

**MOUNTAIN GATE
SPECIFIC PLAN
(SP-89-01)**

**FOR
CITY OF CORONA**

Submitted by

LYON COMMUNITIES, INC.

**Approved by the Corona City Council
June 7, 1989**

**LAST AMENDED: MAY 4, 2011
By SPA 08-005**

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Approved by the Corona City Council:	June 7, 1989
Amended by Specific Plan Amendment #89-13:	November 15, 1989
Amended by Specific Plan Amendment #90-09:	February 20, 1991
Amended by Specific Plan Amendment #93-01:	April 7, 1993
Amended by Specific Plan Amendment #93-05:	December 1, 1993
Amended by Specific Plan Amendment #94-04:	May 17, 1995
Amended by Specific Plan Amendment #97-05:	July 15, 1998
Amended by Specific Plan Amendment #99-04:	October 20, 1999
Amended by Specific Plan Amendment #01-001:	February 21, 2001
Amended by Specific Plan Amendment #01-008:	October 2, 2002
Amended by Specific Plan Amendment #02-005:	November 20, 2002
Amended by Specific Plan Amendment #05-006:	September 21, 2005
Amended by Specific Plan Amendment #06-011:	January 16, 2008
Amended by Specific Plan Amendment #08-005:	May 4, 2011

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AMENDMENTS TO THE SPECIFIC PLAN

CASE NO.	COUNCIL APPROVAL	DESCRIPTION/APPLICANT
SPA-89-13	11/15/89	Amendment of the boundaries of the plan to include 10.0 acres designated as Quasi-Public. (Applicant: Joseph Santeria).
SPA-90-09	02/20/91	Amendment adding single family condominium standards (Section 9.0), establishing a master signage program (Section 9.0), amending the land use plan fire station and intermediate school sites. Establishing SFA Designation for PA-6B; revising text in Section 6.0 (Circulation), Section 10.0 (Community Design Criteria), Section 11.0 (Implementation Plan) and Section 12.0 (Subdivision Standards). (Applicant: Lyon Communities).
SPA-93-01	04/07/93	Amendment to Section 9.3 – Commercial Development Standards – Permitted and Conditional Uses as a permitted use and churches as a conditional use.
SPA-93-05	12/01/93	Amendment to the text and land use plan. (Applicant: Foothill Properties).
SPA-94-04	05/17/95	Amendment to change the location and size of the fire station site and associated tables and text. (Applicant: City of Corona).
SPA-97-05	07/15/98	Amendment to provide for an increase in allowable sign area height, and to eliminate lot coverage requirements. (Applicant: Hughes Investments).
SPA-99-04	10/20/99	Amendment to establish Corona Heritage Park and associated development standards within the Parks Land Use District. (Applicant: City of Corona).
SPA-01-001	02/21/01	Amendment to delete church interim use as a permitted use in the commercial zone and to add a minor conditional use permit for churches not exceeding 10,000 square feet.
SPA-01-008	10/02/02	Amendment to establish two new land use categories (SFD-9.6 and Open Space) and add 120.2 acres (238 maximum allowable dwelling units) to the plan area within these designations; and to add a modified section of Foothill Parkway west of Lincoln Avenue.
SPA-02-005	11/20/02	Amendment to add 66.1 acres as a new Planning Area 33, with a designation of ER-2 (Estate Residential-20,000 s.f. min. lot size); and to modify various development standards pertaining to Planning Area 33.
SPA-05-006	9/21/05	Amendment to add 7.4 acres into PA 5A under the Estate Cluster Residential designation.

AMENDMENTS TO THE SPECIFIC PLAN

CASE NO.	COUNCIL APPROVAL	DESCRIPTION/APPLICANT
SPA-06-011	01/16/08	Amendment to add 75.0 acres as a new Planning Area 34, with a new designation of SFD-14.4 (Single-Family Detached – 14,400 s.f. min. Lot sizes)
SPA-08-005	05/04/11	Amendment to add 25.5 acres into Planning Area 26 under the Estate Cluster Residential designation (Hillside Overlay Zone).

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1.0 INTRODUCTION

The Mountain Gate (formerly known as “Foothill Ranch”) Specific Plan provides the City of Corona with a set of plans, regulations and guidelines to create a quality planned residential community through the orderly development of the site. The Specific Plan envisions the creation of a higher-end, amenity-oriented, family-oriented community that will provide a variety of housing opportunities for the residents of Corona. The plan proposes to link together the residential neighborhoods with a system of parks, greenbelts and an elementary school. With boulevard streetscapes and enhanced parkways along the main roads, these amenities will make Mountain Gate a uniquely attractive new community in south Corona.

Through innovative land planning and community design, it is anticipated that Mountain Gate will set a course toward achieving the City’s goals and objectives for south Corona by encouraging other well planned developments within the surrounding areas.

1.1 Location

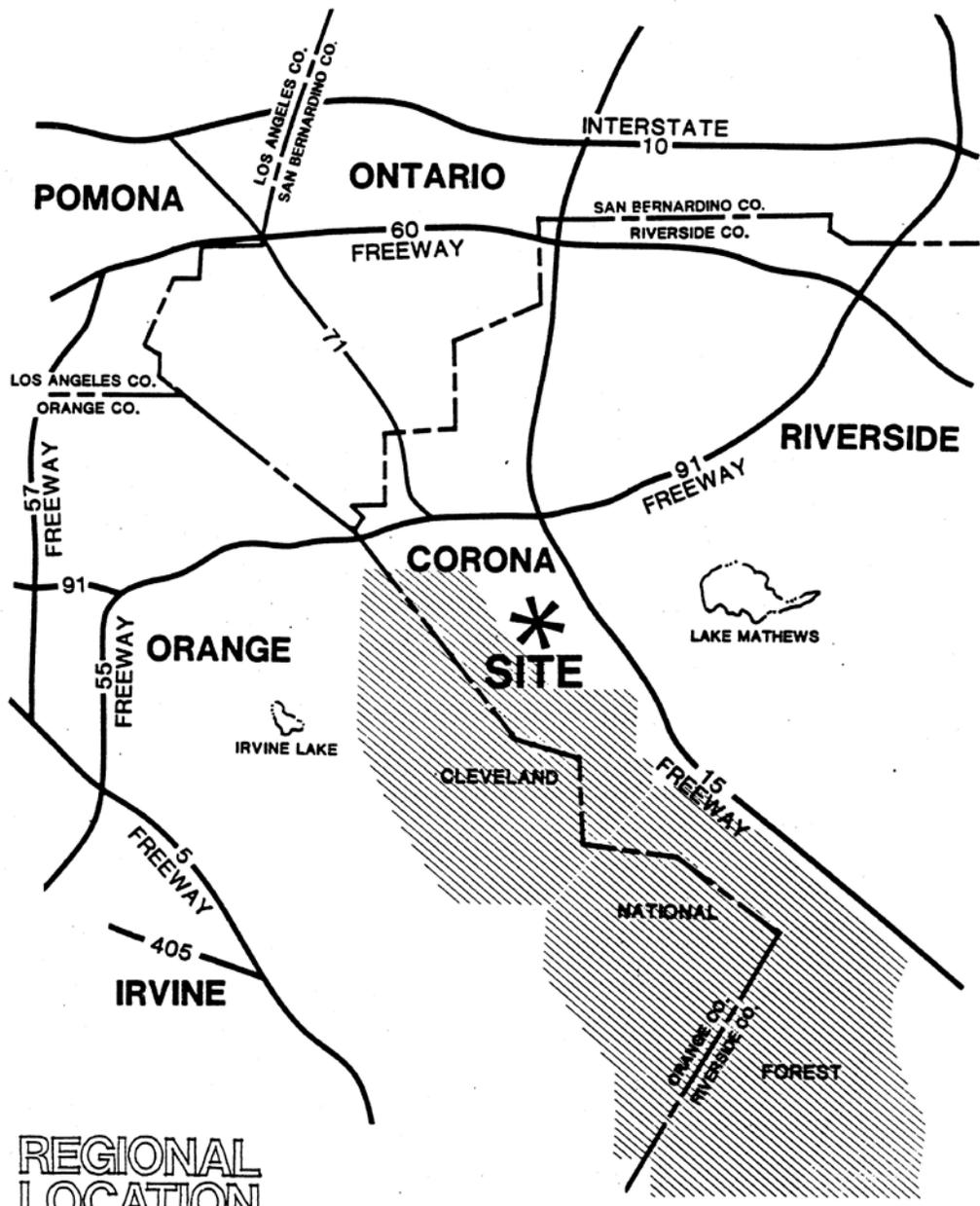
Mountain Gate includes approximately 1,205.4 acres of land located at the base of the Santa Ana mountains in the south part of Corona. Exhibits 1.1 and 1.2 show the location of Mountain Gate. The major roads providing access to the site are Main Street and Lincoln Avenue. The site is located to the south of Chase Drive and west of Garretson Avenue. A portion of the site also extends east of Garretson Avenue between Upper Drive and Cleveland Way (formerly Foothill Drive), and south of Cleveland Way, west of Fullerton Avenue. The Main Street channel and debris basin, owned by the Riverside County Flood Control and Water Conservation District, are located on the eastern part of the site, between Main Street and Garretson Avenue. Cleveland National Forest borders the site to the west.

1.2 Authority and Scope

The California Government Code authorizes cities to adopt specific plans by resolution as policy or by ordinance as regulation. Hearings are required by both the Planning Commission and City Council after which the Specific Plan must be adopted by the City Council to be in effect.

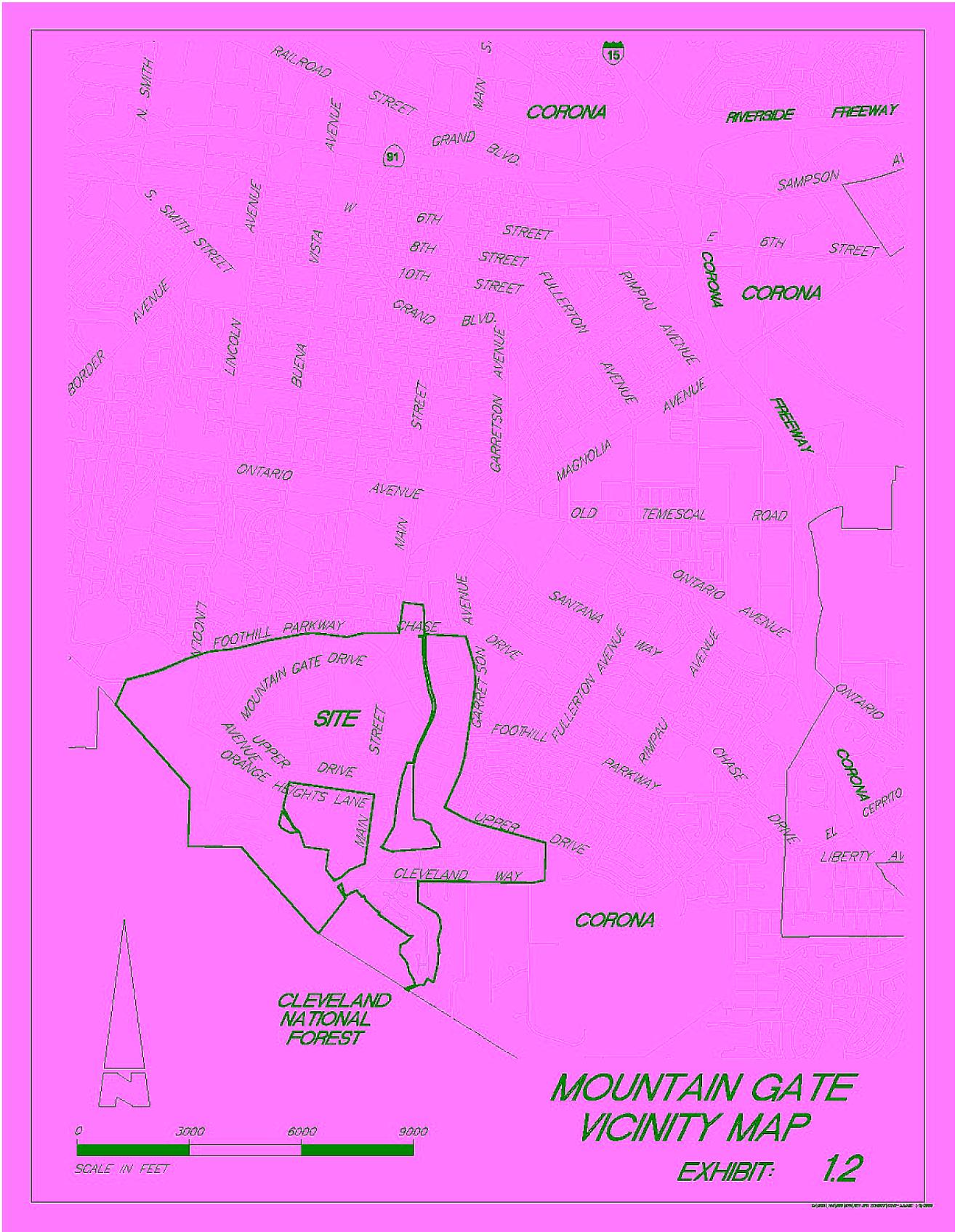
The adoption of the Mountain Gate Specific Plan by the City of Corona is authorized by the California Government Code, Title 7, Division 1, Chapter 3, Article 8 Sections 65450 through 65457.

