a. May contain manufacturer's logos and brand of vehicle sold.
- b. Letter sizes shall be promotional to the size of the mounting surface.
- c. Information may be displayed once per building face for each Street or freeway orientation, up to a maximum of three sides of a building.
- d. Letters shall be channel type and shall be internally illuminated or back lighted. Letters shall be of a single color except for the logo.

5. Information Signs

- a. Signs shall be single sided, mounted flat on a building or wall, and shall not protrude out from the building. Such information signs shall be permitted to identify service areas or other specific functions of a given dealership.
- b. Area shall not exceed four (4) square feet.
- c. Materials shall be compatible with the building's design.

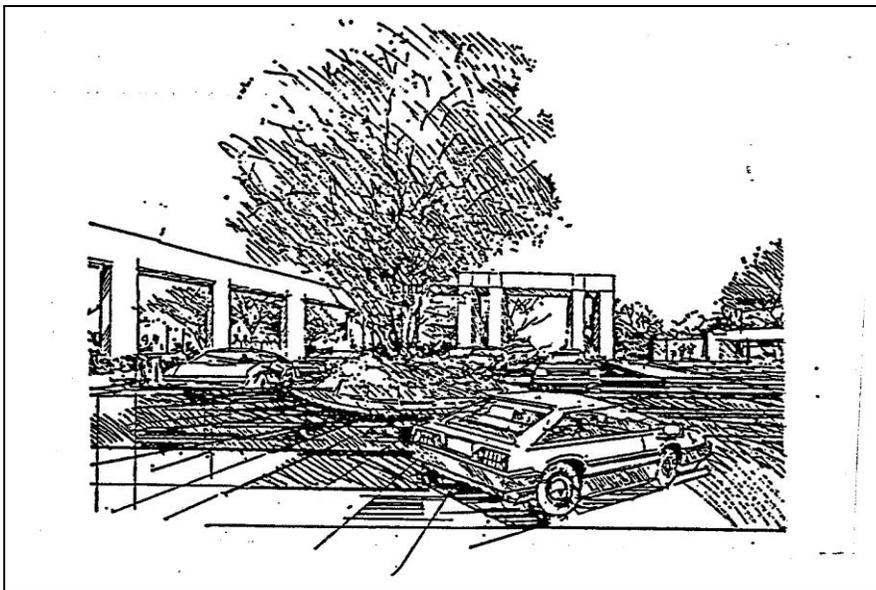


### **Section 305-Other Governmental Codes**

All Development within the ACDD must comply with all applicable governmental codes. When there is a conflict between this Specific Plan and the Municipal Zoning Code, the provisions of this Specific Plan shall take precedence.

### **Section 306-Design Review Requirements**

All projects within this District involving the construction or rehabilitation of property are required to be in compliance with the Design Guidelines (Section 307) and the provisions contained with Chapter 5 entitled “Design Review Process”.



### **Section 307-Design Guidelines**

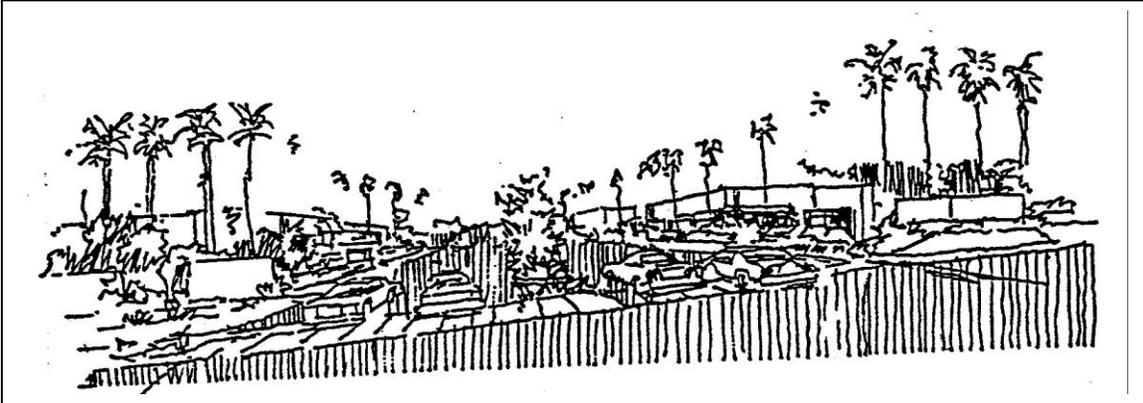
#### **A. Intent**

The Design Guidelines are intended to give direction to developers of auto dealership’ projects within the ACDD to encourage the orderly and harmonious design of structures, landscaping, parking areas, lighting and accessory structures. The Guidelines establish a standard for design quality while maintaining flexibility for individual expression and imaginative design solutions.

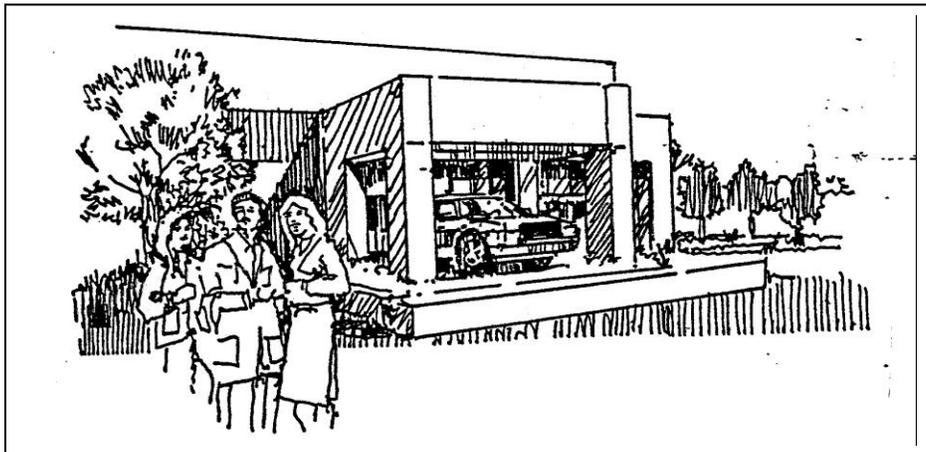
#### **B. Site Planning**

Site planning should involve simultaneously locating indoor and outdoor spaces for the greatest comfort and enjoyment of the retail customer.

1. When locating buildings, parking and walkways on a parcel, all desirable site features (freeway frontage, corner lot, irregular lot) should be used to an advantage.
2. Relate the location of site uses with adjoining properties to avoid possible conflicts such as obscuring vehicle display areas. Walls between the building setback areas of adjacent auto dealerships are discouraged and pedestrian connections are encouraged.



3. Locate pedestrian-oriented uses on the ground level of buildings. To the extent possible, business office space and other uses not dependent on pedestrian traffic should be located on upper levels.
4. Locate buildings on the site to enhance architecture and vehicle display of both the site and adjacent parcels.
5. Consideration should be given to the compatibility of proposed uses with existing adjacent uses. Thus it would be inappropriate to locate entrances to repair facilities adjacent to display areas on adjoining parcels.



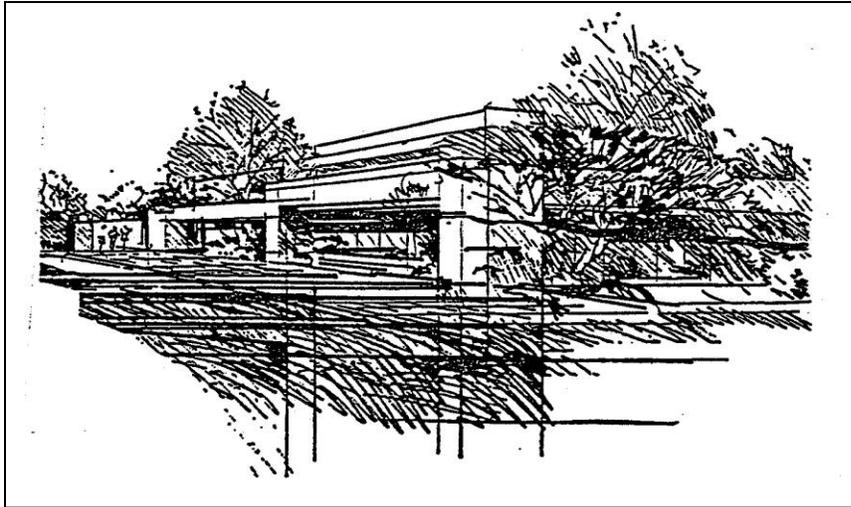
6. Consider setbacks of adjacent properties. While articulation of building edges adds interest to the streetscape, the Street should function as a whole and the setbacks should be related.
7. Vehicular access to retail areas should be controlled and properly spaced to ensure easy ingress and egress.
8. Access to loading and service bays should be separate from customer parking where feasible.
9. All properties shall provide attractive, direct and safe pedestrian access and circulation.
10. To the greatest extent possible, building improvements should be symmetrically placed on a parcel and designed to enhance rather than detract from the visibility of auto displays on adjacent properties.

C. Architecture

The Corona Westgate Specific Plan does not set forth a specific design theme, but rather seeks well-thought-out design solutions that are compatible but distinct from other architecture within the District. The ‘planned’ auto center should result from consistent site planning and landscaping rather than building design. The following standards will apply to the ACDD.

1. Buildings

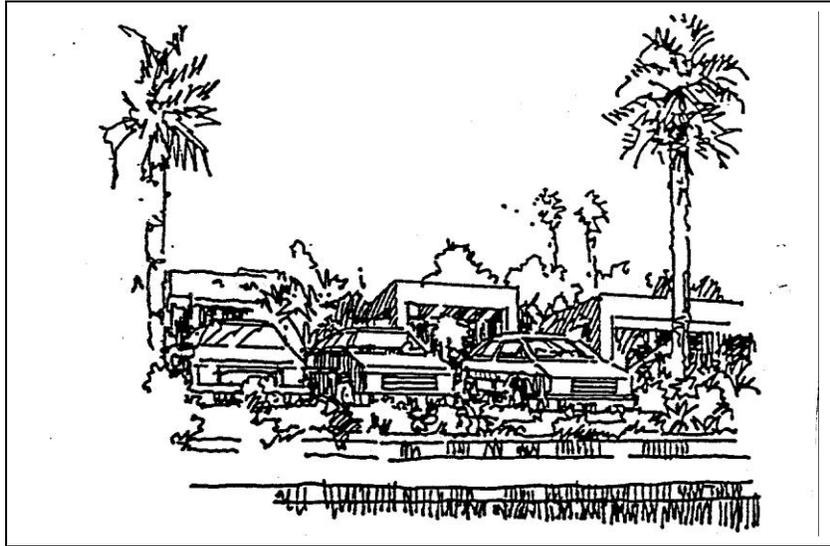
- a. Keep the design statement simple and materials and details Consistent through the use of common colors, signs, lighting treatment and landscaping.
- b. Building materials should be relatively permanent, requiring minimum maintenance, in order to avoid deterioration.
- c. Within an individual project, materials, textures and colors should be consistent with the individual design theme and compatible with the- facing materials.
- d. Shadow patterns created by architectural elements such as overhangs, trellises, projections, reveal and awnings contribute to a building’s character while aiding in climate control.
- e. Building facades which face adjoining streets and the 91 Freeway shall provide visual relief and shadow patterns designed to provide variety to the adjacent streetscape.
- f. Large-scale buildings with undistinguished masses and bulk are discouraged. Efforts should be made to design the structures to a human-scale environment. Contributing factors are varying the planes of exterior walls and varying the height of the structures.



- g. Auxiliary structures should be architecturally compatible with primary structures.
- h. Newspaper racks or phone booths should be integrated into the interior of a structure.
- i. Exterior vending machines, such as soft drink dispensers or cigarette machines or magazine racks are discouraged.

## 2. Roofs

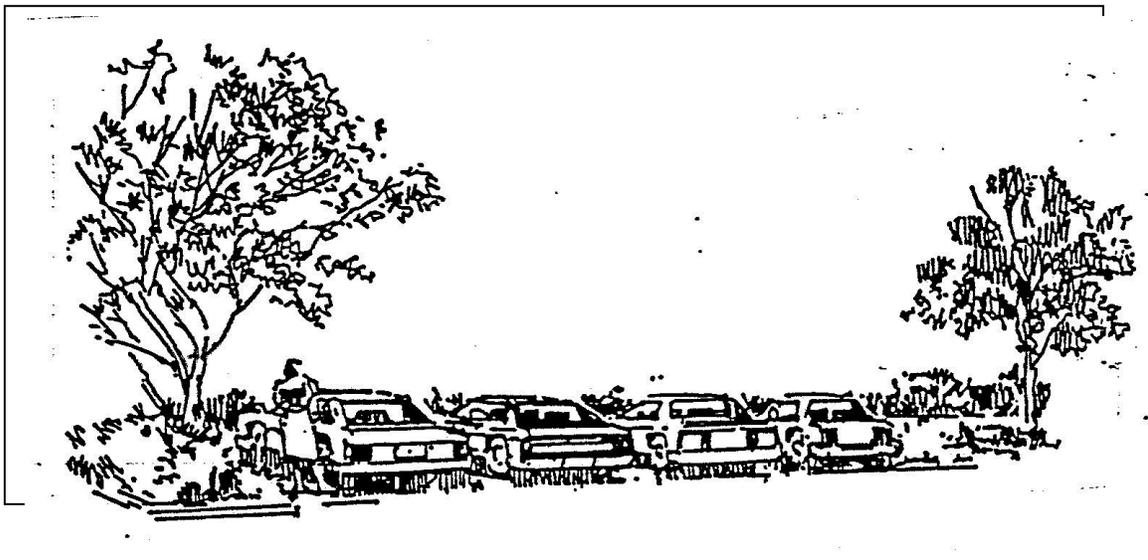
- a. In designing buildings, consider the roof lines of buildings on adjacent properties to avoid conflicting style and materials.
- b. The roof line of a building should be designed in conjunction with its mass and facade so that the building and its roof form a consistent composition as viewed from adjacent street.
- c. Roofing materials should be appropriate to the style and colors of the building. Vertical design details can be used to provide visual diversity.
- d. All roof-top mechanical equipment shall be screened from adjacent properties and public rights-of-way. Screening design and materials shall be consistent with the overall building design.
- e. The relationship of height, bulk, and configuration between developments should be compatible. Individual expression of separate developments or of freestanding buildings should be developed in harmony with the context of the auto shopping center as a whole.



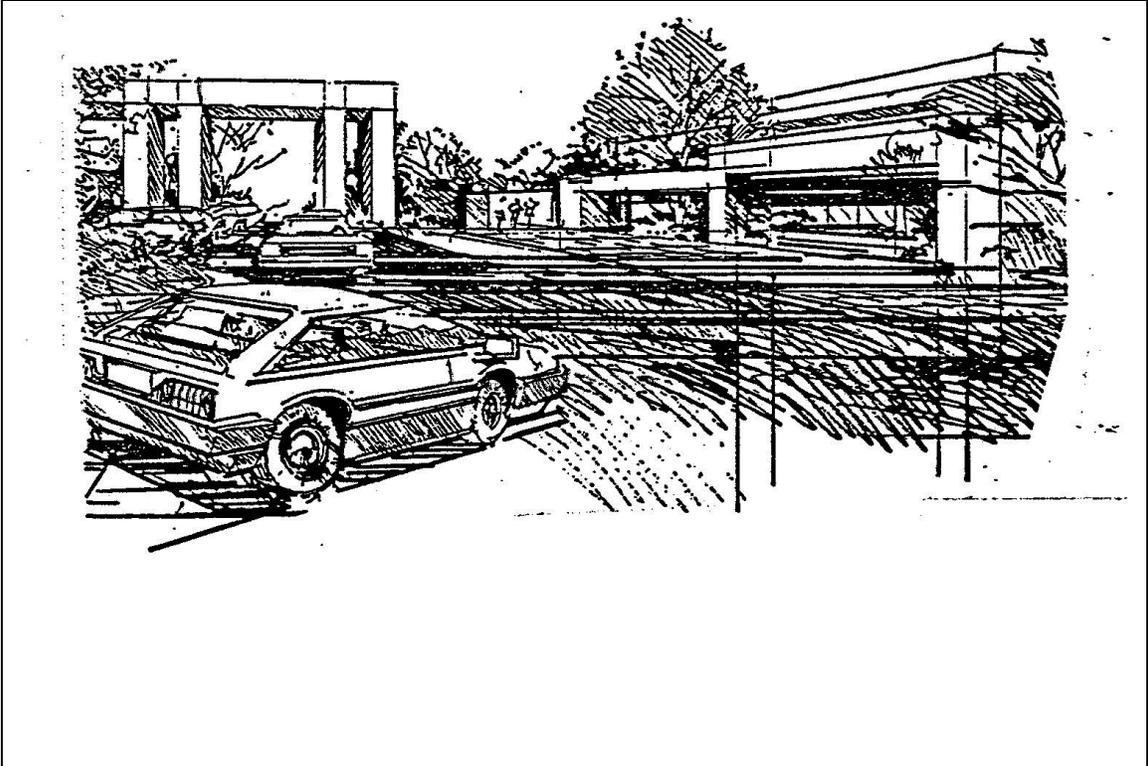
#### D. Landscaping

Landscaped areas should be planned as an integral part of the project, and not merely located in leftover portions of the site.

1. The location of plant materials should respond to the architectural design of the building to key-note entries, contrast with or reinforce, building lines and volumes, and soften hard structural lines.
2. Plant materials should be used to define Street edges, outdoor display areas and pedestrian pathways.
3. Plant materials should be used to screen trash enclosures, utilities and mechanical equipment.
4. Plant materials should be integrated in the building form as architectural element (i.e. cascading from balconies, potted trees on upper story patios, etc.) wherever possible.
5. Landscaping should be used to create nodes within both auto and pedestrian be useful in distinguishing special display areas.



6. When solid building walls or perimeter walls are within the view of a public street, one tree for each fifty (50) lineal feet of building or perimeter shall be planted (trees may be clustered).
7. Encourage the incorporation of artistic expression elements such as sculptures, water elements, etc.
8. Use vertical landscape elements to enhance the street-side elevation of buildings and to allow view of signs and architectural features from the street.



9. Consistent landscape themes shall be used between adjacent developments. It is recommended that a simple grass and California Fan Palm tree design be encouraged throughout the interior of parcels with shrubs, bushes and ground covers from the following list utilized for foundation and accent planting:

a. Shrubs

Tupidanthus  
Hibiscus  
Dwarf Oleander

Tupidanthus calyptratus  
Hibiscus rosa - sinensis  
Nerium oleander 'Petite Salmon' or  
'Petite Pink'

Dwarf Pittosporum  
India Hawthorn  
Compact Natal Plum

Pittosporum tobira 'Variegata'  
Ralphiolepis indica 'Jack Evans'  
Carissa grandiflora 'Boxwood  
Beauty' or 'Tuttle'

Lily of the Nile  
Privet  
Compact Shiny Xylosma  
Pink Escallonia

Agapanthus africanus  
Ligustrum 'Tuttle'  
Xylosma congestum 'Compactum'  
Escallonia fradesli Orange  
jessamine Murraya  
paniculata

Ternstromia  
Fraser's Photinia  
Viburnum

Ternstroemia gymnanthera  
Photinia fraseri  
Viburnum davidli

b. Ground Covers

Star Jasmine  
Prostrate Natal Plum  
Blue Rug Juniper  
Turf grass  
Trailing Lantana

Trachelospermum jasminoides  
Carissa grandiflora 'Green Carpet'  
Junipers horizaontalis  
'Marathon' fescue  
Lantana montividentis

## **CHAPTER 4: INDUSTRIAL/COMMERCIAL DEVELOPMENT DISTRICT**

### **Section 400-Description of Boundaries**

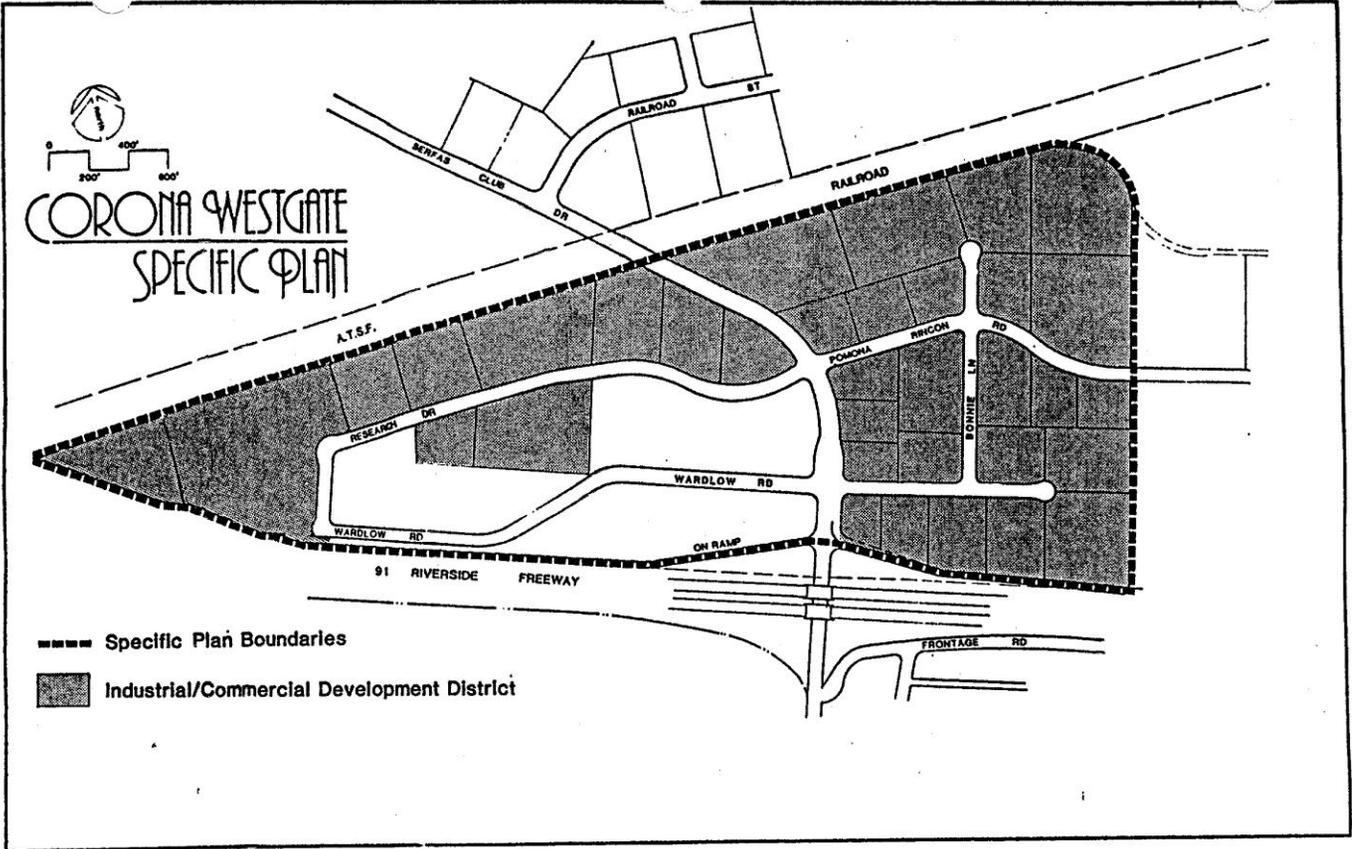
The Industrial/Commercial Development District (is defined by those boundaries identified in the Land Use Map. The ICDD is generally located north of the 91 Freeway, south of the A.T. & S.F. railroad right-of-way and includes those properties within the boundaries of Subdivision Tract Maps Nos. 20137 and 19150-1, and Parcel Map 19420 (except for the ACDD properties). See Figure 8, Industrial/Commercial Development District.