In July 1988, the City of Corona adopted a Community Facilities Plan (CFP) for approximately 5,067 acres in south Corona. The CFP establishes land uses, infrastructure master plans, facilities financing mechanisms and design criteria to ensure an orderly and attractive pattern of development in south Corona. Consistent with the CFP, the Mountain Gate Specific Plan includes guidelines, criteria and regulations to be utilized in developing the property. It includes the following:



**REGIONAL
LOCATION**
FOOTHILL RANCH
FOOTHILL PROPERTIES





Mountain Gate Specific Plan 89-01

- A land use plan with a variety of land uses (residential, commercial, public, open space)
- A circulation plan establishing the location and size of arterial and collector streets
- Utility plans designating the location, size and type of utilities which will need to be constructed as the project is implemented
- A phasing and public facility financing plan for the logical and orderly improvement of the necessary infrastructure systems
- Development regulations establishing standards which replace, for this property, portions of the City's Zoning Ordinance
- Community design criteria which address master plan landscaping improvements
- Architectural design guidelines to ensure the diversity and quality of residential and commercial development
- An implementation plan which addresses the mechanics of future specific project approvals such as precise plans
- A subdivision section which addresses subdivision and street criteria specific to Mountain Gate

All future land use decisions regarding Mountain Gate must be found consistent with the adopted Community Facilities Plan and with this Specific Plan. Approval of subdivision maps, precise plans, use permits and ultimately building permits must be consistent with the criteria and guidelines included in the CFP and in this Specific Plan.

1.3 Background

Development of Mountain Gate as a planned community follows many years of change and growth. Agriculture on the Mountain Gate site has played an important role in the history of Corona. Mountain Gate, also known as the Corona Foothill Ranch, was part of the Corona Foothill Lemon Company, founded in 1911 by S.B. Hampton. The early economic development of Corona was based on citrus ranching, and the Corona Foothill Lemon Company has been an integral, part of this development for more than 75 years. For many years the company also had a controlling interest in the Temescal Water Company, organized in 1887 by the founders of Corona, which supplied water to irrigate most of the citrus orchards around Corona.

The ranch has progressed through many years of growth and decline tied to the rise and fall of economic forces such as production costs and supply and demand in the world citrus market. Although farming continues, in 1981 the ranch was subdivided into 165 five acre parcels (Tract 14792) consistent with the City of Corona's agricultural general plan and zoning.

Chapter 1.0 Introduction

In the early 1980's, the City of Corona began to look closely at the 5,000 acres located south of Ontario Avenue. These deliberations resulted in a shift in City policy away from the preservation of agriculture, and a General Plan Amendment was ultimately approved in 1986 that would allow an urban planned community of approximately 12,500 dwelling units. Subsequent to the adoption of the General Plan Amendment, GPA 85-6, the City adopted a master specific plan, the Community Facilities Plan, in July 1988. The Community Facilities Plan updated the master plan of land use and traffic circulation for South Corona. The CFP includes master plans for public facilities and services to provide future south Corona residents with a full complement of City services. The CFP also establishes design guidelines and development standards with which individual specific plans must conform.

This Mountain Gate Specific Plan and Amendments SPA-89-13, SPA-90-09, SPA-93-05, SPA-94-04, SPA-97-05, SPA-99-04, SPA-01-001, SPA-01-008, SPA-02-005, SPA-05-006, SPA-06-011, and SPA-08-005 are further implementation of the City's General Plan and the Community Facilities Plan for the 1,205.4 -acre Mountain Gate property.

2.0 PLANNING GOALS AND OBJECTIVES

The City of Corona's planning process for south Corona is based upon a set of adopted goals and objectives that are included within the City's General Plan and the south Corona Community Facilities Plan. This chapter addresses the relationship between the Specific Plan, the City's General Plan, and the Community Facilities Plan.

2.1 General Plan Goals

The City's General Plan is divided into various elements, each of which has its own goals. The General Plan goals that correspond to the Mountain Gate Specific Plan are listed below in bold type, and the method used to comply with the goal in the Specific Plan follows.

Land Use Goals:

1. To develop a land use pattern which meets the basic needs of Corona residents for essential services, working and living areas, and areas for pursuit of leisure time activities.

The Mountain Gate Specific Plan provides a wide range of land uses including single family residential, multiple family residential, neighborhood commercial, an elementary school, parks and greenbelts. The land use pattern has been designed to be compatible with the existing and planned surrounding land uses.

2. To economize on the costs of municipal facilities and services and the extension of these services by phasing residential development in a manner consistent with availability for essential services.

The Mountain Gate Specific Plan proposes a phasing plan that acknowledges the need to economize the cost of municipal services by developing the project consistent with the availability of existing services and the proposed timing of services to be completed as contemplated by the south Corona Community Facilities Plan.

Circulation Goals:

1. To provide a safe and efficient system for movement of people and goods In the City of Corona through City actions and coordination of all agencies involved in development of circulation facilities In the Corona area.

The Specific Plan has proposed a circulation system that is based upon the Circulation Plan of the Community Facilities Plan and independent traffic studies that have evaluated on-site and off-site traffic considerations.

2. To increase the mobility of residents through development of an adequate and balanced transportation system that includes automotive and non-automotive transportation considerations.

The Specific Plan proposes an internal system of streets and open space corridors that will permit efficient movement by automobile, bicycling or walking from residential areas to the school, parks or the commercial areas.

Housing Goals:

1. To encourage development of housing to satisfy the shelter and home environment needs of existing and future Corona residents.
2. To encourage provision of a variety of housing types, prices, ownership possibilities and locations.

The Mountain Gate Specific Plan provides a wide range of housing opportunities including large lot estates, conventional single family detached residential, single family attached residential and multiple family residential.

3. To develop neighborhoods properly related to essential community services.

The Specific Plan proposes a mixed use concept where people can own or rent homes, utilize recreational areas such as greenbelts and parks, attend neighborhood schools and shop at adjacent neighborhood commercial centers. The Specific Plan's proposal to amend the south Corona land use plan by adding neighborhood commercial to the range of permitted uses specifically addresses the need to provide the full range of services for future residents of the area.

4. To maintain high quality development standards for residential land development to ensure establishment of neighborhoods with lasting value.

The Mountain Gate Specific Plan includes special provisions for enhanced parkway landscaping; entry monumentation and theme walls; architectural design guidelines; and an extensive system of parks and trails. The land use and circulation pattern is designed to create secure residential neighborhoods that will not be impacted by through traffic. These elements of the plan in combination will help to create and maintain the vitality and appeal of the individual neighborhoods and of the planned community as a whole.

Conservation Goals:

1. To create a productive balance between man and his uses of land and the conservation of areas with unique environmental and aesthetic value.

Areas of the project that may include significant resources -- major water courses and steep hillside areas- have been designated as Estate Cluster areas. This designation will permit the use of sensitive hillside development standards and clustering to provide additional open space, offering opportunities for retaining natural resource values.

Open Space Goals:

1. To achieve a balanced distribution of open space to meet the needs of residents and contribute to logical development of the urban area.
2. To create a public open space network that satisfies the active and passive needs of City residents.

41.3 acres of the Mountain Gate site have been allocated for a community park, a greenbelt park, a neighborhood park and an historic park. These proposed park sites are linked together by a community greenbelt trail system.

Community Design Goals:

1. To develop a City that is visually attractive, efficiently and effectively organized, and understandable both functionally and psychologically.

Community design guidelines proposed in the Specific Plan will result in an aesthetic and attractive planned community. The Specific Plan includes widened boulevard-like streets with enhanced landscaped parkways and greenbelt parks that permit bicycling and walking outside of the automotive circulation environment. Community design features including special entry monumentation and landscaping, and project-wide theme walls will create a special identity and continuity of design in the community.

2. To preserve and enhance the visual aspects of the City's circulation system for scenic purposes.

The Specific Plan proposes streetscape standards that will include the use of tall columnar trees such as palms and eucalyptus that are typical of some of Corona's established neighborhoods.

2.2 South Corona Community Facilities Plan

In July 1988 the Corona City Council adopted the Community Facilities Plan to guide the overall development of the 5,000-acre south Corona planning area. The Community Facilities Plan (CFP) serves as a master specific plan for the south Corona area. Specific plans for individual development areas, including this plan for Mountain Gate, must be evaluated according to their consistency with the plans, standards and guidelines of the CFP. This section of the Mountain Gate Specific Plan addresses the issue of consistency with the CFP.

2.2.1 Land Use Densities

Exhibit 2.1 illustrates the CFP’s proposed land use patterns and densities for Mountain Gate. The CFP land use plan designates Mountain Gate for Estate, Low, Low Medium and Medium density residential development. As shown in Table 2.1, the CFP land use designations will permit a maximum of 3,675 residential units in Mountain Gate.

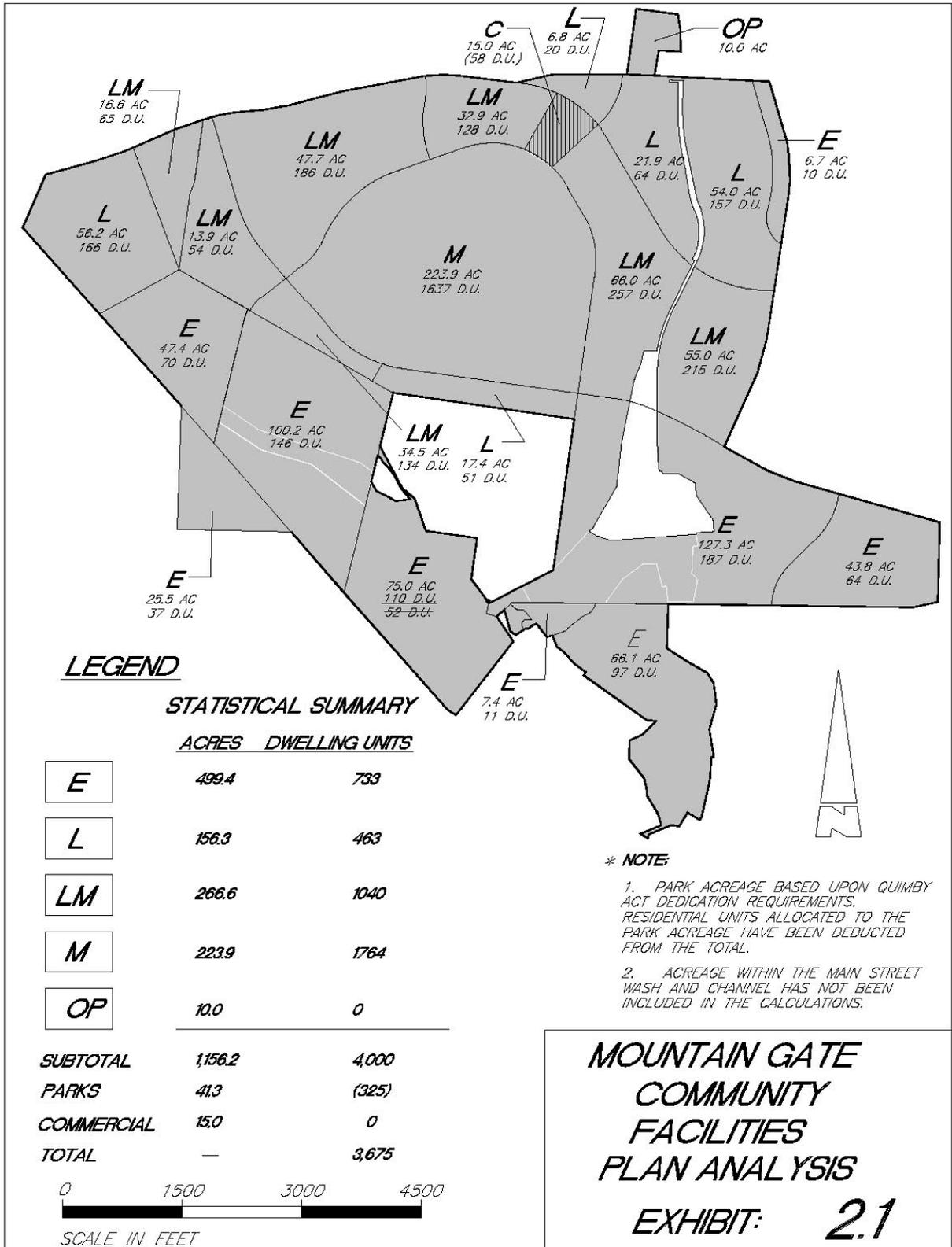
The Specific Plan makes use of the CFP’s density transfer provisions to distribute these units among the various planning areas. However, the Mountain Gate Land Use Plan does not exceed the 3,675 maximum number of units permitted by the General Plan and the Community Facilities Plan.

**Table 2.1
Community Facilities Plan Land Use Designations
For Mountain Gate**

Category	Acres	Target Density	Permitted Units
Estate	499.4	1.47	733
Low	156.3	2.96	463
Low Medium	266.6	3.90	1,040
Medium	223.9	7.88	1,764
Office Professional	10.0		<0>
Subtotal	1,156.2		4,000
Roads	49.1		<0>
Subtotal	1,205.4		4,000
Commercial	<15.0>		<0>
Office Professional	<6.8>		<0>
Schools	<32.0>		<0>
Parks	<41.3>		<325>
Fire Station	<1.3>		<0>
Project Total			3,675

2.2.2 Community Design Goals

Section 3.3.1 of the CFP establishes five overall community design goals for the South Corona area. The relationship of this Specific Plan to these community design goals is addressed below:



A. Character

“The natural and man-made environment of south Corona shall be designed and coordinated to establish the identity of the City while also enhancing the overall character of the south Corona community and the individual character of the four villages of which it is comprised; to improve the image and appearance and to promote the functional efficiency of the City.”

The Specific Plan for Mountain Gate provides a coordinated plan for the major portion of village four, including the entire village core area. The Specific Plan provides a pattern of land use and community design amenities which will create a unique character for village four.