### **Section 401-Purpose and Intent**

The ICDD is intended to encourage the development of the District properties to their highest and best use. The ICDD permits both industrial and commercial development and will allow, in particular, the development of the freeway frontage property and those parcels in the vicinity of the Freeway ramps to develop to commercial uses designed to serve the greater community. The ICDD is designed not only to permit and encourage commercial development but to provide the flexibility to integrate mixed land uses in a compatible fashion.

The use of the regulations and development standards contained herein is intended to fulfill the following objectives:

- A. Encourage the maximum utilization of Freeway-oriented property.
- B. Increase the demand for property within the District by broadening the permitted uses.
- C. Increase employment opportunities within the community.
- D. Provide regional and sub-regional commercial facilities to better serve the community.
- E. Provide flexible design standards to encourage compatible architectural and site design.
- F. Provide appropriate signage for the type of land uses proposed.
- G. Establish appropriate development standards to which all development within the District must adhere.



GRC

FIGURE 8

### Industrial/Commercial Development District

## Section 402-Permitted and Conditional Uses

A. Permitted uses within the ICDD are as follows:

1. Warehousing and distribution of non-toxic goods, materials and products providing that the storage is fully contained within an enclosed building.
2. Manufacturing, assembly, fabrication and painting of non-toxic goods and products providing that all operations are fully contained within an enclosed building.
3. Research and development facilities including laboratories and testing facilities.
4. Office and professional uses and service uses designed to meet the needs of the manufacturing community as listed below [by SPA 89-19]:

Accountants  
Advertising  
Architectural Services  
Attorneys  
Bank and Bank Administrative Offices  
Builders  
Business Associations  
City Services  
Civic Associations  
Commodity Brokers, Dealers & Traders  
Communications Companies  
Copy Equipment sales, engineering, repair  
Counseling  
Credit Bureaus, Agencies, Reporting &  
Collection  
Credit Unions  
Data Processing Services  
Designer - industrial, interior - graphic  
Employment Agencies  
Engineers  
Escrow agents  
Finance Companies  
Financial Management  
Government Agencies  
Holding Companies  
Housing Authorities  
Industrial Associations and Trade Group  
Offices  
Industrial Computer Equipment Sales, Engineering, and Repair  
Industrial Telecommunication Equipment  
Sales, Engineering, and Repair  
Insurance

Investment Bankers  
Investment Services  
Labor Union Administration  
Leasing Company Office  
Loan Companies  
Management Consultants Manufacturer's  
Representatives (industrial products)  
Market Research  
Membership Organizations  
Merchants Associations  
Mortgage Companies  
Nurses Registries  
Personnel Agencies  
Pharmaceutical Sales  
Political Organizations  
Professional Organizations  
Public Relations Services  
Real Estate Appraisers  
Real Estate Brokers  
Real Estate Developers  
Savings and Loan Association Secretarial Services  
Securities Brokers  
Social Organizations  
Social Services  
Surveying  
Title Companies  
Trade Associations  
Travel Agencies  
Similar uses permitted by Commission determination

5. Commercial facilities with a regional or sub-regional orientation such as the sale of appliances, furniture, electronic goods and other consumer products occupying a floor space in excess of 2,000 square feet.
6. Restaurants other than restaurants with drive-through service.
7. Other uses permitted by Commission determination pursuant to Chapter 17.88 of the Corona Municipal Code.
8. Those permitted uses in the Auto Center Development District (ACDD) whether or not in conjunction with a new car franchise.

B. Conditional Uses within the ICDD are as follows:

1. Distribution and warehouse facilities with outdoor storage providing that such outdoor storage is not visible from a public street or adjacent parcels.
2. Manufacturing facilities with outdoor storage facilities providing that such outdoor storage is not visible from a public Street or adjacent parcels.

3. Uses requiring the storage or parking of more than five (5) trucks or tractors for more than four hours per day.
4. Restaurants with drive-through service.
5. Medical facilities.
6. Automotive body repair and painting shop with all operations fully contained within an enclosed building.

C. Prohibited Uses within the TCDD are as follows:

1. Auto wrecking and towing
2. Construction equipment yards
3. Disposal or transfer facilities
4. Truck terminals
5. Truck fuel and overnight parking facilities
6. Batch plants handling bulk materials such as cement, sand, rock, etc.
7. Neighborhood-level commercial uses such as residential real estate offices, beauty shops, mini-marts, grocery stores, drug stores, sundry shops, and other similar uses occupying less than 2,000 square feet of building area.
8. Those manufacturing uses unable to meet the performance standards contained in Chapter 17.84 of the Municipal Zoning Code or contained within this Specific Plan.
9. Livestock or poultry raising, breeding or processing.

### **Section 403-Minimum Site Development Standards**

#### **A. Building Heights**

No building shall be constructed having a structural height in excess of fifty-five (55) feet. Rooftop mechanical equipment and screening may exceed said height limitations by not more than ten (10) feet.

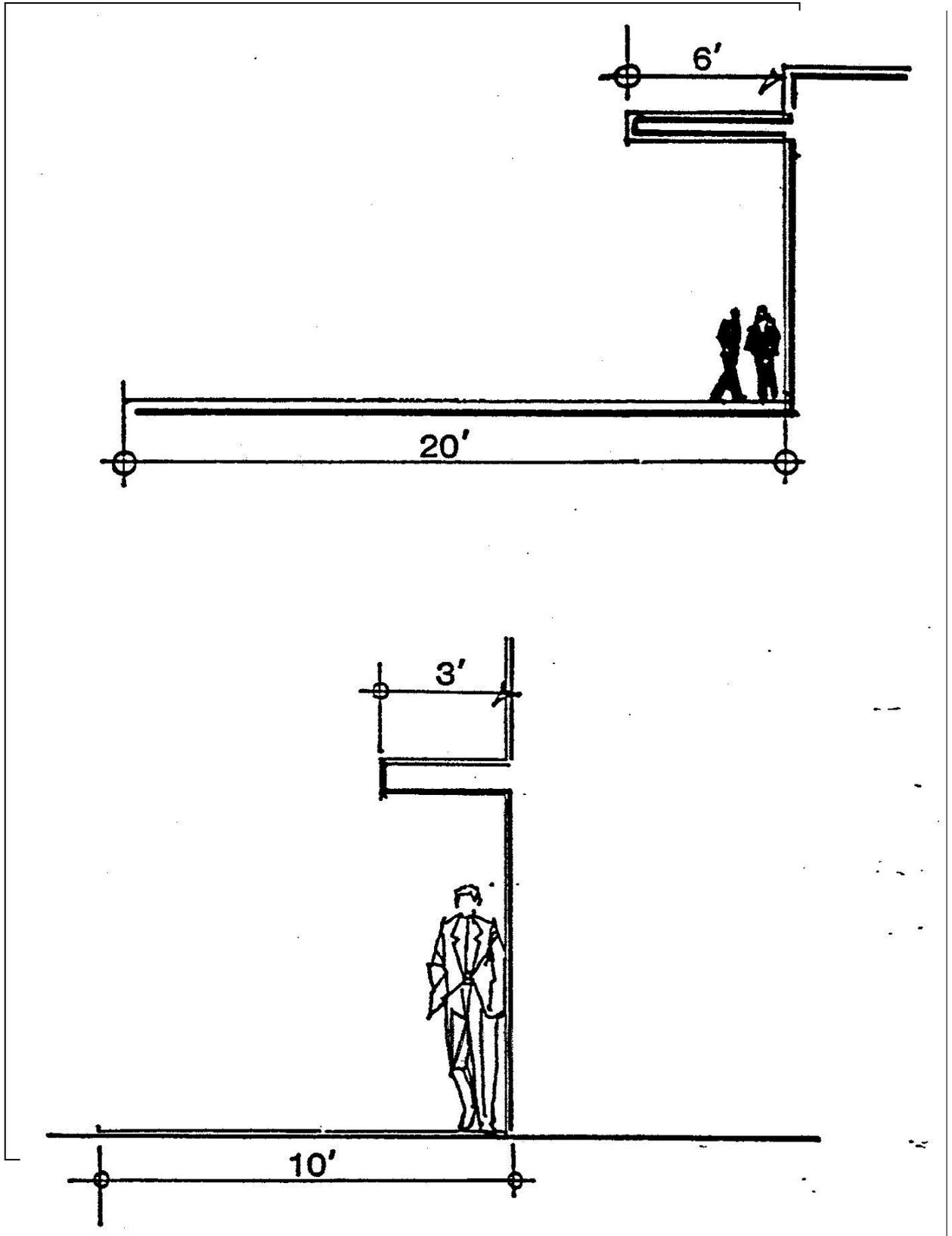
#### **B. Building Setbacks**

All building setbacks shall be measured from property line. A setback area shall extend for the full width of the parcel and shall be parallel to the property line. Building setbacks shall be of the following depths:

1. Front yard and side yard setbacks adjacent to Serfas Club Drive shall be a minimum of twenty (20) feet. Front yard and side yard setbacks adjacent to all other streets shall be a minimum of ten (10) feet. No parking shall be permitted within these setbacks. Entry monuments identifying the industrial center shall be exempt from the setback requirements per Chapter 17.70 of the Corona Municipal Code.

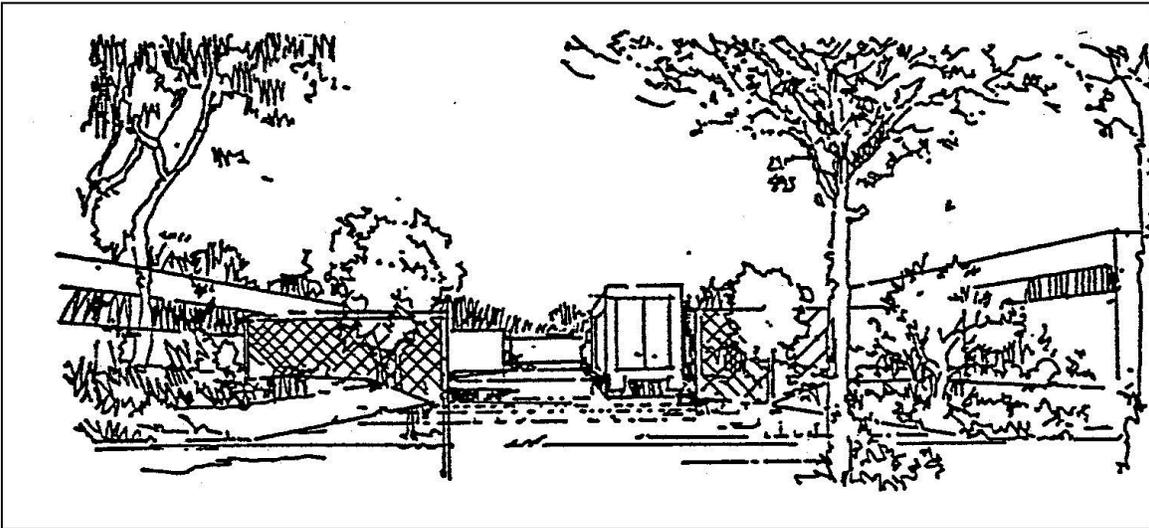
2. Side yard and rear yard setbacks, which are not adjacent to a Street, shall be a minimum of zero (0) feet for one yard and ten (10) feet for all other yards. Only one zero setbacks per parcel will be permitted.

3. Roof overhangs or architectural features may project six (6) feet into twenty (20) foot setback areas and three (3) feet into a ten (10) foot setback area.



### C. Site Requirements

Minimum site size for all parcels contemplating an industrial use shall be one (1) acre. The minimum site size for parcels contemplating commercial uses may vary, provided that each such parcel meets the on-site parking and landscaping requirements of this Specific Plan.



### D. Landscaping

The following landscaping requirements shall apply to all commercial or industrial projects within the ICDD

#### 1. Undeveloped Areas

Ten (10) feet along the street frontage of areas proposed for development in a future phase which are under the same ownership, shall be temporarily turfed, seeded, and irrigated with an automatic sprinkler system, if construction of said phase will not begin within six (6) months following completion of the previous phase.

#### 2. Off-street Parking Areas

- a. A minimum of five (5%) percent of the total off—street parking area for commercial development, not including required setbacks, shall be landscaped with at least one (1) fifteen (15) gallon tree per eight (8) parking stalls (which may be clustered or grouped) and appropriate ground cover.
- b. A minimum of four (4%) percent of the total off-street parking area for industrial development, not including required setbacks, shall be landscaped with at least one (1) fifteen (15) gallon tree per eight (8) parking stalls (which may be clustered or grouped) and appropriate ground cover.

3. Site Landscaping Each parcel shall contain one or more areas which are landscaped in compliance with this Specific Plan and which in the aggregate is not less than ten (10%) percent of the square footage of such parcel net of street rights-of-way.

4. Adjacent to Building

Trees shall be planted in areas of public view adjacent to and along the side and rear building lines, at an equivalent of one (1) fifteen (15) gallon tree per fifty (50) linear feet of building which has public exposure. Trees may be clustered or grouped.

5. Automatic Irrigation Systems

All landscaped areas shall be served by an automatic irrigation system, approved by the Planning Director that provides adequate coverage and irrigation. Efficient, state-of-the-art irrigation systems shall be used, such as drip systems, for water conservation.

6. Plant Materials

Landscape designs shall consider drought-tolerant plant materials for water conservation and shall provide full ground coverage within six (6) months following the initial planting.

7. Maintenance

Property owners are responsible for the continual maintenance of all landscaped areas on-site, as well as contiguous planted areas within the public right-of-way. All landscaped areas shall be kept free from weeds and debris, maintained in a healthy, growing condition and shall receive regular pruning, fertilizing, mowing and trimming. Unhealthy, dead or damaged plant materials shall be promptly removed and replaced in no event later than thirty (30) days following written notice from the Planning Director.

E. Off-street Parking and Loading

1. Parking

The off-street parking standards contained within the Municipal Zoning Code shall apply to all development projects and to all changes in occupancy.

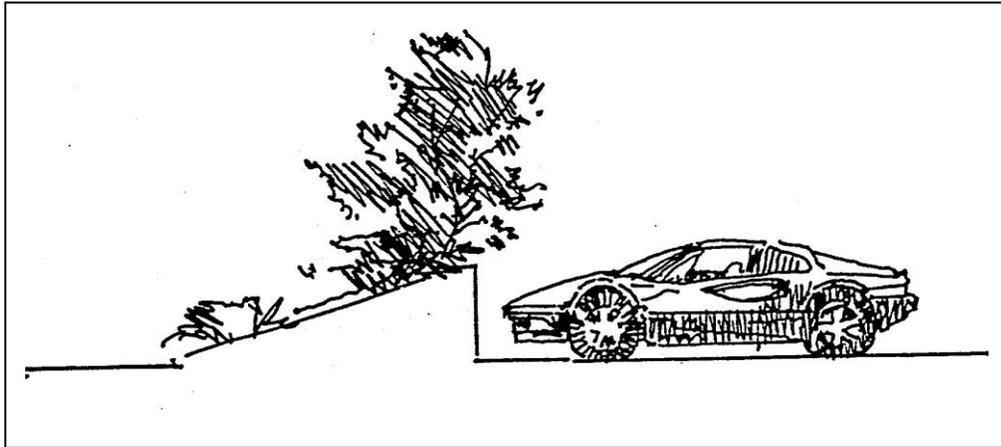
2. Truck Loading and Internal Circulation

Internal circulation patterns and all access drives shall be designed and maintained in accordance with the Corona Municipal Code standards and all vehicle movements involved in loading, parking and turning around shall occur on site.

### 3. Screening of Parking Areas.

The use of or a combination of berming, landscaping materials and walls not more than thirty-six (36) inches in height shall be required to partially screen parking areas from public streets. All walls to be used for screening purposes shall be of solid masonry construction and ornamental in texture, pattern or shadow relief and shall be used in conjunction with foreground landscaping and mounding.

Permanent Signs [by SPA 89-22]



### F. Signs

#### 1. Permanent Signs [Amended by SPA-89-22]

All permanent signs shall be regulated and controlled as specified under Section 17.74 of the Corona Municipal Code for the ICDD District properties; except for modifications as herein stated:

A. Signs may be constructed or maintained in the ICDD District along the uppermost level of any commercial office building or multi-tenant building with shared frontages having two or more levels, subject to the following conditions:

1. No combination of signs shall be constructed or maintained on more than one level of any building.
2. Signs shall contain only such subject matter which refers to the name of the establishment on the premises on which the sign is located.
3. The maximum area of an enterprise sign shall not exceed 200 square feet.
4. No more than two enterprise signs shall be located on one building frontage.

5. Signs shall not be constructed or maintained on more than three frontages.
6. No sign shall extend a lateral distance greater than 75% of any building elevation.
7. No sign constructed along the uppermost level of any building shall exceed three feet in height.
8. All signs constructed or maintained along the uppermost level of any building shall be of uniform size, alignment, and design.

## 2. Temporary Signs

For the marketing of property, temporary signs not exceeding six (6) square feet in area per sign face may be placed on each parcel. Each designated development phase may, in addition, contain one temporary sign not exceeding twenty-four (square feet in area per sign face for marketing purposes. Temporary signs may not be displayed for a period exceeding six (6) months in duration.

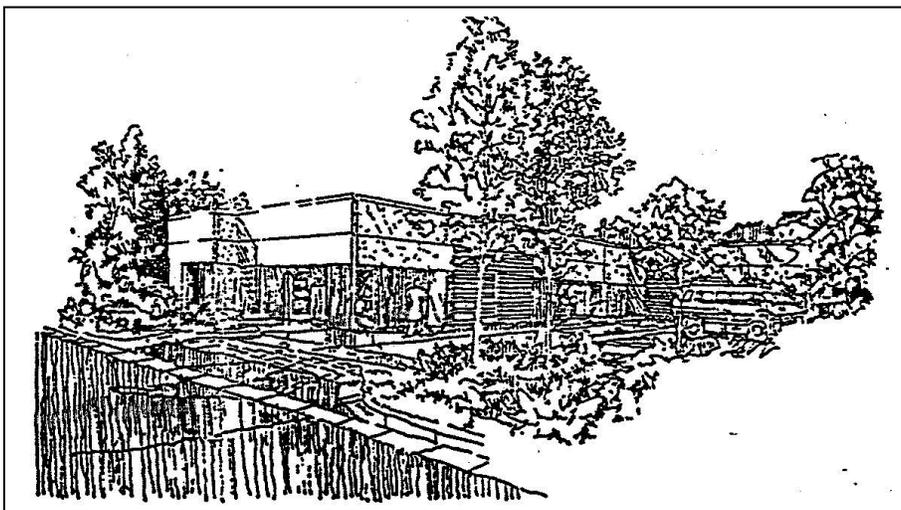
## G. Fences and Screen Walls

### 1. Height

- a. No fence or screen wall shall exceed six (6) feet in height with the exception that an additional two (2) feet in height may be approved for security purposes and an additional six (6) feet may be approved for the screening of Truck Loading Areas. All wall materials, locations and height shall be subject to the approval of the Planning Director.
- b. No walls in excess of three (3) feet shall be constructed within the setback area adjacent to a Street.

### 2. Restrictions on Materials

- a. Screen walls or fences of sheet or corrugated iron, steel, aluminum asbestos, or security chain-link fencing are specifically prohibited.



- b. Chain-link fencing is permitted when combined with redwood or plastic slats in all areas except in setback areas adjacent to a public street.

## **Section 404-General Provisions**

The following general development standards shall apply to all development within the ICDD:

### **A. Maintenance of Paved Areas and Structures**

1. All property owners shall maintain paved areas in the manner required to eliminate safety hazards, standing water, weeds, inefficient drainage patterns and deterioration of sub-base materials. Paved areas shall be impervious to water and shall be maintained in a sanitary condition free from refuse and debris. All property owners shall perform such maintenance as required by the property owners association within forty-five (45) days following written notification of any pavement maintenance deficiencies pursuant to this Section and seven (7) days following written notification of u conditions.
2. Property owners shall maintain all structures, including buildings, accessory buildings and signs, in the manner required to protect the health and safety of users, occupants and the general public. The aforementioned requirement shall consist of property owners meting the applicable standards of all governmental codes, including but not limited to, building, mechanical, electrical, fire and health codes.

### **B. Public Safety**

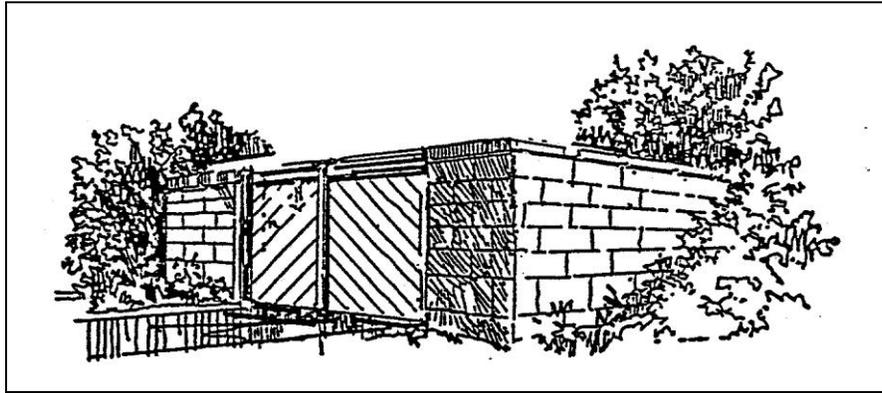
No operation in the manufacture, compounding, assembling, processing, or treatment of any product, and no material stored on any parcel within the District shall be injurious to the health, welfare, or property of persons residing or working in the ICDD or the surrounding neighborhood.

### **C. Temporary Structures**

No structure of a temporary character, trailer, camper, boat or similar vehicle or equipment shall be permitted to remain upon the exterior portion of any parcel With the exception of temporary parking of delivery trucks, service vehicles and other commercial vehicles being used in the furnishing of products or services to an owner or occupant, or the parking of vehicles belonging to or being used by such owner or occupant for loading or unloading purposes, there shall be no parking on the property other than in designated parking spaces.

### **D. Refuse Collection Areas**

All outdoor refuse collection areas shall be screened from view from adjacent parcels and from outside the property with solid materials similar to those used in the construction of the buildings on the parcel. No refuse collection area shall be permitted between a public Street and a building and all enclosures shall meet the City of Corona standards for enclosures on file in the Planning Department.