B. Land Use

“Land use shall be developed and managed with respect to location, ownership, timing density and intensity of development in order to be consistent with the capabilities of the City to provide services. Land use shall be consistent with existing uses and ownership through compatibility with adjacent use, smooth transitioning and appropriate buffers when required. A land use pattern shall be developed which meets the basic needs of Corona residents for essential services, working and living areas, and areas for pursuit of leisure time activities.”

The Specific Plan includes a phasing plan with a commitment from the developers to participate in the financing required to ensure that City services and infrastructure will be extended to serve the needs of the development. The Specific Plan provides master plans for streets, sewers, water, storm drains and other public facilities to ensure the adequacy of these facilities. The Specific Plan utilizes density transitions to provide compatible estate and single family development next to the several areas of existing single family homes adjacent to Mountain Gate. The Specific Plan includes proposed sites for an elementary school and neighborhood and community parks to meet the needs for these essential services. In addition, the Specific Plan proposes a neighborhood commercial site in order to address the local shopping needs of the residents -- an essential service which was not provided by the original CFP for Mountain Gate, but which was subsequently added.

C. Circulation

“The organization of land uses within south Corona shall provide for efficient use of the private automobile, expand transit routes while concurrently supporting provision of a system of recreational trails including pedestrian, bikeways and equestrian trails.”

The Specific Plan proposes a land use pattern which is designed to optimize efficient traffic circulation: individual planning areas are designed for good access to the arterial and collector street system, and to minimize through traffic in residential neighborhoods. The Specific Plan also proposes to locate the medium density planning areas adjacent to the Foothill Parkway, Lincoln and Main Street arterial roads, which will maximize traffic efficiency.

With respect to pedestrian and bike trails, the Specific Plan includes a community green-belt network within the village core area; which will serve to link the parks, school and central residential neighborhoods with pedestrian and bike paths. The arterial and collector streets in the plan are designed with right-of-way adequate for the development of on-street bicycle lanes should the City decide to adopt these requirements.

D. Recreation

“Recreation facilities shall be provided to meet the needs of all segments of the community for recreation activities and social interaction.”

The Specific Plan provides one neighborhood park and one community park consistent with the CFP’s recommendations. The Specific Plan identifies another five acre site which is proposed as a potential site for a historic park. The central greenbelt park system is designed to function as a linear park with a series of recreational activity areas to be developed along the greenbelt trail network. The Specific Plan includes a program for recreational improvements in the parks and greenbelt, which are designed to provide specific facilities that will meet a broad range of recreational needs of the future residents of the community.

E. Natural Resources

“The community’s natural resources shall be respected, and protection, preservation and enhancement of those resources shall be encouraged. Resources include existing mature trees, view shed hilltop and ridges, natural streams and drainage areas and open spaces and the adjacency and access to the Cleveland National Forest”

The Specific Plan is consistent with the CFP’s provisions for the preservation of palms and other significant trees. The Specific Plan requires that hillside design standards be approved prior to the design and development of the hillside areas in planning areas 26, 32, and 33, adjacent to the Cleveland National Forest. The estate cluster development concept for planning area 5 offers the opportunity to preserve the Main Street wash above the debris basin in an unimproved state.

2.2.3 Land Use Policies

Section 3.3.1 of the CFP establishes land use policies to guide the development of south Corona. This section addresses the consistency of the CFP with those policies.

Land use policies 1 and 2 are concerned with the timing of development in south Corona, and the monitoring of such development by the Planning Department. The city planning department has organized a development monitoring program to track the pattern and rate of new development in south Corona.

Land use policy 3 encourages the use of specific plans for larger development parcels and the village core areas. To provide incentives for preparation of specific plans, the City will permit unit transfers between land use categories where specific plans are prepared. The Specific Plan is consistent with this policy, and makes use of the density transfer provisions.

Land use policy 4 encourages clustering of community service, educational and recreational uses with the village core areas. Consistent with this policy, the Specific Plan provides park sites, an elementary school site, and a recreational greenbelt in the village core area. The zoning regulations in this Specific Plan provide opportunities for the development of additional community service uses such as churches, day nurseries, and homes for the aged or children as conditional uses. The proposed locations of the neighborhood commercial and quasi-public sites adjacent to the northeast part of the village core provide needed added opportunities for development of essential community services.

Land use policy 5 addresses the mixture of residential densities and transitions between densities within new development. The Specific Plan contains provisions in the land use plan itself and in the zoning regulations to comply with these specific standards:

- a. Separation between single family attached (SFA) and single family detached (SF1)) residential units: 20 foot minimum rear yard setback for SFA adjacent to SFD; front on separation by a collector street with 25 foot front yard setbacks.
- b. Separation between SF1) and apartments: a collector street, park or minimum 40 foot wide greenbelt. Separation between apartment and townhouse residential: 20 foot yard setbacks or, on a back-on condition, by a park or minimum 25 foot landscape area.
- c. Separation between commercial and SF1): a collector or arterial street, or landscape buffer and setback. 15 foot landscape buffer to separate the street from parking which faces a residential area. SFA or SF1 adjacent to a commercial area, 20 foot landscape buffer to be provided on the commercial property.

Land use policy 6 addresses the interface between new residential development and existing residential neighborhoods in south Corona. The Specific Plan contains the following provisions to provide a compatible transition between new development and existing residential neighborhoods:

- a. Existing neighborhood east of Garretson and north of Chase Drive at Garretson - the Specific Plan designated planning area 2 for one acre residential lots adjacent to this existing neighborhood. The lots in planning area 2 will front on Garretson.
- b. Existing neighborhood south of Cleveland Way near Garretson - the Specific Plan establishes a special requirement for planning area 5, which is designated for estate cluster development. The Specific Plan requires one acre residential lots to be developed north of Cleveland Way, across the street from this portion of the Crown Ranch neighborhood, and along the easterly edge of Planning Area 33, which abuts a portion of the Crown Ranch neighborhood.
- c. Existing homes south of Orange Heights Lane west of Main Street - the Specific Plan designates planning area 14 for one acre residential lots adjacent to these existing homes. The lots in planning area 14 will front on Orange Heights Lane. In addition, the Specific Plan proposes one acre lots along the northeasterly edge of planning area 26, which is designated for estate cluster development.

- d. Existing homes north of Chase Drive which front on Main Street - the Specific Plan designates planning area 8 for single family detached residential development. Chase Drive separates the lots in planning area 8 from the existing homes, which front on Main Street. The new lots in planning area 8 will not front on Chase Drive.
- e. Existing home north of Chase Drive, between Main Street and Taylor Street -- the Specific Plan designates planning area 9 opposite this home as a quasi-public use site, which permits such uses as a post office, day care center, and/or fire station. The quasi-public site is separated from the existing home by Chase Drive. The quasi-public site will not front on Chase Drive, and a special landscape buffer area will be provided opposite the existing home consistent with the requirements of the CFP.
- f. In all of these situations where new development is proposed near existing homes, the Specific Plan complies with the CFP's requirements relating to setbacks, landscaping, and/or decorative walls.

2.2.4 Village Core Concept

Several sections of the CFP address the "village core" concept. Page 3-9 of the CFP provides the following:

"Village cores are intended to be the focal point of the village or planning area in terms of a place where residents can meet and participate in community activities.. . .The precise type and mix of village core activities and their location shall be determined in conjunction with specific plans for villages. The configuration of the core areas should be flexible..."

With respect to village four, page 3-20 of the CFP provides the following:

"This village will focus on a medium density village core area located in the north- central section of the village with access from [Parkway], Upper Drive and a northerly loop road. One of the most unique characteristics of this village is that a major portion of this area is under single ownership and this ownership encompasses the total village core area. This offers the opportunity to create a core which is totally unified by land use and design elements that can be accomplished through this Specific Plan process."

Section 3.3.5.3 of the CFP establishes specific village core policies for south Corona. These policies include:

- "Provide locational opportunities for village services
- Establish a unique character or identity
- Provide a place for social interaction
- Provide for recreational needs of varying levels
- Provide a village with a functional urban structure/form
- Establish a visually attractive environment
- Efficiently and effectively organize land use patterns
- To provide a focal point for village and inter-village linkages (trails, paseos, greenbelts, etc.)
- To function as a unifying element of the village."

This Specific Plan proposes to develop the village four core area as a unique, central and unifying element of the Mountain Gate planned community. The Specific Plan envisions the village core as a higher end, recreation-oriented residential area, comprised of single family detached and single family attached neighborhoods oriented towards a family housing market.

Each of these neighborhoods in the core will have direct access to a community greenbelt that is designed as an integrated linear park system. The greenbelt park will link these neighborhoods with the community and neighborhood park sites located at the northeast and west edges of the village core; the greenbelt park will also provide bicycle and pedestrian trail access to the elementary school site located adjacent to the neighborhood park site.

The Specific Plan proposes to locate two medium density residential areas adjacent to but outside of the village core itself. Although it proposes a different approach to locating the medium density development areas, the village core concept for Mountain Gate is consistent with each of the nine specific CFP village core policies referenced above. The amended CFP recognizes the density distribution proposed in this Specific Plan.

2.2.5 Community Character Policies and Standards

Section 3.32 of the CFP provides policies and standards relating to community character in south Corona. The following addresses the Specific Plan's consistency with those policies and standards.

A. "Policy - Reinforce south Corona's identity by the use of appropriate architecture for commercial and public buildings."

The CFP standards to implement this policy establish a Spanish colonial architectural theme for commercial and public buildings. A variety of architectural themes is encouraged for residential neighborhoods in each village.

The Mountain Gate Specific Plan includes architectural design guidelines for residential development to ensure the diversity and quality of architecture in the residential neighborhoods. Certain types of development in the Specific Plan will also be regulated by the pending city design review ordinance. The Specific Plan requires that commercial and public buildings comply with the CFP's architectural standards.

B. "Policy - Reinforce community identity and create a sense of continuity throughout the area through appropriate landscape features within arterial rights-of-way and landscape maintenance districts will be established to maintain landscaping within the public rights-of-ways and landscape easements."

The CFP's standards to implement this policy require the continuation into south Corona of certain existing street trees north of Ontario Avenue. New streetscape landscaping standards are established for the arterial and collector streets in south Corona Improvements within street rights-of-way and adjacent landscape easements must conform with city standards for landscape maintenance districts.

The Mountain Gate Specific Plan requires that a landscape maintenance district be established for the purpose of maintaining parkway landscape improvements and adjacent dedicated landscape lots. The community design element of this Specific Plan establishes parkway landscaping requirements to implement the CFP; certain changes in plant materials for selected roads within the Specific Plan are proposed. The Specific Plan proposes to expand upon the CFP's landscape easement requirements by providing wider landscape lots adjacent to the village core Loop Road.

- A. "Policy - Concentrate commercial development adjacent to existing commercial uses and at highly accessible and visible locations within the community as adopted in the General Plan."

The CFP establishes standards and guidelines for permitted uses, architecture and design for the two planned commercial development centers located along Ontario Avenue.

The Mountain Gate Specific Plan is consistent with the amended general plan for south Corona in its location of a new neighborhood commercial center at the intersection of Main Street and Foothill Parkway. This location is central to the south Corona area, at one of the most visible arterial road intersections. An analysis of the future commercial market demand in south Corona clearly supports the need for additional neighborhood commercial development, and the desirability of this particular location for commercial uses. This proposed neighborhood center is intended to serve some of the daily local shopping needs of the future residents of south Corona.

- B. "Policy - Reinforce south Corona's relationship to the City through compatible adjacent uses and continuation of key existing street tree palettes into south Corona on adjoining streets."

Standards in the CFP to implement this policy provide that: (1) "estate uses shall always be adjacent to single family detached and not next to attached product or commercial development unless the estate area is separated by a greenbelt or arterial roadway," and (2) "multi-family and more single-family development shall be concentrated in four village cores with arterial streets separating this density from less dense single-family products." A third standard requires the continuation of existing street tree palettes into south Corona.

The Mountain Gate Specific Plan is consistent with standard (1) referenced above in that, estate home development in planning areas 5, 26, and 33 is located adjacent to planned single-family detached development, or separated from higher density development by a road or greenbelt. The plan also provides a special landscape buffer area along the north edge of planning area 9 to ensure compatibility with adjacent existing homes.

The Specific Plan proposes to modify standard (2) referenced above by locating medium density areas outside of but adjacent to the village core. Adequate buffers around the proposed medium density development areas are provided either by arterial/collector street separation or through the use of appropriate building setbacks and landscaping. The community design element of the Specific Plan is consistent with the standard relating to the continuation of certain existing street tree palettes.

2.2.6 Open Space Policies and Standards

Section 3.3.3 of the CFP establishes policies and standards relating to open space in south Corona.

A. “Policy - Connect each of the four villages with a continuous open space recreational trail system.”

The CFP provides standards to implement this policy through requirements for bicycle trails through south Corona. In adopting the CFP, the City Council removed provisions for certain on-street bicycle lanes. Consistent with the CFP, the Mountain Gate Specific Plan provides for off-street bicycle trails along portions of Old Chase Drive, the new Foothill Parkway arterial road, and within the greenbelt through the community core area. The Specific Plan prohibits on-street parking along the arterial and collector streets in the plan. The arterial and collector street cross sections include adequate pavement for emergency parking, which could be striped for bicycle lanes should the City Council decide to do this in the future.

B. “Policy— Protection of significant landform features shall be required such as existing eucalyptus stands and certain natural drainage courses.”

The CFP’s standards to implement this policy include provisions for preservation of existing eucalyptus stands, preservation or relocation of palm trees, and preservation of natural drainage courses where feasible. Retention of some existing citrus trees is also encouraged.