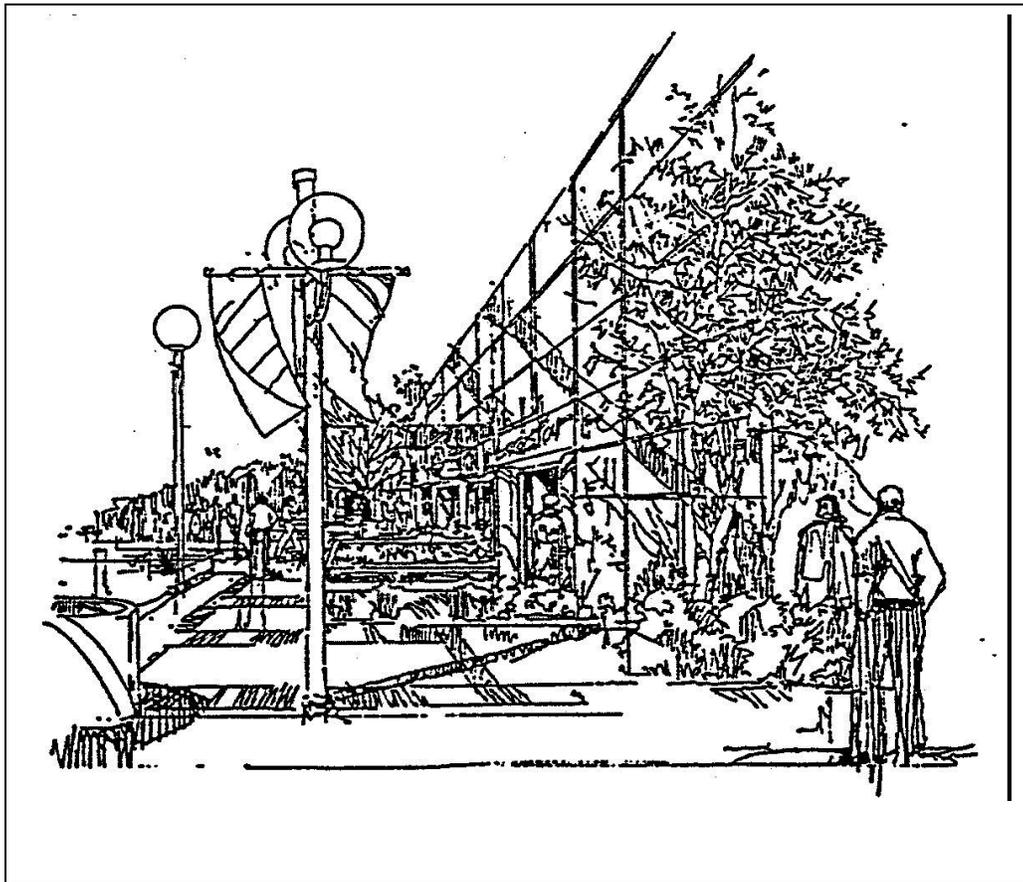


E. Performance Standards

In addition to those performance standards presented within Chapter 17.84 of the Municipal Zoning Code the following performance standards shall apply throughout the ICDD:

1. Lighting

Parking lot fixtures shall have an overall maximum height of sixteen (16) feet. Walkway lighting fixtures shall have an overall maximum height of ten (10) feet. Security lighting fixtures shall not project above the fascia or roof line of the building and shall be shielded. The shields shall be painted to match the surface to which attached.



2. Noise

Areas used for primary circulation, for frequent idling of vehicle engines or for loading activities shall be designed and located to minimize impact on adjoining properties, including provisions for screening or sound attenuation.

3. Mechanical and Electrical Equipment

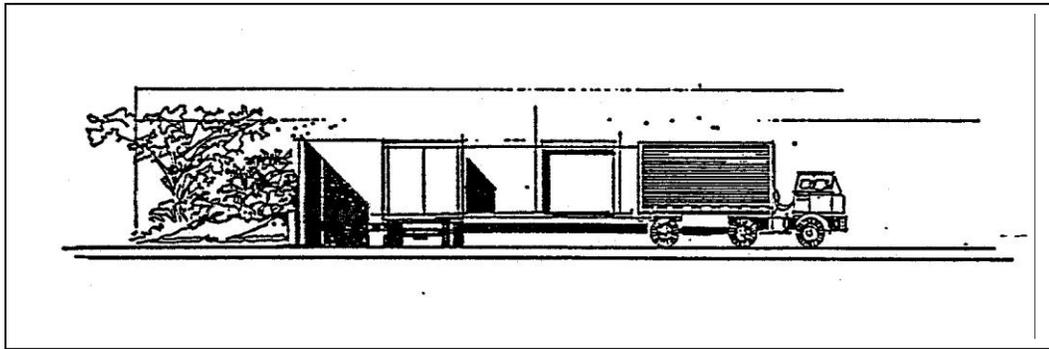
All such equipment, including air conditioners, antennas, pumps, transformers, heating and ventilating equipment, shall be screened from public view.

4. Outdoor Storage

All outdoor storage permitted in conjunction with a Conditional Use Permit shall be visually screened from adjacent parcels and from view from outside property. Such screening shall be opaque to a point at least six (6) feet in vertical height.

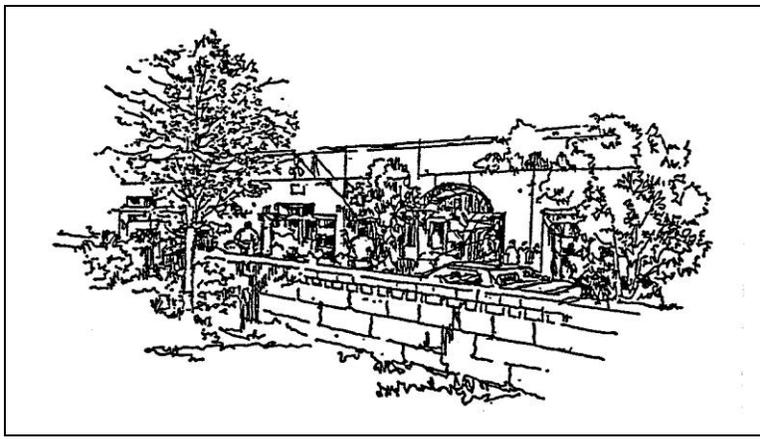
F. Screening of Service Yards

All exterior service yards, electrical cage enclosures and storage tanks shall be screened from view from streets, and adjacent parcels by a fence, wall or mature landscape materials.



G. Separation Between Uses

The Planning Director may require solid masonry walls not to exceed six (6) feet in height between adjoining commercial or industrial properties or along the rear or side property lines, when due to the nature of the industrial/commercial activities it is appropriate to construct walls for screening or sound attenuation purposes..



### **Section 405-Other Governmental Codes**

All Development within the ICDD must comply with all applicable governmental codes. When there is a conflict between this Specific Plan and the Municipal Zoning Code, the provisions of this Specific Plan shall take precedence.

### **Section 406-Design Review Requirements**

All project within this District involving the construction or rehabilitation of property are required to be in compliance with the Design Guidelines (Section 407) and the provisions contained with Chapter 5 entitled “Design Review Process

### **Section 407-Design Guidelines**

#### **A. Intent**

The Design Guidelines are intended to encourage the orderly and harmonious design of structures, landscaping, parking area, lighting and accessory structures.

#### **B. Site Planning**

1. When locating buildings, parking and walkways on a parcel, all desirable site features (freeway frontage, corner lot, irregular lot) should be used to an advantage.
2. Relate the location of site uses with adjoining properties to avoid possible conflicts between adjoining uses.
3. `Consideration should be given to the compatibility of proposed uses with existing adjacent uses. Thus it would be inappropriate to locate, for example, truck loading ramps adjacent to neighboring retail-commercial areas.
4. Consider setbacks of adjacent properties. While articulation of building edges adds interest to the streetscape, the street should function as a whole and the setbacks should be related.

5. Access to loading and service bays should be separated from employee/customer parking where feasible.
6. Consider screening truck loading docks and ramps from view of public streets by orienting docks and ramps to the rear or side of industrial buildings, or other alternative methods.
7. Parking of any trucks or service vehicles for more than four (4) hours per day shall be confined within screened storage areas. No storage shall be permitted between a frontage Street and a building.

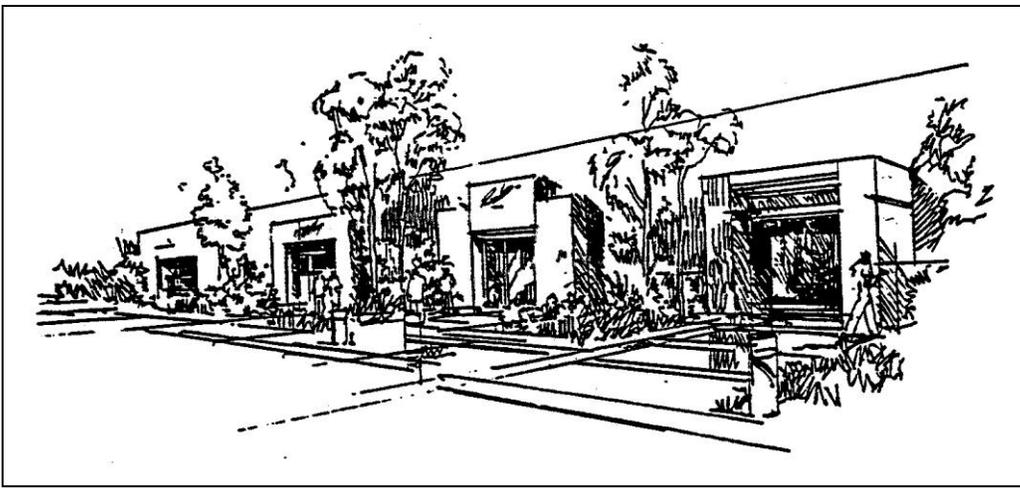
C. Building and Wall Design

1. Color

Colors, materials, and finishes are to be coordinated on all exterior elevations of the buildings to achieve total c of design.

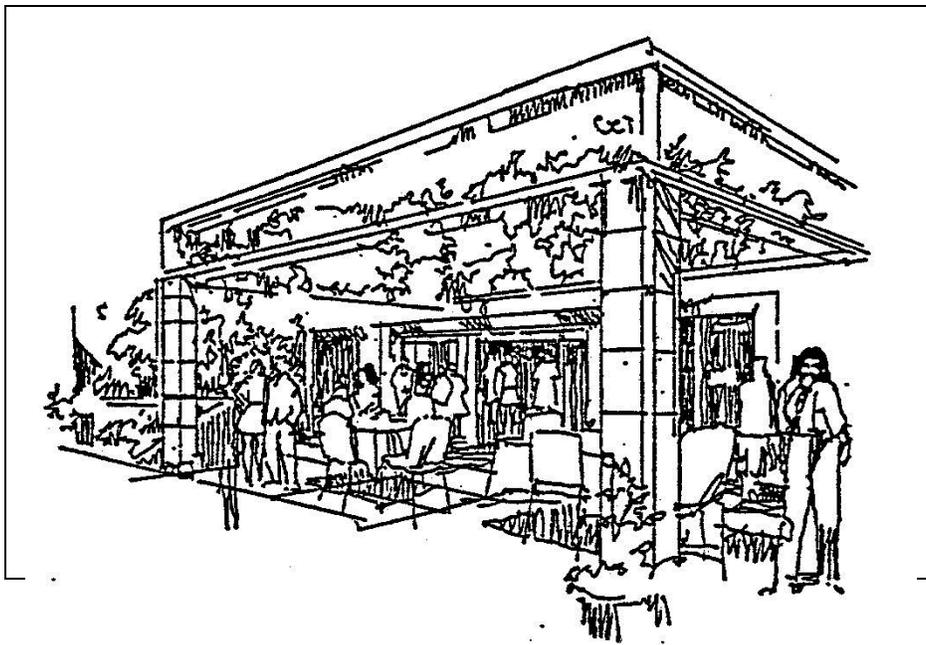
2. Mechanical Equipment and Ductwork

- a. All roof-mounted and mechanical equipment and/or ductwork, which projects vertically more than one and one-half (1-1/2) feet above the roof or roof parapet shall be screened by an enclosure which is designed consistent with the building. Screening shall be set back twenty (20) feet from an exterior building edge and shall project above the requirement which it is designed to shield from view.
- b. All roof-mounted mechanical equipment and/or ductwork which are not screened shall be painted consistent with the color scheme of the building.
- c. No mechanical equipment shall be exposed on the wall surface of a building.
- d. Cyclone blowers shall be located below the fascia and/or roof line of the building and be located on the rear or hidden side of the building. In addition, they shall be screened by a wall, a fence or landscape materials.
- e. Roof-mounted ventilators shall be a maximum of one and one half (1-1/2) feet above the point to which attached shall be painted or prefinished consistent with the color scheme of the building.
- f. Vents, louvers, exposed flashing, tanks, stacks, overhead doors, rolling and man service doors shall be painted consistent with the color scheme of the building.