The Mountain Gate Specific Plan requires that eucalyptus stands be evaluated for possible preservation during future detailed design of individual development projects; it is recognized that many eucalyptus trees cannot be preserved because of tree disease, road and utility improvement requirements, and aesthetic considerations. The Specific Plan requires that existing palm trees either be preserved or be relocated for incorporation into the planned streetscapes and project entry landscaping. The Specific Plan’s proposal for an historic park site would include citrus trees as part of a “citrus heritage” historic site.

The drainage element of the Specific Plan has evaluated the Main Street wash and Eagle Canyon wash areas upstream of the existing debris basin in planning area 5. The Specific Plan designates this area for estate cluster development. If the cluster design concept includes open space areas along the washes, it may be feasible to preserve the wash areas in their current condition, which has already been modified and in some locations channelized during many years of agricultural use. Because of the City's requirements for adequate flood protection for new development areas, preservation of these existing drainage courses would be neither feasible nor desirable if the cluster development program includes residential development adjacent to the channels.

C. "Policy - Preservation of hillsides and wash area (ridge and valley area) of the southeast portion of south Corona."

The CFP establishes several standards to implement this policy. These include provisions to preserve important vistas, maintain the overall character of natural landforms, preserve significant vegetation and rock outcrops, and the use of contour grading. The CFP also requires the use of City hillside roadway standards and rural road sections where appropriate. The CFP further requires that "specific development guidelines for these hillside areas shall be prepared and implemented by the City prior to development."

Although Mountain Gate is not located in "the southeast portion of south Corona," the southern portion of planning area 26 does include a hillside area. This area has already been extensively regraded and planted in avocados as part of ranch farming operations. Planning areas 32 and 33 also include a hillside area. The Specific Plan proposes that within the context of the estate cluster residential land use designation, this hillside area will be developed for custom view lots to take advantage of the excellent views from this area. Because planning area 26 and 33 are planned as the final phase of development within the project, and thus this development will not take place for a number of years, the Specific Plan does not at this time include specific hillside development standards for this area. The Specific Plan requires that specific hillside development standards be prepared and approved prior to the approval of detailed subdivision and development plans for this hillside area.

D. "Policy - Development which occurs adjacent to natural wildland areas as defined by the Fire Department shall be protected by a fuel modification program."

In these situations, the CFP standards require replanting with fire resistant materials within 100 feet of a structure, with fuel modification plant palettes to be consistent with city standards.

The landscaping standards of the Mountain Gate Specific Plan include the CFP's fuel modification requirements. Maintenance responsibilities for the fuel modification areas are addressed in section 8.2 of this Specific Plan.

2.2.7 Community Character of Estate Residential Areas in Villages 3 and 4

Section 3.3.4 of the CFP addresses certain development standards for estate residential areas.

- A. “Policy - Reinforce south Corona’s identity by reinforcing the rural estate character of the hillside areas through rural themes via area of lots, setbacks of structures, use of rolled curbs and rural theme fencing.”

Standard 1 requires that an arterial street separate rural estate from low-medium density single-family products, and requires a 25 foot buffer of informal eucalyptus stands between estate lot and street.

Standard 2 provides for minimum 15 foot side-on and 30 foot back on setback to property line for estate lots. A rural estate 6 foot wall is required along rear and side property lines along Upper Drive.

Standards 3 and 4 recommend an informal street curb, and require consistency with City public works standards for rural roads.

In the Mountain Gate Specific Plan, planning areas 5, 14 and 26 and 33 are located in the general plan “Estate” area south of Upper Drive. Planning area 14 is planned for one-acre estate lots to provide a buffer for the existing estate homes south of Upper Drive. Planning Area 33 is designated for ER-2 (20,000 square foot minimum), however, the southern portion of the site is within the Hillside Development Overlay District and will require the development of one-acre lots. Planning areas 5 and 26 are designated for estate cluster development. This designation will permit the creation of distinctive planned communities within the overall Mountain Gate development. Consistent with the intent of the estate cluster designation, open space areas and trails will be combined with a variety of single family products, ranging from estate lots to more conventional detached and attached homes. Consistent with the CFP, the overall average density for the estate cluster areas shall not exceed 3 units per acre. This makes use of the general plan’s provisions for clustering of homes in the estate area. The Specific Plan provides further requirements to ensure that the new development in planning areas 5, 26 and 33 is compatible with existing estate development on adjacent properties:

- (1) In planning area 5, one-acre lots shall be developed along the north side of Cleveland Way west of Royal Vista Circle, and along the east side of Main Street opposite existing large lot estate homes, for compatibility with existing estate homes on adjacent properties.
- (2) In planning area 26, estate lots will be developed along the northeast edge of this planning area opposite the existing estate homes south of Orange Heights Lane and in the southerly portion of this planning area adjacent to City of Corona boundary.
- (3) In Planning Area 33, one-acre lots shall be developed adjacent to the estate homes within the existing neighborhood of Crown Ranch Road.

Consistent with the intent of the CFP, the hillside area adjacent to the Cleveland National Forest boundary (planning area 32) has been designated as open space to reinforce the area’s rural character. The development standards and community design element of the Specific Plan are consistent with the CFP’s requirements for the eucalyptus buffer zone and wall along Upper Drive, and with the setback requirements for estate lots.

2.2.8 Village Character Policies and Standards

Section 3.3.5.1 of the CFP establishes a number of policies and standards relating to the village cores, school and park sites, village entries and project walls, landscape edges and setback criteria

- A. “Policy - Optimal location of schools and parks in villages shall reinforce individual village identity.”

The CFP standard for this policy encourages the location of school and park sites adjacent to one another for combined parking and large open play areas where feasible.

The Specific Plan does locate the proposed elementary school site adjacent to a proposed neighborhood park site to take advantage of the relationships between these two uses.

- B. “Policy - A landscaped recreational corridor shall be developed within each village core which links schools, parks and other community facilities together with the residential areas. This linkage shall be developed in a 30 foot wide landscaped area and will include a 10 foot wide combined pedestrian and bicycle pathway. This corridor may be located either on the arterial road which defines the village edge or be located centrally within the medium density village cores...”

The Mountain Gate Specific Plan includes provisions for a central greenbelt consistent with this policy. The Specific Plan goes further than the CFP requirements, by proposing the improvement of the greenbelt with a series of recreation areas to create a linear park amenity. In addition, the development plans for the two estate cluster areas (planning areas 5 and 26) may provide for additional greenbelt trails or open space areas as part of the cluster concept.

- C. “Policy - Require special landscape and design treatments at major entrances to the four villages.”

By establishing specific landscaping and design treatments for the hierarchy of entries to the planned community the community design element of the Mountain Gate Specific Plan is consistent with this policy and the implementing standards in the CFP.

- D. “Policy - Reinforce south Corona’s identity by establishing four villages with individual image by landscape materials, entry monumentation and walls unique to each.”

By establishing specific standards and design details for theme walls, entry monumentation and landscaping the community design element of the Mountain Gate Specific Plan is consistent with this policy and the implementing standards of the CFP.

- E. “Policy - Setbacks In each village shall be established for consistency along arterial roadways.”

To implement this policy, the CFP establishes specific standards for landscape lots adjacent to major roads, theme walls at the edge of the landscape lots, and minimum building setbacks from the walls.

The community design element of the Mountain Gate Specific Plan is consistent with the landscape lot and theme wall provisions of the CFP. In some cases, the Specific Plan proposes landscape lots which are wider than the minimums established in the CFP. The zoning regulations of the Specific Plan are consistent with the CFP's special setback provisions adjacent to the major roads.

2.2.9 Architectural Design

Section 33.6 of the CFP establishes policies and standards relating to architectural design for future commercial and residential development in south Corona. These policies and standards apply a unifying architectural theme for commercial and public buildings; encourage a variety of residential architectural styles; encourage the use of traditional construction materials; and discourage monotony in residential dwelling design.

Consistent with the CFP, the Specific Plan does establish residential architecture design guidelines to ensure a diversity of styles and a high quality of architectural design. Diversity of architectural styles in residential development is further encouraged through the broad range of product types provided for in the Specific Plan, including estate lots, single family detached homes, single family attached homes, and townhomes. Diversity within the single family detached neighborhoods is supported by the range of implied densities established for the various planning areas; these varying densities are intended to accommodate different lot areas, lot widths, and neighborhood streetscape designs. In addition, certain types of development in the Specific Plan will be regulated by the design review board process that is being established by the City.

2.2.10 Landscape Design

Section 33.7 of the CFP establishes policies and standards relating to community streetscapes in south Corona. Specific policies in the CFP include the following:

- “A. Develop a landscape character which strengthens the residents’ perception of the City community as a unique ‘place.’
- B. Create a soft transition between urban development and natural open space.
- C. Enhance the pedestrian character and climate of the community areas and other people gathering places.’
- D. Differentiate community streets, arrival and entry elements.
- E. Provide plant materials that minimize strain on the City’s water supply.
- F. Encourage a landscape theme that creates a shady appearance to counter the hot, arid conditions of the community’s summer climate.”

The Mountain Gate Specific Plan is consistent with these CFP policies, and is generally consistent with the specific landscape standards established by the CFP. The Specific Plan does propose to change certain plant material palettes on certain streets in the plan, for the purpose of creating more shade and more of a “boulevard” effect along the major road entries to the planned

community. The details of the Mountain Gate landscape proposals are presented in the community design chapter of this Specific Plan.

2.2.11 Traffic Circulation Plan and Standards

Section 3.4 of the CFP presents the south Corona traffic circulation plan and street standards. The circulation master plan for Mountain Gate is presented in chapter six of the Specific Plan. This circulation master plan is consistent with the CFP with respect to street sections and points of connection to the CFP's arterial and collector road alignments at the project boundaries.

The Specific Plan does propose to modify the detailed alignments of some of the arterial and collector streets within the interior of Mountain Gate. The Specific Plan also proposes to use a 1500 foot minimum radius for Foothill Parkway where it transitions from the existing Chase Drive alignment to the existing Pacific Drive alignment. These road alignment refinements are proposed to create better configurations for development areas within the ranch.

2.2.12 Public Facilities Plan

Section 3.5 of the CFP contains plans, policies and standards relating to parks and trails, schools, fire stations, police service, libraries and post office facilities in south Corona. The relationship of the Specific Plan to these plans, policies and standards is discussed below.

Parks

The CFP designates two general locations for parks within the boundaries of the Mountain Gate Specific Plan. These include a 15 to 20 acre community park southwest of the intersection of the Mountain Gate Drive and Main Street, and a 5 acre neighborhood park at the intersection of Lincoln and the Mountain Gate Drive. The latter is proposed as a joint use park/drainage retention facility.

The Specific Plan is consistent with this element of the CFP, through its designation of a 20-acre community park, and a 5-acre neighborhood park designed for joint use retention. These parks would be linked by the community greenbelt park which includes satellite recreation areas and facilities developed along the length of the greenbelt. In addition, the Specific Plan also proposes a 5-acre historic park encompassing the site headquarters area and adjacent citrus grove at Foothill Parkway and Taylor Avenue.

The Specific Plan also proposes a program for development of recreation facilities in these parks. The elements of this park facilities program are specifically designed to respond to the facilities needs and priorities identified in the Corona parks and recreation user survey prepared as part of the new parks and recreation master plan.

Schools

The CFP proposes one elementary school site, one junior high school site, and a possible senior high school site within the boundaries of the Specific Plan.

The Specific Plan proposes one elementary school site adjacent to the westerly neighborhood park site, consistent with the CFP. The Specific Plan does not propose a high school site, because the school district staff has indicated that the future south Corona high school should be located further to the east, to better serve the future development planned in unincorporated areas in the Temescal Valley.

The school district has expressed an interest in discussing the acquisition of the northerly 20 acres of planning area 6, located between Main Street and the Main Street channel. The Specific Plan designates this site as a “Potential Junior High School Site” with an underlying designation for single family detached residential development. If negotiations with the school district lead to an agreement for the acquisition of this site, it will be developed as a school.

Fire Station

The CFP proposes that a fire station be located in the vicinity of Lincoln and Foothill Parkway. The Mountain Gate Specific Plan provides for this fire station within planning area 29, at the southwest corner of the intersection of Main Street and Upper Drive.

Police

The CFP does not anticipate future police facilities will be constructed in south Corona. Instead, police operations will continue to be headquartered at the Civic Center on Sixth Street.

Library

The CFP proposes that a branch library be located in the commercial center at Magnolia and Main. The Corona Library Board has embarked on a major project to expand the existing public library at Main and Sixth. The Library Board has made no recommendations to the City Council regarding the possible establishment of branch libraries in south Corona.

The Specific Plan’s proposal for a neighborhood shopping center at the corner of Main Street and Foothill Parkway could accommodate a store front branch library in the future, should the city decide to pursue this possibility. It is also possible that the quasi-public site or the proposed historic park site may also be suitable for future branch library operations.

Post Office

The U.S. Postal Service is seeking a site for a new post office to serve the area east of Main Street. The developers of Mountain Gate have met with Postal Service representatives to discuss the feasibility of locating a post office in planning area 9, the proposed quasi-public site on the northwest corner of Foothill Parkway and Main Street. The Postal Service has expressed an interest in this site, and has indicated that it would be a desirable location for the new post office. Limitations in federal funding for Postal Service land acquisition preclude a decision by the Postal Service at this time.

2.2.13 Utilities Plan

Sewer Plan

Section 3.6 of the CFP contains a summary of the sewer facilities plan for south Corona. The Mountain Gate Specific Plan is generally consistent with the CFP master sewer plan. The Specific Plan does propose an amendment to the south Corona sewer master plan to permit gravity sewer service for planning areas 1, 3 and part of 5 which are located in the lower arroyo area adjacent to the Main Street channel. Details of the Mountain Gate sewer master plan are presented in chapter 7 of this Specific Plan.