### 3. Exterior Design

- a. No part of the roof may project above the parapet.
- b. All exterior wall elevations of buildings facing streets and the freeway are to have architectural treatment. Designs incorporating multiple building materials are recommended (i.e. concrete and glass), and articulation of building surfaces, variations in building height, and use of shadow or color patterns are encouraged.

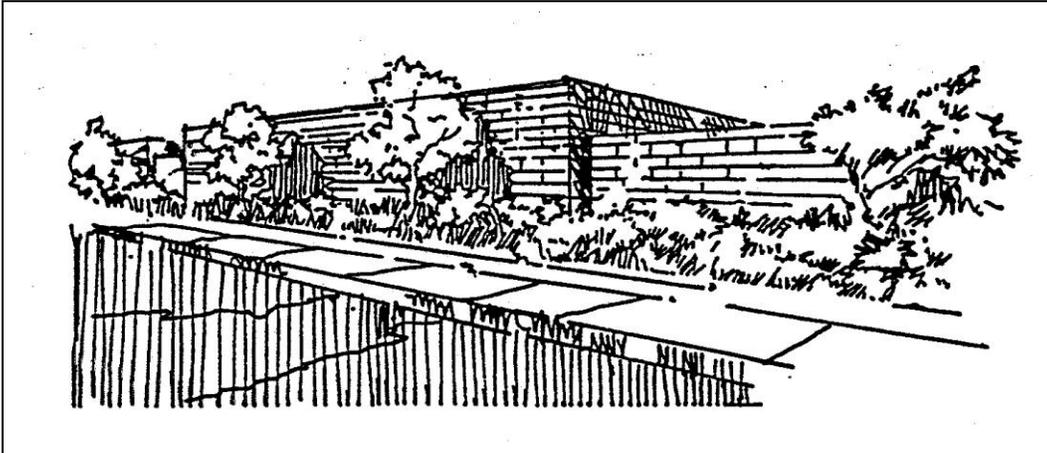


- c. Auxiliary structures should be architecturally compatible with primary structures.
- d. No metal buildings shall be permitted.
- e. No concrete bumpers shall be permitted in parking areas other than continuous curbing to protect pedestrian ways, adjoining property, and landscaping.

- f. All sprinkler pipes shall enter buildings at the lowest possible point and shall be screened with landscaping.
- g. All changes to the exterior of any of the buildings or yards are subject to the provisions of the Design Review Process.
- h. No television, radio or other electronic antenna or device of any type shall be erected or permitted to remain on the property without the prior written approval of the Planning Commission or Planning Director.

#### 4. Wall Design

- a. All masonry walls shall be of an ornamental design in texture, shadow pattern or off-set when visible from public streets.
- b. Screen walls facing a street shall be designed to appear to be an integral part of the building using the same materials. Said screen wall shall be located to the rear of the front yard setback.



#### D. Signs

- 1. Signs and graphics shall be of the finest available materials and design. The design, materials, location and placement of all signs and graphics shall be approved through the Design Review Process prior to installation.
- 2. Identification signs should be either of a monument design or wall mounted and should be in proportion to the site and improvements.

E. Landscaping Design Guidelines

All landscape design shall comply with the following guidelines:

1. Landscape elements shall be of the “long-lived” variety. “Short-lived” materials may be utilized, but only as a supplement to “long-lived” elements.
2. Landscape elements shall relate to architectural design elements. Landscape materials are considered to be a strong unifying element and, therefore, should reflect the physical, functional and aesthetic qualities of the Site.
3. Simple combinations of plant materials in simple compositions are recommended to achieve a park-like character.
4. Extensive horizontal and vertical building or wall surfaces comprised of singular materials shall be modulated or interrupted by foliage masses.
5. Trees, either lines and/or masses, shall be utilized to distinguish exterior spaces within an individual site such as separating parking from loading areas and identifying entry and pedestrian areas.

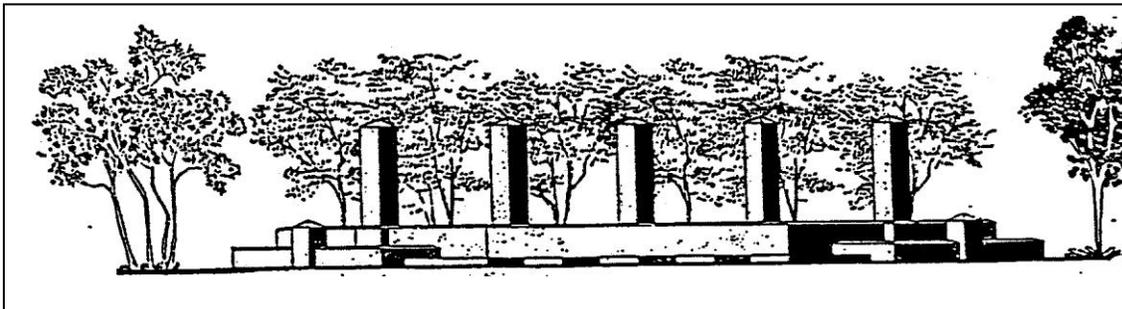
F. Plant Palette

- |  |  |
|--|--|
| 1. Street trees                        |  |
| a. Serfas Club Drive                   | Liriodendron Tulipifera<br>(2) 24” box @ 40’ o.c.                    |
| b. Pomona-Rincon Road and Wardlow Road | Brachychiton populneum<br>(2) 24” box @ 40’ o.c.                     |
| c. Bonnie Place and Bonnie Court       | Tristania Conferta (4) 15<br>gallon per 1000 SF random<br>spacing    |
| 2. Parcel entry driveway trees         | Prunus pissardi (3) 24” box<br>along each side of the drive          |
| 3. Intersection trees (corner parcels) | Prunus pissardi (5) 24” box  |
| 4. On-site trees                       |  |
| a. Parking lot                         | Cupania anacardiodes or<br>Podocarpus gracilior 15<br>gallon minimum |
| b. Property lines                      | Eucalyptus sideroxylon<br>‘Rosea’ 15 gallon minimum                  |

5. Screening shrubs
    - a. Parking at street
    - b. Trash enclosures, transformers, storage or loading areas
- Photinia fraseri 5 gal  
 @ 30" o.c.  
 Ligustrum lexanum, 5 gal.  
 @ 30' o.c.  
 Nerium oleander, 5 gal. @  
 4' o.c.  
 Pittosporum tobira, 5 gal.  
 @ 4' o.c.  
 Viburnum suspensum, 5  
 gal. @ 4' o.c.  
 Xylosma congestum, 5 gal.  
 @ 4' o.c.
6. Shrubs, general application
    - Agapanthus africanus
    - Hebe spp.
    - Hemerocallis spp.
    - Raphiolepis indica
    - Pittosporum tobira 'Wheeleri'
    - Pittosporum tobira 'Variegata'
    - Pyracantha 'Santa Cruz'
    - Carissa grandiflora
    - Trachelospermum jasminoides
    - Moraea iroides
    - Nerium oleander 'Dwarf'
    - All screening shrubs
  7. Ground Covers
    - Tall fescues, hydroseeded
    - Hedera helix
    - Gazania 'Mitsuwa Yellow'
    - Lonicera japonica 'Halls'
    - Fragaria chilonensis
    - Annual Color

G. Landscape Cross Sections

The required street, driveway, parkway and intersection planting diagrams and cross sections are attached hereto and incorporated herein as Addendum No. 1.



## **CHAPTER 5: DESIGN REVIEW PROCESS**

### **Section 500-Purpose**

This Section establishes a design review procedure for all development proposals within the Corona Westgate Specific Plan area to ensure that development complies with the guidelines and development standards. This Section is not intended to restrict imagination, innovation or variety, but rather to focus on design principles which can result in creative and imaginative solutions for the project and a quality design for the City. It is therefore, the purpose of this Section to:

- A. Encourage the orderly and harmonious appearance of all structures within the Corona Westgate Specific Plan with associated facilities, such as signs, landscaping, parking areas, and the street and compliance with the Specific Plan Design Guidelines.
- B. Assist private developers to be more aware of Corona's concern for the aesthetics of development.
- C. To ensure that new developments minimize adverse impacts upon existing adjoining properties.
- D. Preserve and enhance the particular character and unique assets existing within the Specific Plan area.
- E. Avoid future maintenance problems and to protect the health and safety of all Corona residents.
- F. Ensure that proposed projects are in compliance with the intent of the Development Standards and Design Guidelines contained herein.

### **Section 501-Development Project Reviews**

#### **A. Director Reviews-Minor Projects**

The Planning Director shall review all development projects involving the construction of commercial buildings or additions of less than one thousand (1,000) square feet within the Corona Westgate Specific Plan area. This review shall include building additions, facade and roofing alterations, parking lots of less than twenty-five (25) spaces, and any other projects significantly influencing, in the Director's judgment, the design or development of proposed sites, buildings or signs. Notwithstanding the foregoing requirements, the Director is authorized to require that a minor project be subject to the Planning Commission review and determination procedure rather than the provisions of this subsection.

#### **B. Planning Commission Reviews**

The Planning Commission shall review all development projects within the Corona Westgate Specific Plan area with the exception of the minor projects referenced in subsection (A) above.

## **Section 502-Administration**

### **A. Application**

Applications for design review shall be filed by the owner or owners of the property for which the determination is sought or his authorized agent. Application shall be made to the Department of Planning. The application shall be accompanied by information specified in Paragraph (3) below and a filing fee shall be paid for the purpose of defraying the costs incidental to the proceedings. Appropriate application and appeal fees shall be determined by City Council resolution.

### **B. Staff Investigation**

The Planning Director shall make an investigation into the facts bearing on the case to provide the information necessary for a determination regarding consistency) with the intent of this Specific Plan and the General Plan, and shall report its findings and recommendation to the Planning Commission.

### **C. Information Requirements**

The following project data shall be submitted in conjunction with the Design Review Process:

1. A legal description of the area proposed for development, including a statement of present and proposed ownership.
2. A development schedule indicating the approximate date when construction can be expected to begin and be completed.
3. Project statistics including, but not limited to: project area, proposed lots, dimensions and square footage, and other related information as specified by the Planning Director.
4. Site plan(s) and supporting displays drawn to scale, fully dimensioned, easily readable, and containing the following data:
  - a. Title block (applicant's name and date drawn).
  - b. Scale and north arrow.
  - c. Property lines or building sites, dimensioned.
  - d. Existing use of property and site conditions.
  - e. Location, acreage, and proposed type of use for each building site.
  - f. The location and floor size of all existing and proposed buildings, structures and improvements within the building site.