Water Plan

Section 3.7 of the CFP contains a summary of the water facilities plan for south Corona. The Mountain Gate Specific Plan is consistent with the CFP water plan. As described in chapter 7 of this Specific Plan, a water master plan is proposed for the project which provides for the phased development of the necessary elements of the south Corona water system.

Drainage Plan

Section 3.8 of the CFP contains a summary of the drainage facilities plan for south Corona. The Mountain Gate Specific Plan is consistent with the CFP drainage plan. Depending on the future plan for the single family cluster development in planning area 5, an addition to the CFP drainage plan may also be needed for the area upstream of the Main Street debris basin. As described in chapter 7 of this Specific Plan, a drainage master plan is proposed for the project which provides for the phased development of the necessary elements of the south Corona drainage system. This includes both permanent retention facilities as called for in the CFP, and also temporary retention facilities in the event that the Oak Street channel and/or the Lincoln and Buena Vista storm drain systems are not completed prior to the latter phases of Mountain Gate development.

2.2.14 Plan Implementation

Section 3.9 of the CEP addresses the requirements for plan implementation and the role of Specific Plans in that process. Specific elements are required to be addressed in future Specific Plans.

The Mountain Specific Plan contains almost all of the components required for Specific Plans in south Corona. Hillside development standards are not included in the original version of the Mountain Gate Specific Plan, but the Specific Plan does require that such standards be adopted prior to detailed development approvals for the hillside area in planning area 26. Specific details of offsite project infrastructure phasing and participation in the financing plan for south Corona are addressed in the conditions of approval for the Specific Plan and the accompanying master tentative tract map, and in the proposed Mountain Gate development agreement.

2.2.15 Summary of Consistency with the Community Facilities Plan

As described in this section, the Mountain Gate Specific Plan is generally consistent with the adopted Community Facilities Plan. Where particular differences between the plans exist, these differences are proposed to be addressed either through (1) an accompanying general plan amendment, (2) an amendment of the community facilities plan, or (3) an interpretation of the intent of the Community Facilities Plan.

3.0 EIR MITIGATION MEASURES

The following mitigation measures were adopted by the Corona City Council as part of the certification of the final Environmental Impact Report for the Mountain Gate Specific Plan SP-89-1 and the related General Plan Amendment GPA-89-3 and Community Facilities Plan Amendment SPA-89-2.

Land Use

1. Precise plans for Planning area 21 shall include design features to avoid potential land use conflicts with lower density residential areas planned off-site. These features may include landscape screening, berms, walls, judicious setback distances from planning area boundaries, and orientation of land uses to minimize off-site disturbance, in addition to requirements of Policies #5 and #6 of the CFP. Similar features shall be incorporated into precise plans for Planning areas 9 and 10.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Precise plans for Planning Areas 9,10 and 21

2. Site plans shall adhere to the provisions of the south Corona Community Facilities Plan (CFP) in regard to transitions between densities.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Site Plans and Tentative Tract Maps

3. Grading plans in the area of Main Street Canyon (planning area 5), and the unnamed canyon and two minor drainages in the southwestern portion of the site (planning area 26) shall be reviewed for compliance with policies of The Conservation Element which require that canyons be preserved from encroachment, and that creeks and channels be retained in their natural state whenever feasible. Although modification of channels is necessary to provide storm drain improvements, some preservation of natural canyon areas may be feasible.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Grading Plans

4. Development within the Specific Plan shall preclude any structures or any features which cause significant interference to operations at the AT&T facility south of the Specific Plan site. Tract maps within Planning Area 5 shall be reviewed by AT&T prior to approval.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Tentative Tract Maps for Planning Area 5

Geology and Soils

5. Development shall conform to a 50 foot setback zone along the south side and a 100-foot setback zone along the north side of the Main Street branch of the Whittier Elsinore Fault zone on-site as shown in Exhibit 9 and as further defined in Plate 1 of the Geologic Fault Investigation for the site (Highland Soils Engineering, Inc., July 1987) on file with the City Planning Department and Public Works Department. No human occupancy structures shall be permitted in this setback zone.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Tentative Tract Maps for Planning Area 26

6. All structures shall be designed in accordance with the seismic design provisions of the Uniform Building Code relative to seismic conditions identified in the site specific geotechnical studies for the project.

Responsibility to Implement: Developer

Responsibility to Monitor: Building Department

Timing: Building plan check

7. Prior to approval of a tentative map for development purposes, the developer shall submit a conceptual grading plan. The conceptual grading plan shall show proposed areas of cut and fill, topography, steepness of slope, locations and extent of buttresses and bench drains, and shall illustrate conformance with the City's grading ordinances.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing Tentative Tract Maps

8. Prior to approval of a final tract map, rough grading plans shall be approved, and prior to building permit issuance, a precise grading plan shall be approved. Both rough and precise plans shall be prepared by a Civil Engineer and be based on recommendations of the Soils Engineer and an Engineering Geologist subsequent to completion of detailed soils and geologic investigation for each subdivision area. The site specific geotechnical studies shall provide specific feasible recommendations relative to slope stabilization, soils engineering (including seismically-induced settlement and liquefaction), and appropriate drains and subdrains. Grading plans shall be reviewed by the City Engineer and Planning Department and shall be subject to a grading permit.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works and Planning Departments

Timing: Final Tract Maps

9. Rough grading plans shall include an erosion, siltation, and dust control plan to be approved by the Public Works Department. The plan shall include provisions for measures such as immediate planting of vegetation on all exposed slopes, temporary sedimentation basins and sandbagging, if necessary, and a watering and compaction program. The plan shall ensure that discharge of surface runoff from the project during construction activities will not result in increased erosion or siltation immediately downstream of the property.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Rough grading plans

Hydrology and Water Quality

10. The developer shall construct storm drains coincident with development of each subdivision map area per the phasing plan presented in the Specific Plan. These improvements shall be constructed according to final engineering drawings. Improvements shall be approved by the City Engineer.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Final Tract Maps

11. Site planning for the neighborhood park site (Planning Area 22) shall ensure that no structural uses shall be permitted within the detention basin; non-structural uses such as play fields, parking lots, picnic areas, trails, etc. shall be permitted.

Responsibility to Implement: Parks and Recreation Department

Responsibility to Monitor: Parks and Recreation Department

Timing Park Site Plans for Planning Area 22

12. Additional temporary detention basins which may be constructed on-site shall be approved by the Public Works Department and Planning Department, subject to a detailed hydrology study. The study shall address the off-site properties which might also require similar facilities. Approval of any such basins shall be accompanied by site specific plans for fencing and permitted landscaping per the Special Landscape Standards of the Mountain Gate Specific Plan.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works and Planning Departments

Timing: Tentative Tract Maps

13. During construction phases, the developer shall be responsible for providing regular street sweeping on all roadways on-site. This service shall be conducted to the satisfaction of the City Engineer and shall be initiated immediately after paving of each roadway.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: During construction

14. Additional requirements may be imposed in the future in the implementation of the proposed NPDES enforcement program on non-point discharges.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Indefinite

Biological Resources

15. Development in Planning Area 26 shall include an open space buffer along the site's southern boundary.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Tentative Tract Maps for Planning Area 26

16. Precise plans for open space and individual tract maps within Planning Area 5 shall be designed so as to retain the stand of oak trees within the southern boundary of the site in permanent open space. Preservation of the oak tree stand and provisions for natural drainage insuring their survival shall be reviewed by the Department of Fish and Game in conjunction with Section 1603 requirements related to streambed alteration.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department, Department of Fish and Game

Timing: Tentative Tract Maps and Precise Plans for Planning Area 5

Paleontological Resources

17. Prior to any grading or development within areas on-site underlain by the older Quaternary alluvium, Terrace deposits, or the Silverado Formation (see Paleontology Sensitivity Map in Appendix E of EIR), the developer shall submit rough grading plans and grading schedule to a qualified paleontologist. The paleontologist shall conduct a walkover survey (restricted to natural terrain within these areas) to determine if any significant fossils are exposed. Any fossils located shall be evaluated for their significance and vulnerability to direct or indirect impacts, and, if necessary shall be collected, based upon the professional judgment of the paleontologist and the judgment of the Planning Director that such actions are consistent with the City's intent to research, recover, and preserve significant paleontological resources.

Responsibility to Implement: Developer's paleontology consultant

Responsibility to Monitor: Planning Department

Timing: Rough Grading Plans

18. During grading operations within areas underlain by Terrace deposits or the Silverado Formation, the developer shall retain a qualified paleontologist to perform periodic inspections for exposure of fossils. In the event that fossils are exposed, the paleontologist shall be allowed to divert or direct grading in the area of exposure to facilitate evaluation, and (if identified as potentially significant) to salvage. Salvage operations shall be funded by the developer.

Responsibility to Implement: Developer's paleontology consultant

Responsibility to Monitor: Developer

Timing: Grading operations

19. During grading operations within areas underlain by older Quaternary alluvium, the developer shall permit periodic inspections by a qualified paleontologist. In the event that fossils are exposed, the paleontologist shall be allowed to divert or direct grading in the area of exposure to facilitate evaluation, and if identified as potentially significant) to salvage. Salvage operations shall be funded by the developer.

Responsibility to Implement: Developer's paleontology consultant

Responsibility to Monitor: Developer

Timing: Grading operations

20. All fossils collected shall be prepared and identified by a qualified paleontologist. They shall then be donated to a suitable institution with a research interest in the materials. Selection of the appropriate institution shall be made by a qualified paleontologist.

Responsibility to Implement: Developer's paleontology consultant

Responsibility to Monitor: Developer

Timing: Grading operations

Historical Resources

21. Prior to approval of any discretionary permits or grading of Planning Area 18, the City and applicant shall jointly prepare a feasibility study to determine the potential viability and community value of the proposed historic park. The study shall address:

- a. An appropriate party and funding mechanism to purchase, maintain and manage the proposed historic park and its resources.
- b. Provisions for the preservation of historical resources within the park in conjunction with adaptive reuse of structures to provide opportunities for community related uses of on-site facilities (such as library, meeting halls, senior citizen center, historical museum, research center, art museum, etc.)
- c. Provisions for interpretive information and/or exhibits related to on-site historical resources.

Based on this study, the City shall determine whether or not to implement the historic park.

Responsibility to Implement: Planning Department and Developer

Responsibility to Monitor: Planning Department

Timing: Study to be commenced prior to recordation of phase 1 final map

22. Because preservation of resources in the park is uncertain, and because the park could not possibly preserve the entire Mountain Gate complex, mitigation is required through photo documentation and detailed mapping of resources. Prior to commencement of grading for any portion of the site, structures and facilities of potential historic interest in that area shall be photo-documented by a qualified architectural historian, in general accordance with Historic American Building Survey (AHBS) guidelines. Photo-documentation shall include a detailed mapping of all structures and features of historical interest. The resulting report shall be submitted for review and approval by the Planning Director and the approved report shall be retained on file for public review.

Responsibility to Implement: Developer's historic consultant

Responsibility to Monitor: Planning Department

Timing: Prior to grading of areas with facilities of historic interest

Population and Housing

23. Foothill Properties shall provide relocation assistance to families currently residing on the Mountain Gate site in the form of financial assistance toward moving expenses. Agricultural operations will gradually be phased out and Foothill Properties will attempt to place farm workers in other operations as development proceeds.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Prior to removal of existing farm worker housing

Traffic and Circulation

24. The developer shall participate in the City's south Corona infrastructure financing program as required by City ordinance.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Mello-Roos District formation and/or final map recordation

25. In order to mitigate commuter traffic demands upon the freeway system, the developer shall participate a pro-rata share in the funding of a park and ride facility, when such an improvement and funding program has been established by the City Council.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Indefinite

26. The developer shall prepare detailed plans based on a technical study for signalization and intersection geometrics, including detailed striping for collectors, secondary arterials, and major arterials. Detailed plans shall also specify access locations along Main Street and Foothill Parkway for proposed commercial and quasi public uses. Plans shall be reviewed and approved by the City's Public Works Department concurrently with approvals for development in parcels adjacent to these roadways and intersections.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Tentative and Final Tract Maps

27. Development shall comply with phasing and infrastructure plans as identified in the Mountain Gate Specific Plan and the City's Community Facilities Plan.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Public Works and Utilities Departments

Timing: Tentative Tract Maps

28. The developer and City Engineer shall coordinate with the Riverside Transit Authority prior to the recordation of the master tentative tract map regarding the provision of mass transit to serve the development and the need for bus turnouts.

Responsibility to Implement: Developer, Public Works Department, and RTA

Responsibility to Monitor: Public Works Department

Timing: Prior to phase one map recordation

29. The developer shall be responsible for the provision of information packets to potential buyers of property regarding mass transit and ride sharing options available to Corona residents. The contents of such packets shall be approved by the Planning Director and be placed at the model homes sales office.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Prior to home sales

30. The City Engineer shall continue to coordinate with Caltrans regarding the potential for the installation of ramp metering at various on-ramps within the City.

Responsibility to Implement: Public Works Department and Caltrans

Responsibility to Monitor: Public Works Department

Timing: Indefinite

Air Quality

31. During grading on-site, dust suppression measures shall be implemented. These shall include frequent watering of fill material, early paving, and frequent cleaning of haul roads. During construction phases, unpaved dirt roads shall be frequently watered.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Grading operations

32. If deemed appropriate by the City and the local transit district, the project shall provide bus turnouts and bus shelters. These features shall be incorporated into site plans as necessary.

Responsibility to Implement: Developer, Public Works Department and RTA

Responsibility to Monitor: Public Works Department

Timing: Indefinite

33. Development shall include bicycle trails and walkways linking to the City's master planned system, as proposed in the Specific Plan, encouraging non-vehicular travel

Responsibility to Implement: Developer

Responsibility to Monitor: Planning and Public Works Departments

Timing: Tentative tract maps

34. Provision for commercial and quasi-public uses in the Specific Plan will reduce vehicular emissions.

Responsibility to Implement: Accomplished through Specific Plan approval

Responsibility to Monitor: Not required

Timing: Already Completed

35. Additional measures include energy conservation through building design features and traffic signal synchronization through facilities funded by the City's south Corona Funding Program.