- g. Sufficient information on land areas adjacent to the site to indicate the relationships between the proposed development and existing and proposed adjacent areas (both within and outside of the Specific Plan area), including land uses, zoning classifications, densities, circulation systems, public facilities, and unique natural features of the landscape.
  - h. The existing and proposed pedestrian circulation system and dimensions, including its interrelationship with the vehicular circulation system, indicating points of conflict and proposed treatments.
  - i. The existing and proposed circulation system, including existing and proposed improvements to off-street parking areas, service areas, loading areas, major points of access to public rights-of-way (including major points of ingress and egress to the development) and dimensions.
  - j. The existing and proposed on-site utility systems, including sanitary sewers, storm sewers, and water, electrical, gas, and telephone lines and cable systems and their connections to off-site systems.
  - k. Location and size of all areas to be conveyed dedicated or reserved for public or semi-public use.
  - l. Location, heights, dimensions, materials and copy, if available, of all signs.
  - m. The location, height and materials of all fencing and walls.
  - n. Landscaping and screening areas.
  - o. Any additional background and supporting information as the Planning Director deems necessary including required environmental documentation pursuant to the California Environmental Quality Act (CEQA).
5. Preliminary landscape plans, drawn to scale and including the following information:
- a. Areas identified as visually sensitive as part of the Specific Plan, yards and open space areas, and any special elements of the site per architectural guidelines and requirements of the Specific Plan.
  - b. Proposed location of all plant materials, by common and botanical name.
  - c. Proposed size and quantities of plant materials.
  - d. Any additional information as the Director deems necessary.
6. Architectural elevations of all structures (including walls and signs), including, but not limited to the following:

- a. All exterior materials.
- b. All exterior colors.
- c. Building height and mass.
- d. Any additional information as the Director deems necessary.

## **Section 503-Authority**

### **A. Action**

The Planning Commission and Director are authorized to approve or deny projects and impose reasonable conditions upon such approval, subject to the right of appeal. Conditions may include, but shall not be limited to: requirements pertaining to open spaces, site coverage, architectural features, screening and buffering of adjacent properties, fences, a walls; requirements for installation and maintenance of landscaping and erosion control measures; requirements for Street improvements and dedications, regulation of vehicular ingress and egress, and traffic circulation; regulation of signs; regulation of hours or other characteristics, of operation; requirement for maintenance of landscaping and other improvements; requirements for development schedules or time limits for performance or completion; and such other conditions as the Commission or Director may deem necessary to ensure compatibility with surrounding uses, to preserve public health, safety, and welfare, and to enable the Commission or Director to make the appropriate findings.

The approval of the Planning Commission or Director will be based upon the project conforming to the following findings:

1. The design and layout of the proposed development is consistent with the intent of the Corona Westgate Specific Plan and the design guidelines and development standards of the appropriate development district.
2. The design and layout of the proposed development will not unreasonably interfere with the use and enjoyment of neighboring existing or future developments, and will not create traffic or pedestrian hazards.
3. The architectural design of the proposed development is compatible with the character of the surrounding neighborhood and will maintain the harmonious, orderly and attractive development contemplated by the' Corona Westgate Specific Plan.
4. The design of the proposed development will provide a desirable environment for its occupants, its neighbors and the visiting public through the use of materials, texture and color that will remain aesthetically appealing and will retain a reasonably adequate level of maintenance and service.
5. The design of the proposed development complies with applicable covenants, conditions and restrictions (CC&R's).

## B. Process and Appeal Procedures

### 1. Minor Project Process and Appeal Procedures

The Director shall provide project applicants a written decision approving, denying or conditionally approving a proposed project following submission of a complete application pursuant to this Chapter. The Director's decision shall contain the appropriate findings and reasons for his/her decision. The Director shall maintain a copy of the decision on file in the Planning Department.

The Director's decision shall be final and shall become effective within ten (10) days following the date of his/her decision. Provided, however, that if within such ten (10) day period an aggrieved party files an appeal, the Director's decision shall be suspended until the appeal is acted upon by the Planning Commission or dismissed by the appellant.

### 2. Planning Commission Process and Appeal Procedures

The Planning Commission may approve, conditionally approve, or disapprove an application (or an appeal relating to a minor project) and shall announce and record its decision following submission of a complete application (consistent with CEQA provisions). The decision shall set forth the findings in a formal resolution of the Commission and shall be filed with the City Clerk. A copy of the resolution shall be mailed to the applicant.

The decision of the Planning Commission shall be final and shall become effective ten (10) days after the adoption of the resolution by the Commission. Provided, however, that if within such 10-day period an appeal of the decision is filed by an aggrieved person, the applicant or the City Council, the filing of such an appeal, with fee, within such a time limit shall suspend the decision of the Planning Commission until a determination of the appeal by the City Council or its dismissal by the appellant. • Such appeal shall be filed in writing with the City Clerk on forms furnished by said clerk. Appeal procedures shall be as follows:

- a. The hearing date shall be set by the City Clerk after the filing of the appeal on the forms provided.
- b. The Planning Commission secretary shall transmit to the City Council the original application, records, written reports, and Planning Commission resolution disclosing in what respect the application and' facts offered in support thereof met or failed to meet the requirements set forth in this Section.

## **CHAPTER 6: ENFORCEMENT**

### **Section 600-Enforcement**

- A. The Planning Director shall have the duty to enforce the provisions of this Specific Plan.
- B. Any use of a building or structure hereafter erected, built, maintained or used contrary to provisions of the Specific Plan shall be a public nuisance.
- C. Any person violating any provisions of this Specific Plan is guilty of an infraction.
- D. Any details or issues not specifically covered by this Specific Plan shall be subject to the regulations of the City of Corona Municipal Code.
- E. If any regulation, condition, program or portion thereof of the Specific Plan is for any reason held invalid or unconstitutional. by any court of competent jurisdiction, such portion shall be deemed a separate, distinct and independent provision and the invalidity of such provision shall not affect the validity of the remaining provisions hereof.

### **Section 601-Relationship to the Zoning Code**

This Specific Plan augments the development regulations and standards of the Corona Municipal Zoning Code. When an issue, condition or situation occurs which is not covered or provided for in this Specific Plan, the regulations of the Zoning Code that are most applicable to the issue, condition or situation shall apply. In the event that the provisions of the Specific Plan are in conflict with the Zoning Code, the provisions of the Specific Plan shall prevail.

### **Section 602-Interpretation**

The Planning Director shall have the responsibility to interpret the provisions of the Specific Plan. All such interpretations shall be reduced to written form and permanently maintained. Any person aggrieved by such an interpretation may request that such interpretation be reviewed by the Planning Commission.

### **Section 603-Compliance with Governmental Codes**

All development and construction within the Specific Plan area shall comply with the applicable codes of all governmental agencies having jurisdictions in such matters including, but not limited to, building, mechanical, fire and electrical codes, codes pertaining to drainage, waste water, public utilities, subdivisions and grading.

## **Section 604-Definitions**

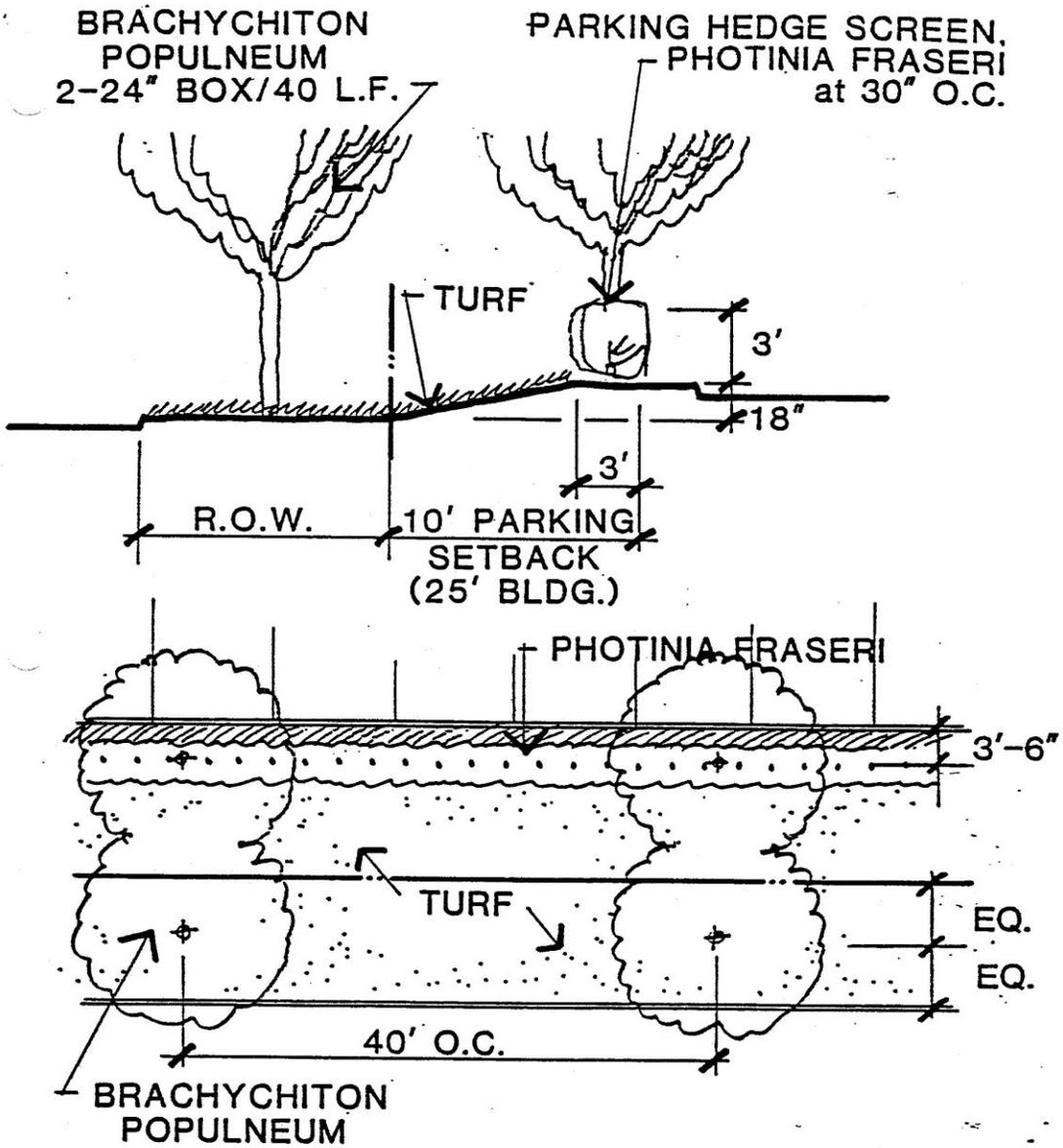
Words, phrases and terms not specifically defined herein shall have the same definition as provided in the Zoning Code.

### A. Definitions

1. Toxic Substances - refers to those substances, in the judgment of the City of Corona or other applicable governmental agencies, which may limit the use and enjoyment of adjacent parcels, due to hazardous or threatening conditions to the health, safety and welfare of property occupants.
2. Commission - means the Planning Commission of the City of Corona.
3. Director - shall mean the designated Planning Director of the City of Corona or his/her designee.
4. Municipal Zoning Code - shall mean Title 17 of the Corona Municipal Code.