Responsibility to Implement: Developer and Public Works Department

Responsibility to Monitor: Building and Public Works Departments

Timing: Building plan check, traffic signal improvement plans

Acoustic Environment

36. A detailed site specific acoustic analysis shall be prepared based on final building pad locations and elevations for each residential planning area. The study shall specify precise locations and dimensions for acoustic barriers to achieve attenuation of present and projected noise so as not to exceed an exterior standard of 65 dB CNEL in private outdoor living areas and an interior standard of 45 dB CNEL in all habitable rooms. Evidence that these standards will be satisfied shall be submitted to the Planning Department and approved prior to recordation of any tract map for development purposes within the planning area. In areas proposed for Theme Wall construction noise mitigation may be accomplished in combination with, or through appropriate design of the Theme Wall.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Tentative Tract Maps

37. Prior to issuance of any residential building permit building specifications describing the acoustical design features of the structures required to satisfy the interior noise standards of 45 dB CNEL shall be submitted to the City Planning Department for approval along with satisfactory evidence which indicates that the sound attenuation measures specified in the approved acoustical report(s) have been incorporated into the design of the project.

Responsibility to Implement: Developer

Responsibility to Monitor: Building Department

Timing: Building plan check

38. Construction adjacent to existing residential development shall be limited to the hours of 7 am. to 6p.m. on Monday through Friday or as otherwise established by City ordinance.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Construction operations

39. Construction of school facilities on-site shall comply with State of California standards for noise attenuation.

Responsibility to Implement: School District

Responsibility to Monitor: School District

Timing: Design and construction of schools

Visual Resources

40. The designated Hillside Overlay Zone shall include all foothill slopes greater than 15 percent.

Responsibility to Implement: Accomplished in specific plan

Responsibility to Monitor: Not required

Timing: Already accomplished

41. Grading of slopes in the southern portion of the site shall provide a smooth and gradual transition between graded slopes and existing grade while also preserving the basic topographic character of the existing site. Variation and combination of slopes shall be used to create a natural character within graded areas. All grading and earthwork activities shall be performed in accordance with the City of Corona Grading Ordinance.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Grading plan check

42. The existing rows of palm and eucalyptus trees bordering roadways within the site shall be retained or relocated to the extent feasible.

Responsibility to Implement: Developer

Responsibility to Monitor: Public Works Department

Timing: Improvement plan check

43. Development shall comply with Community Design Criteria as presented in the Specific Plan.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning and Parks Departments

Timing: Tentative and Final Tract Maps

44. As stipulated in the Specific Plan, prior to or concurrent with approval of a development plan for the hillside overlay zone designated in planning area 26, specific hillside development standards shall be approved by the City Planning Department and shall be adopted to govern development within this area.

Responsibility to Implement: Developer

Responsibility to Monitor: Planning Department

Timing: Tentative Tract Maps for planning area 26

Water and Wastewater

45. The water supply system for Mountain Gate shall be developed in accordance with the Water System Concept of the Specific Plan. Phasing of water system improvements shall be in compliance with the infrastructure phasing plan identified in the Specific Plan. The water transport system plan for the Ranch shall be approved by the City's Utilities Department prior to approval of the Specific Plan.

Responsibility to Implement: Developer

Responsibility to Monitor: Utilities Department

Timing: Tentative and Final Tract Maps

46. Irrigation systems for parks, greenbelts and parkways shall be in accordance with the City's specifications for landscape irrigation.

Responsibility to Implement: Developer and/or Parks Department

Responsibility to Monitor: Parks Department

Timing: Landscape improvement plans

47. Water-efficient plumbing fixtures shall be used on-site as required by State law:

- Low-flush toilets (see Section 17921.3 of the Health and Safety Code).
- Low-flow showers and faucets (California Administrative Code, Title 24, Part 6, Article 1 T20-14)
- Insulation of hot water lines in water recirculating systems (California Energy Commission regulations).
- Self-dosing faucets in all public lavatories (Government Code Section 7800).

Responsibility to Implement: Developer

Responsibility to Monitor: Building Department

Timing: Building plan check

48. Prior to acceptance of the two wells and associated feeder lines by the City as proposed from Foothill Properties, a detailed evaluation shall be completed of the quality of water from the wells and the condition of the facilities. If the facilities are found to be unacceptable the applicant shall either 1) make improvements to achieve a level of acceptability by the City, or 2) participate in other water supply programs available to the City.

Responsibility to Implement: Developer

Responsibility to Monitor: Utilities Department

Timing: Prior to city acceptance of well dedications

49. Prior to approval of the Specific Plan, the wastewater collection system for Mountain Gate shall be approved by the City Utilities Department. Upon acceptance of the proposed sewer plan, the sewer master plan of the CFP shall be amended to reflect changes as proposed by Foothill Properties. Timing and funding of necessary improvements on and off-site shall be in accordance with the Public Facilities Phasing and Financial Plan of the Specific Plan.

Responsibility to Implement: Already accomplished

Responsibility to Monitor: Not required

Timing: Already accomplished

Electricity and Natural Gas

50. All structures shall comply with building standards in Title 24 of the California Administrative Code. Provisions for natural heating and cooling through techniques including but not limited to variable shading, overhangs, clerestory windows, louvers, and energy efficient building orientation should be included in project design to the extent feasible. Energy efficient lighting shall also be used (e.g., high pressure sodium outdoor lighting and fluorescent indoor lighting).

Responsibility to Implement: Developer

Responsibility to Monitor: Building Department

Timing Building plan check

Solid Waste

51. Commercial uses on-site shall comply with State requirements regarding collection and recycling of disposable materials.

Responsibility to Implement: Developer

Responsibility to Monitor: State of California

Timing: Ongoing following commercial development

Police and Fire Services

52. Project architects of future developments shall incorporate defensible space concepts and safety features into the physical design of project developments including the provision of adequate lighting, well-spaced, easy to read housing identification, and on-site circulation that facilitates vehicular patrol of the project.

Responsibility to Implement: Developer

Responsibility to Monitor: Building Department

Timing: Building plan check

53. Due to the demand for additional fire protection service created by this project and adjacent properties, an additional fire station will be required when development occurs outside the service area of the Magnolia Avenue station during later phases of the project.

Responsibility to Implement: City of Corona

Responsibility to Monitor: City of Corona

Timing: Tentative Tract Maps

54. if development of projects outside the 5-minute response time proceeds prior to the operation of a new station within the project area, measures may be implemented which would increase fire protection, such as sprinkler systems, low fuel potential landscaping and other structural means.

Responsibility to Implement: Developer

Responsibility to Monitor: Fire Department

Timing: Tentative Tract Maps

55. A fuel modification plan shall be prepared and implemented for new development in planning areas 5 and 26 which are located adjacent to wild land fire hazard areas, as defined by the Corona Fire Department. Said fuel modification plan shall include clearing or thinning of brush and the installation of irrigated landscaping within 100 feet of any structure in areas abutting native brush land. This fuel modification plan shall be submitted to the Fire Department for approval (Specific Plan, Section 10.6). In addition to this requirement, an ordinance is being prepared that would require a per acre fee for any area within one mile of Cleveland National Forest to provide for specialized fire equipment.

Responsibility to Implement: Developer

Responsibility to Monitor: Fire Department

Timing: Tentative Tract Maps for planning areas 5 and 26

Park and Recreational Facilities and Schools

56. Prior to recordation of each Subdivision Map within Mountain Gate, the project applicant shall provide certification from the Parks and Recreation Department that the applicant has completed proceedings for the purpose of providing park facilities to meet Quimby Act requirements for the subdivision area.

Responsibility to Implement: Developer

Responsibility to Monitor: Parks Department

Timing: Prior to Final Tract Maps

57. Prior to issuance of building permits for residential units, the applicant shall provide evidence of compliance with State law requiring payment of fees to the School District.

Responsibility to Implement: Developer

Responsibility to Monitor: School District

Timing: Prior to building permit issuance

58. The Specific Plan area shall include school sites adequate to meet the needs for providing facilities to the project's student population.

Responsibility to Implement: Already accomplished in Specific Plan

Responsibility to Monitor: Not required

Timing: Already accomplished

Cumulative Impacts

59. The EIR identifies project specific mitigation for many of these impacts.

These impacts have been addressed in their cumulative sense in the previous EIR for the south Corona General Plan Amendment. Appropriate mitigation measures were identified in the prior EIR to reduce the level of impact; however, these impacts cannot be completely eliminated and are unavoidable with implementation of the General Plan for south Corona. Unavoidable impacts were addressed in the Statement of Overriding Considerations adopted by the City along with the south Corona GPA.

4.0 EXISTING CONDITIONS

The Specific Plan for Mountain Gate has been designed to be compatible with the local environmental conditions of the property, the surrounding pattern of land uses, and the economic characteristics of the housing market. The plan is also designed to be consistent with the General Plan and the Community Facilities Plan established by the City Council to guide development in south Corona. This section of the Specific Plan describes the environmental and policy framework which has shaped the development of the plan for Mountain Gate.

4.1 Topography

Mountain Gate lies at the foot of the most northern extension of the Santa Ana Mountains, which divide coastal Orange County from western Riverside County. Most of the ranch lies on an alluvial terrace that begins at the base of the mountains and falls to the north with a gentle slope averaging five percent. Areas of steeper slopes include the hillside area in the southwest part of the site, and the old escarpments which run along both sides of the Main Street wash. Elevations on the site range from 1050 to 1500 feet above sea level. The topographic conditions on the site are shown in exhibit 4.1.

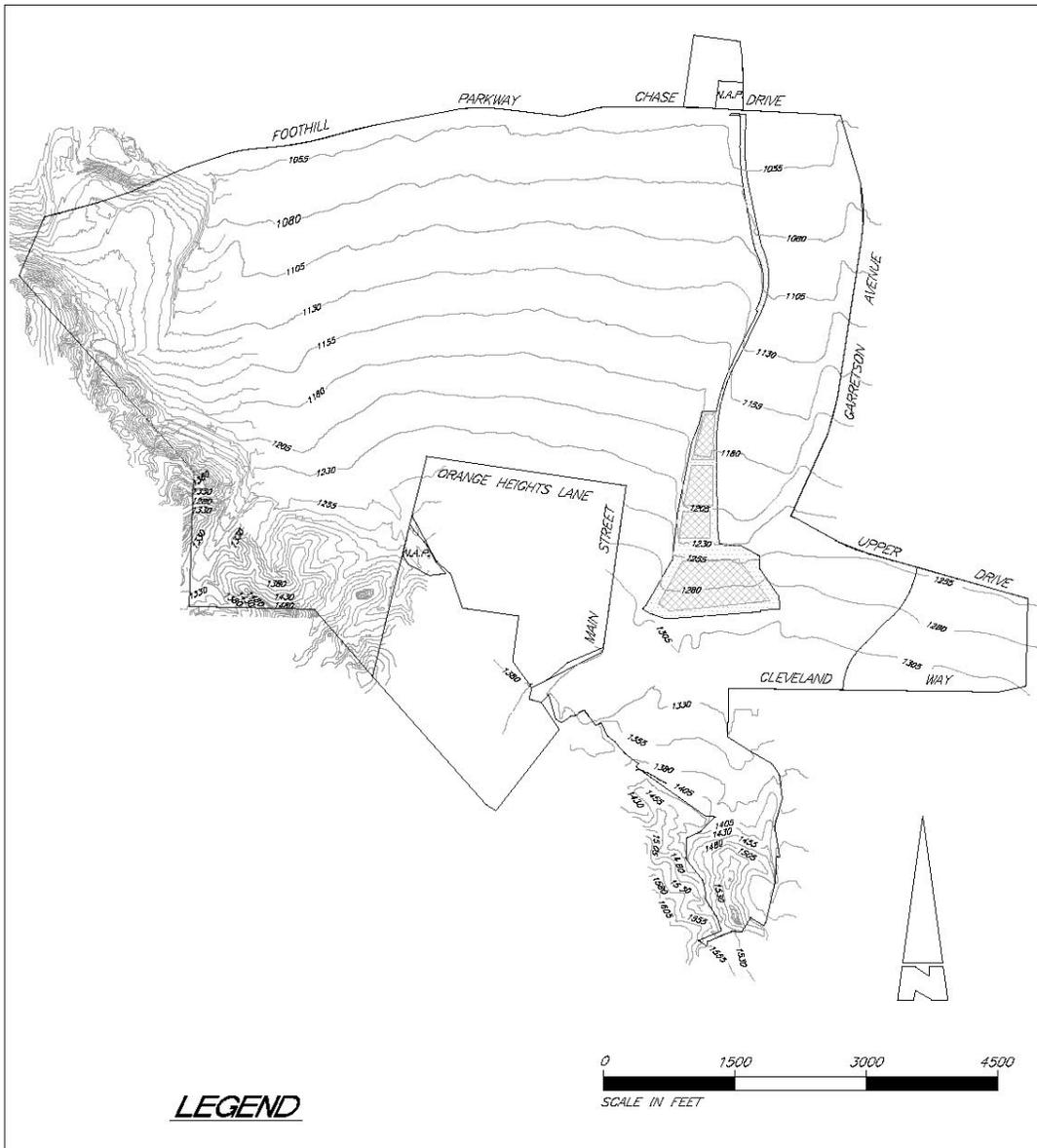
4.2 Geology and Soils

Most of the Mountain Gate site is underlain by late Pleistocene alluvial fan deposits. These deposits consist primarily of gravely sands and silty sands interspersed with gravel and cobbles. The maximum depth of the alluvial deposits exceeds 200 feet. The steeper areas in the southwest part of the site along the foothills are underlain by the Silverado Formation. This Paleocene age formation consists primarily of sandstone, conglomerate, siltstone and shale.

The soils on the site generally exhibit a very low to low expansion potential. Soil conditions present no significant constraints for development. Geological and soils conditions on the site are shown on exhibit 4.2.

As is the case throughout southern California, Mountain Gate is located in a region of generally high seismicity. The site has the potential to experience moderate to high ground shaking during a seismic event. However, secondary seismic hazards such as liquefaction, lateral spreading and seismic-triggered landslides or rockslides are not likely to occur at the site.

Portions of the site at the base of the Santa Ana Mountains are located within an Alquist-Priolo Special Studies fault zone. Fault traces expected to occur within the zone belong to the Whittier-Elsinore fault system. The Elsinore fault and the Chino fault are members of this system, and they cross the southwest part of the site. The Elsinore fault is designated as an active fault by the State of California. At this time, the Chino fault is not designated as a fault-rupture hazard zone by State or County agencies.



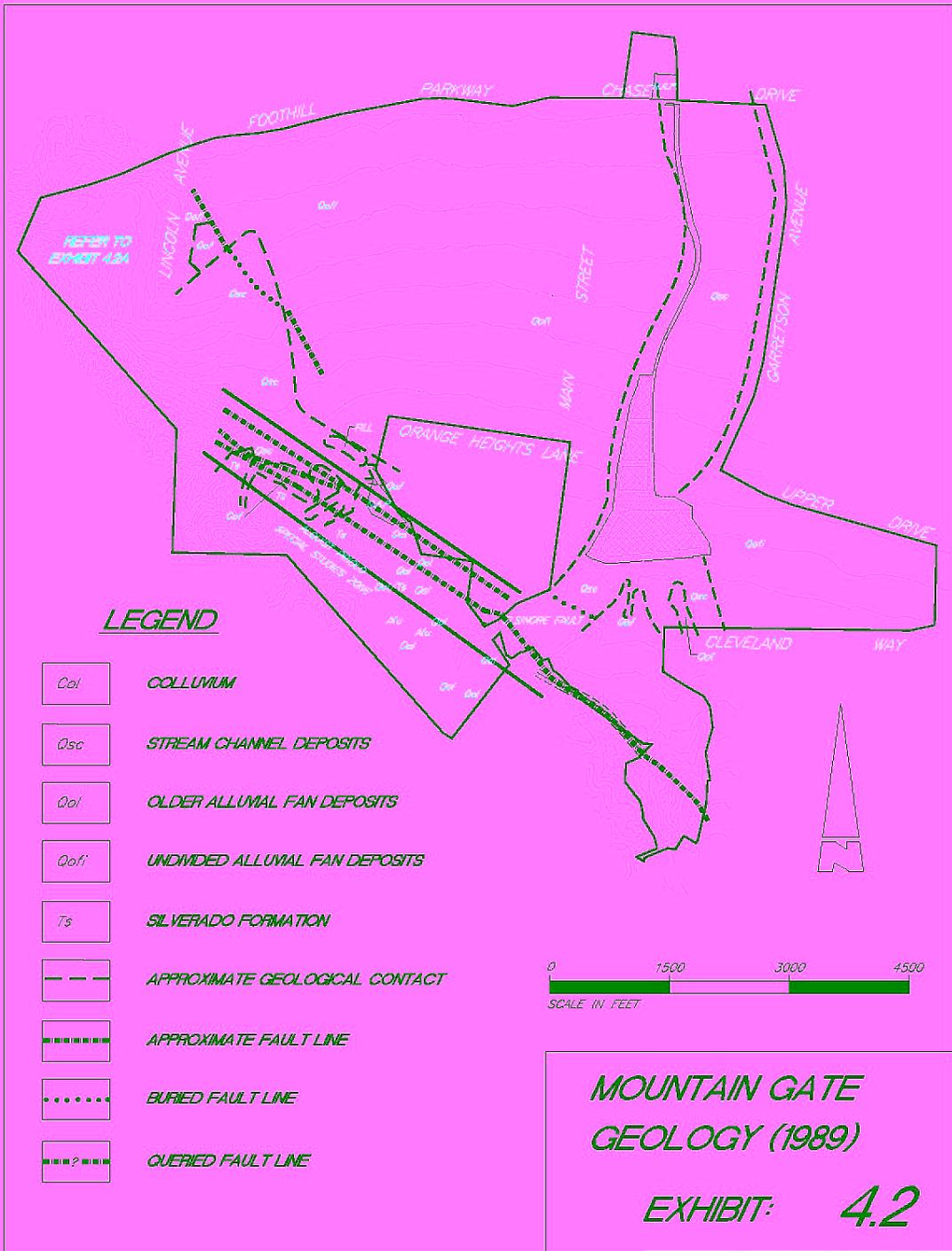
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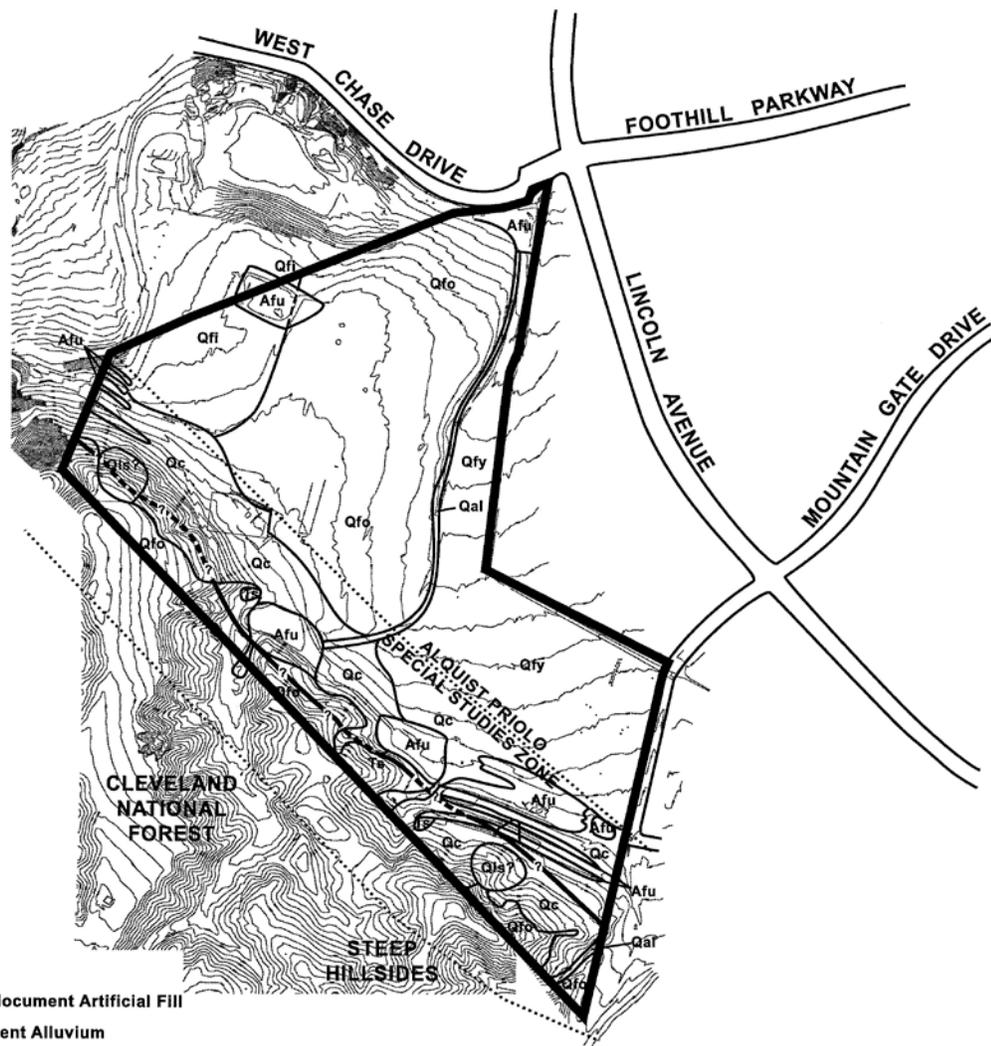
1655 CONTOUR ELEVATIONS

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SCALE IN FEET

**MOUNTAIN GATE
TOPOGRAPHY**

EXHIBIT: 4.1





- | | |
|--|--|
| | Undocument Artificial Fill |
| | Recent Alluvium |
| | Colluvium |
| | Landslide Debris |
| | Younger Alluvial Fan Deposits |
| | Intermediate Alluvial Fan Deposits |
| | Older Alluvial Fan Deposits |
| | Silverado Formation |
| | Approximate Geological Contact, Dotted where Buried, Queried where Uncertain |
| | Approximate Fault Line, Dotted where Buried, Queried where Uncertain |

GEOLOGY (2001: PA 31 & PA 32)

MOUNTAIN GATE

LYON COMMUNITIES INC.



In accordance with City and State requirements, a fault hazard investigation has been conducted on the site. Trenching has been carried out to precisely locate the fault traces, and to establish the required building setback areas along the faults. The report documenting this investigation has been submitted to the City for review and approval.

4.3 Hydrology

Mountain Gate lies within the Santa Ana River Basin. The site is divided into two separate drainage areas. The eastern part of the site is within the Eagle Canyon/Main Street Canyon watershed, which is tributary to the improved Main Street channel. The western part of the site lies downstream of an unnamed canyon, and eventually is tributary to the Lincoln Avenue and Oak Street channel system. The three canyons located upstream of the site encompass a total of approximately 3,100 acres. Hydrologic conditions on the site are shown in exhibit 4.3.

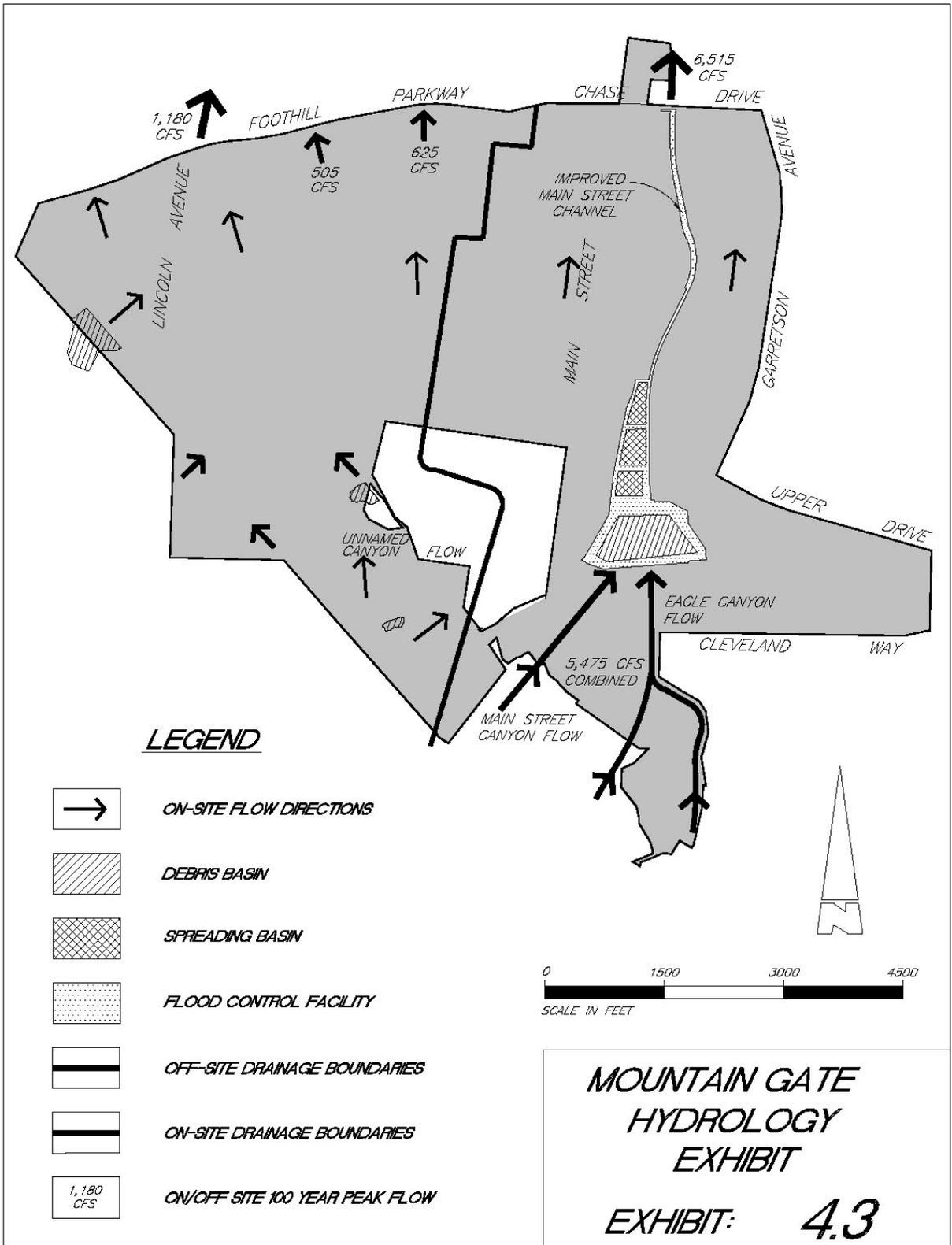
Flows from Main Street and Eagle Canyons are combined just upstream of the existing Main Street debris basin, and discharge into the Main Street Channel. In the event of a major storm, the flows from the unnamed canyon to the west of the site overflow the existing storm drain channels within the property and sheet flow northward towards Foothill Parkway. Bulk flow rates associated with 100-year storm events in each of these drainage basins produce peak discharges of 1,180 cfs at Lincoln Avenue and Foothill Parkway, 505 cfs at Buena Vista and Foothill Parkway, and 625 cfs at Taylor Avenue and Foothill Parkway. With the exception of wash areas upstream from the Main Street debris basin, the site is elevated above the 100-year floodplain.