## **ADDENDUM 1**

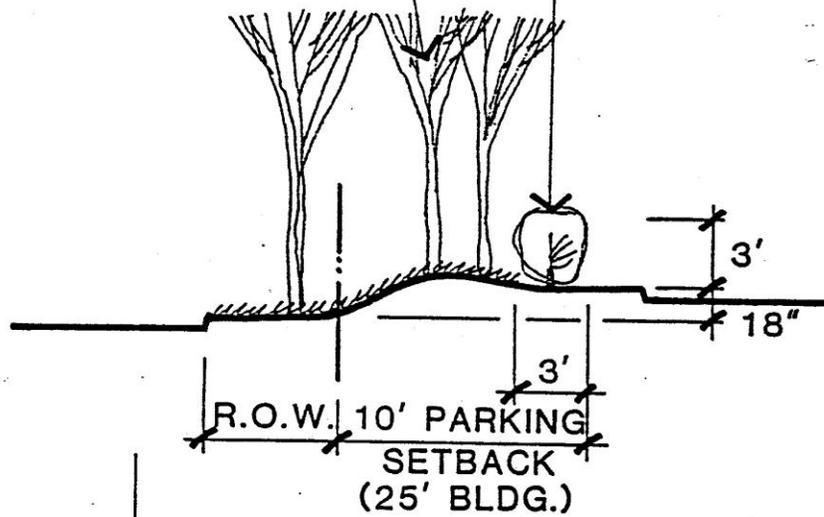
Street, Driveway, Parkway and Intersection planting diagrams and cross sections.



WARDLOW RD.  
POMONA-RINCON RD.  
RESEARCH DR.

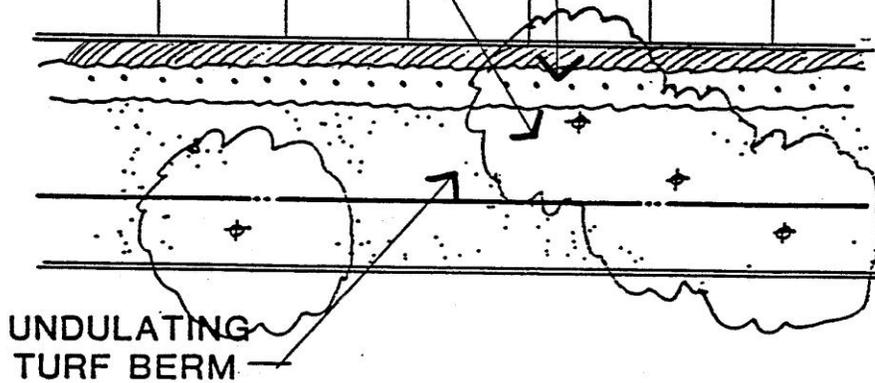
EUCALYPTUS RUDIS,  
4-15 gal/1000 S.F.  
RANDOM SPACING

PARKING HEDGE SCREEN,  
PHOTINIA FRASERI  
at 30" O.C.

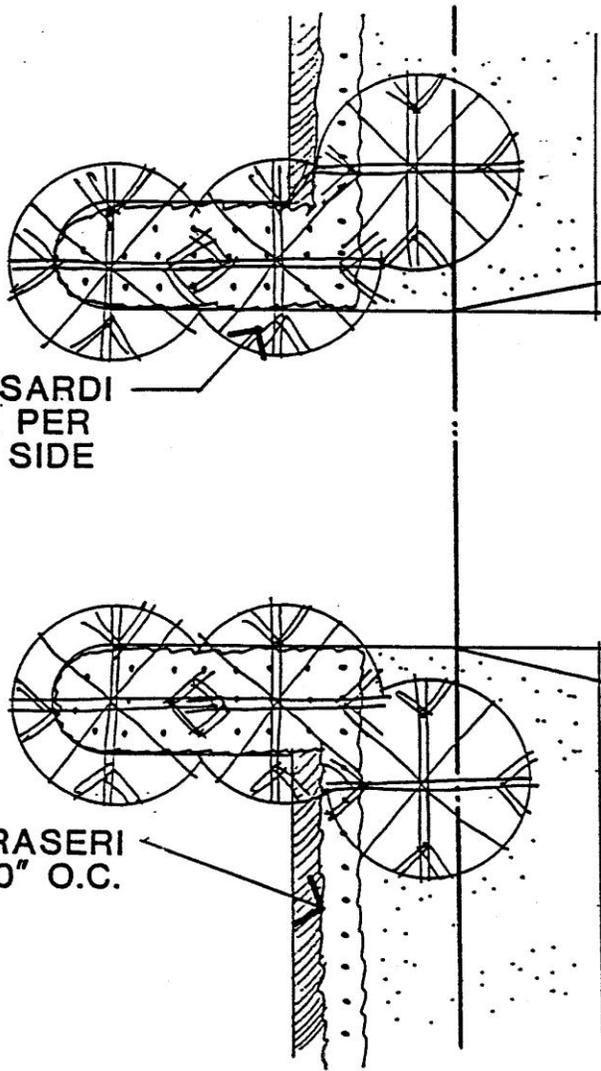


EUCALYPTUS RUDIS

PHOTINIA FRASERI



BONNIE PLACE  
BONNIE COURT

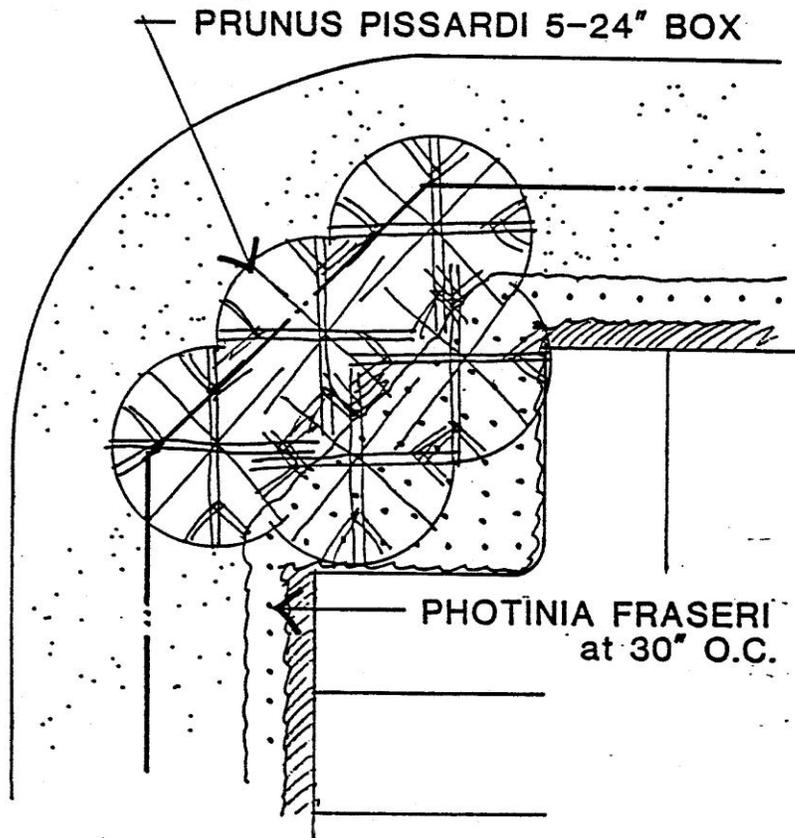


PRUNUS PISSARDI  
3-24" BOX PER  
DRIVEWAY SIDE

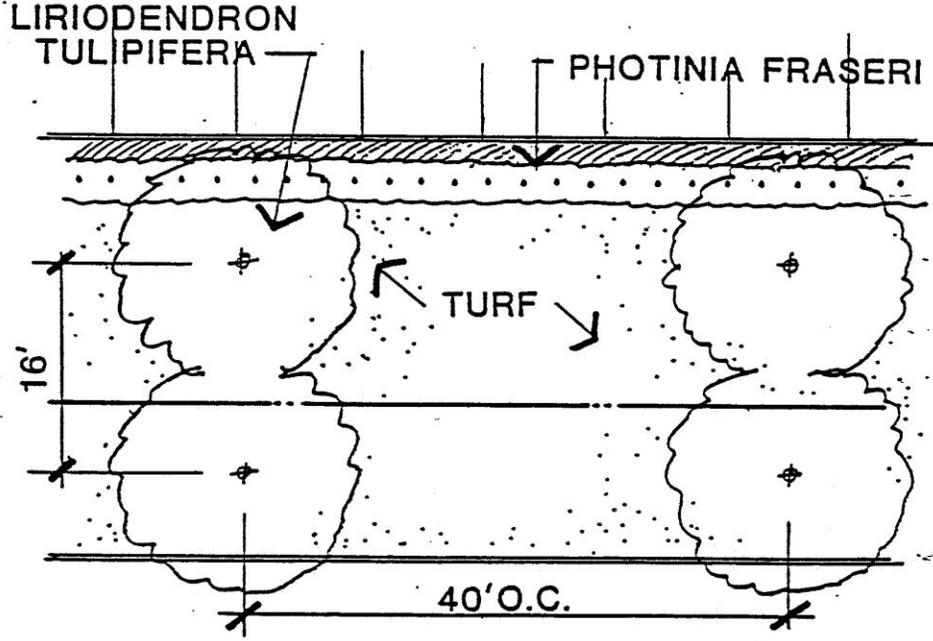
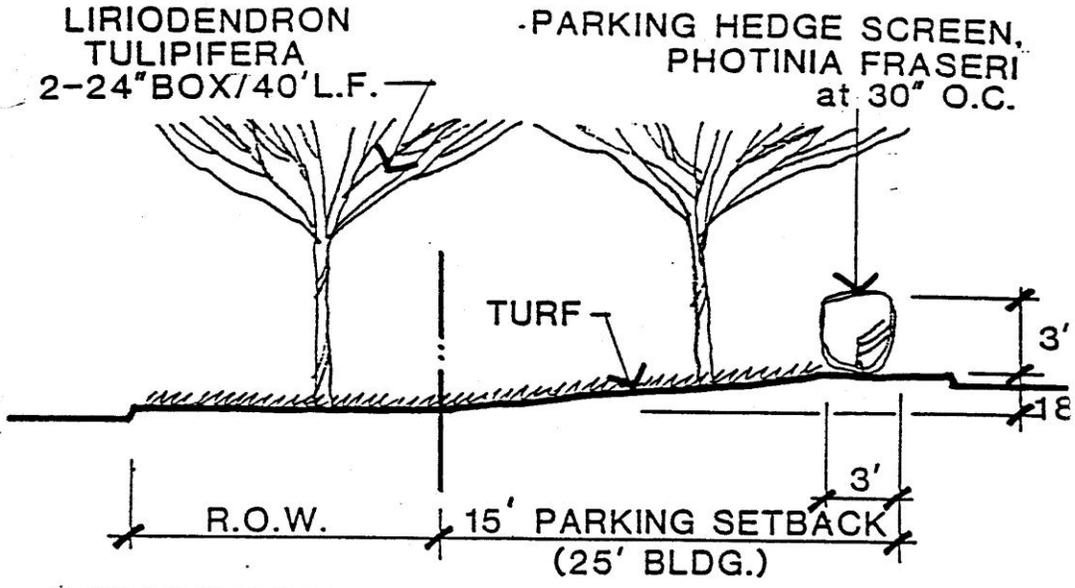
The diagram shows a plan view of a driveway with two rows of trees. The top row consists of three circular trees, each with a cross-section showing a central trunk and branching structure. A line points from the text label to the first tree in this row. The bottom row consists of three circular trees, also with cross-sections. A line points from the text label to the first tree in this row. A vertical line represents the driveway, and a horizontal line represents the driveway side. The trees are arranged in a staggered pattern along the driveway side.

PHOTINIA FRASERI  
at 30" O.C.

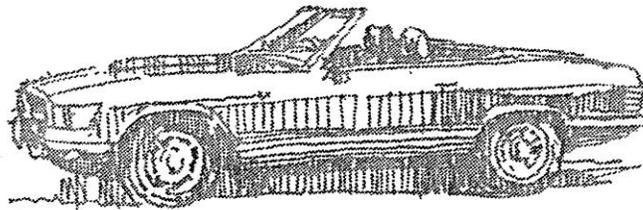
ENTRY DRIVE  
TREES



INTERSECTION  
TREES



SERFAS CLUB DR.



# CORONA WESTGATE SPECIFIC PLAN

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CORONA REDEVELOPMENT AGENCY

GRC