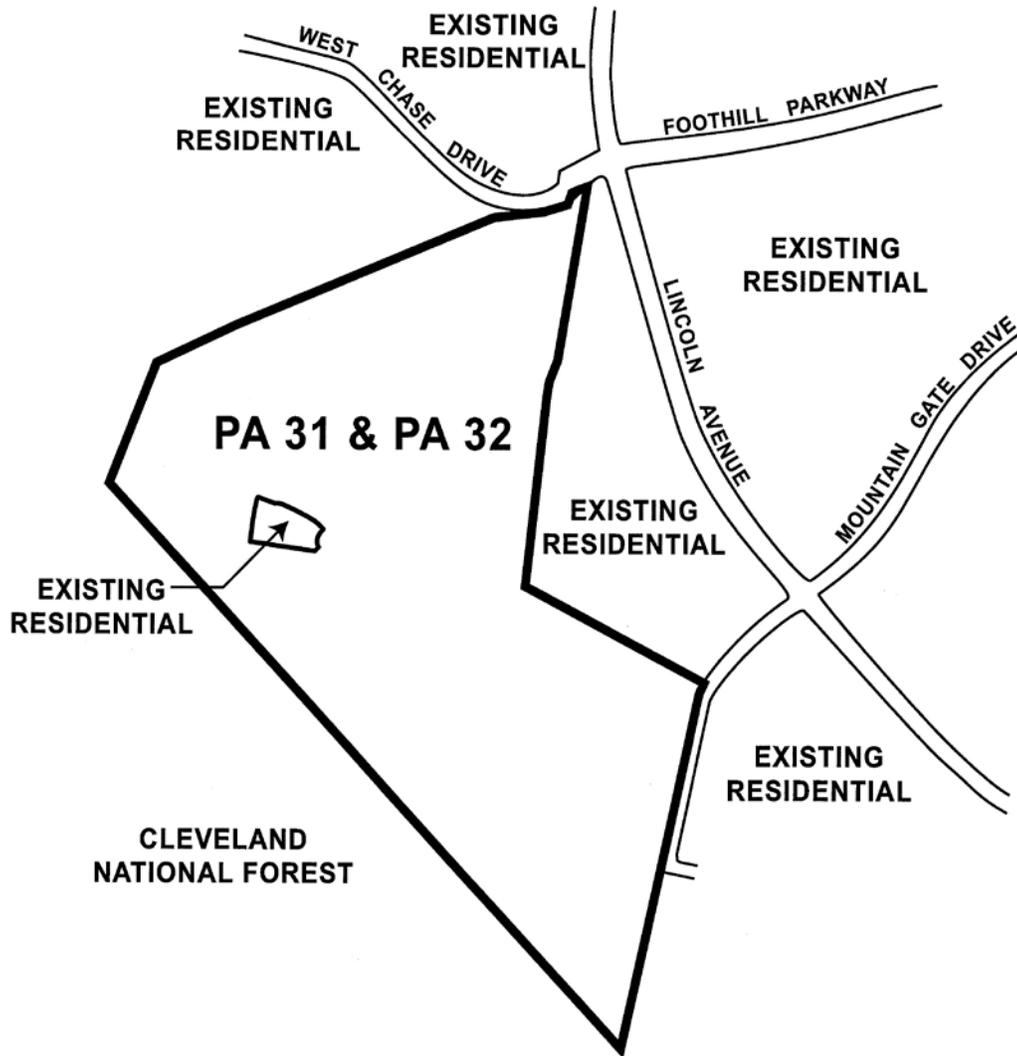
4.4 Biological Resources

The major part of the site is cultivated for the production of citrus, avocados and field crops. Thus, native vegetation is primarily restricted to parts of the foothill areas and along the outwash channels associated with each of the major canyons. In the southwest part of the site, much of the hillside area has been cleared of native vegetation for the expansion of avocado groves.

The limited native vegetation found on the site includes species associated with chaparral, riparian woodland, alluvial fan scrub, oak woodland, and inland sage scrub plant communities. Of these vegetation types, the plant species of the riparian woodland and southern oak woodland are considered to have the most habitat value. Riparian and oak woodland areas are found in the wash area upstream of the Main Street debris basin. No rare or endangered species of plants are found on the site.

Wildlife habitats occur together with each primary native plant community. Areas of the site in agricultural use do not provide significant habitat potential. No mammals or birds found on the site are considered sensitive by any government agencies.





LAND USE (2001: PA 31 & PA 32)

MOUNTAIN GATE

LYON COMMUNITIES INC.



EDAW

EXHIBIT 4.4A

4.5 Land Use

At the time of adoption of the specific plan, existing uses on and surrounding the site were for the most part rural residential and agricultural. In addition to the groves, improvements on the site include the Foothill Properties agricultural headquarters and several areas of employee housing. Related facilities include irrigation reservoirs, pump stations, wind machines, equipment storage and repair yards, sheds, and barns. Existing land uses on the Mountain Gate site and in the surrounding area are shown in exhibit 4.4.

Rural residential uses on surrounding lands include a number of single family homes on lots of one acre or more. Existing areas of rural residential development near Mountain Gate are clustered to the south along Orange Heights Lane and Main Street; to the south along Cleveland Way and Garretson (the Crown Site); and to the northeast along Garretson and Chase Drive east of Garretson. In addition to these clusters of homes, there are also several individual rural residential homes located adjacent to other parts of the site. Smaller lot single family detached subdivisions have also been constructed north of the site along Lincoln and Taylor Avenues.

A subsequent Specific Plan Amendment in 2001 (SPA-01-008) proposes to include additional 120.2 acres of primarily vacant, undeveloped land in the Mountain Gate property. Exhibit 4.4A shows existing land uses on and adjacent to the site at the time of the proposed amendment.

4.6 General Plan and Zoning

South Corona General Plan Amendment

Mountain Gate is part of the larger 5,000-acre area in south Corona for which a General Plan Amendment (GPA 85-6) was adopted by the City Council in 1986. The general plan established a series of four separate villages in south Corona. The general plan permits residential densities ranging from estates at 0-3 dwelling units per acre, to medium density residential at a density of 6-15 dwelling units per acre.

Community Facilities Plan

In June 1988 the City Council adopted the south Corona Community Facilities Plan. The CFP refined the overall land use and traffic circulation pattern that had been adopted in the south Corona General Plan Amendment. The CFP also revised the target densities for each residential category shown in the general plan, to provide a basis for estimating and planning for future services and infrastructure requirements in south Corona.

As the largest contiguous parcel in south Corona under one ownership, Mountain Gate encompasses most of village four as identified in the Community Facilities Plan. Because of this, the site also contains all residential density categories established by the general plan.

A maximum of 3,675 dwelling units are permitted by the CFP for Mountain Gate. The CFP land use designations for Mountain Gate are shown on exhibit 2.1, and the analysis of target densities is presented in section 2.2.1 of this Specific Plan, Consistency with the Community Facilities Plan. The CFP's provisions for master plan road alignments, infrastructure systems, development standards and design guidelines are also summarized in section 2.2 of this document.

Zoning and Agricultural Preserve Status

Existing zoning for the Mountain Gate site at the time of the Specific Plan was Agriculture (A), allowing for single family detached residential development on 5-acre parcels. The site was previously subdivided as Tract No. 14792, which created 165 separate lots consistent with the Agricultural zoning. The major part of the site is also subject to an agricultural preserve contract within the framework established by the Williamson Act. The two agricultural preserve contracts have been noticed for non-renewal, and the contracts expired in December 1990. Existing zoning and the boundaries of the areas subject to the agricultural preserve contracts are shown in Exhibit 4.5.

Subsequent Specific Plan Amendments have included an additional 278.7 acres into the Mountain Gate property. As shown in exhibit 4.5, the zoning for these properties at the time of the amendments was Agriculture (A) — minimum 5-acre lot size. Upon approval and adoption of SPA-08-005, this 25.5- acre property will be included as part of the Mountain Gate Specific Plan (SP-89-1) and designated per the respective land uses in Exhibit 5. 1

4.7 Traffic Circulation

Regional access to south Corona and Mountain Gate is provided by the 91 Freeway and by Interstate 15. The arterial, collector, and local streets which provide access to the site are shown in exhibit 4.6 and are described below.

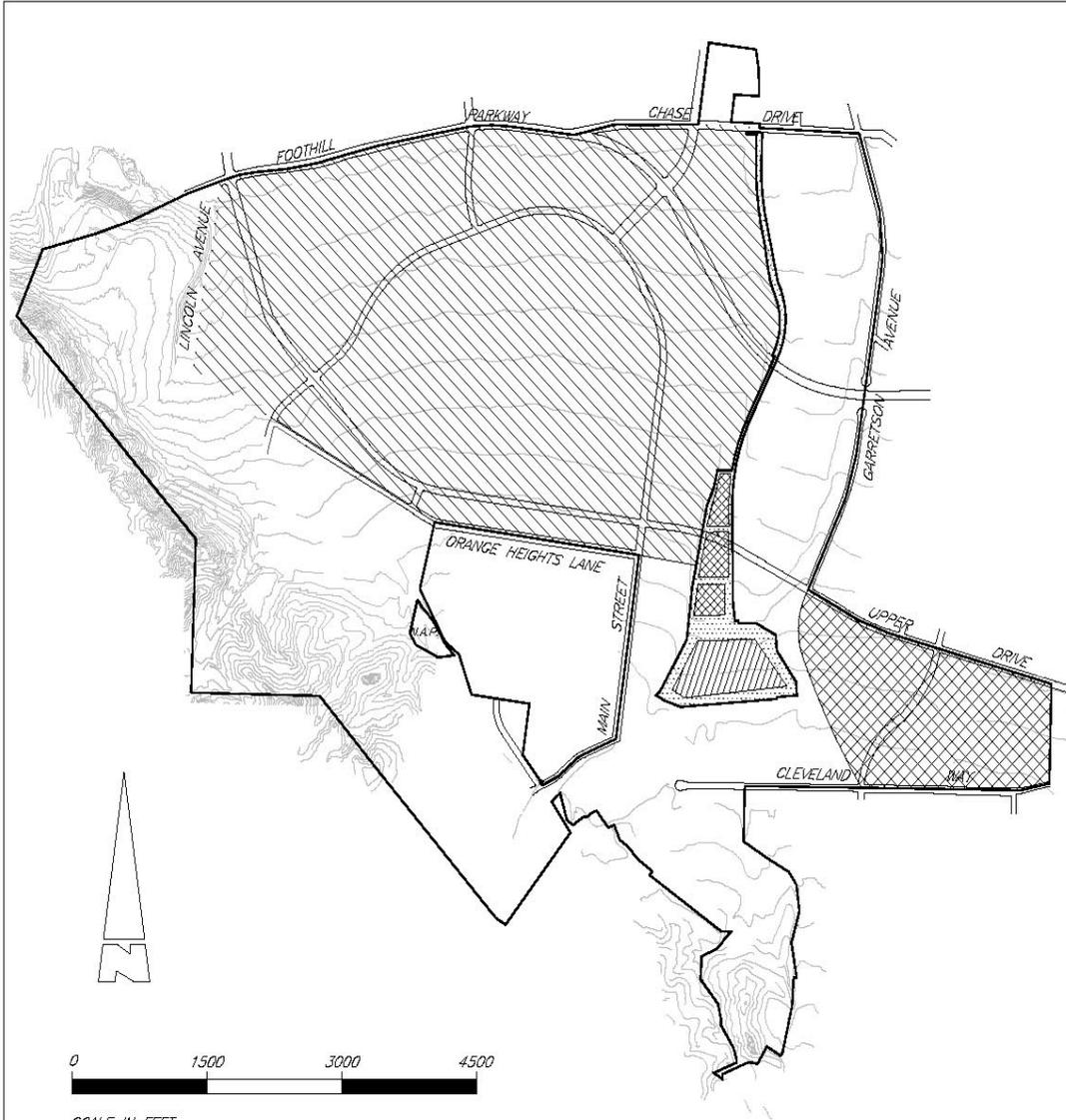
Lincoln Avenue: This north-south roadway is a four-lane arterial between Ontario Avenue and 6 Street, but narrows to two lanes as it crosses over the 91 Freeway where an interchange is located. South of Ontario, Lincoln is a two lane roadway with partial widening adjacent to several developed properties. Lincoln Avenue provides access to the freeway and the developed areas of Corona from the westerly part of Mountain Gate.

Main Street: Main street provides access north from the site to central Corona and the 91 Freeway. Main Street is currently an existing two-lane arterial northerly to Eighth Street, with four lanes north from there to the 91 freeway.

Ontario Avenue: This east-west street is classified as a four-lane arterial, with current improvements varying from two to four lanes. Ontario Avenue is the major east-west roadway in south central Corona, and provides access to the I-15 freeways.

Magnolia Avenue: Magnolia Avenue provides access northeasterly from the intersection of Ontario Avenue and Garretson Avenue to the I-15 Freeway. Main Street, Ontario and Magnolia Avenues form the shortest rout from mountain Gate to the existing freeway system. Almost all of Magnolia has been fully improved between the freeway and Ontario Avenue.

Garretson Avenue: This two-lane road provides north-south access the eastern edge of the site. A cluster of single family homes with direct driveway access has been built along Garretson north of Chase Drive.

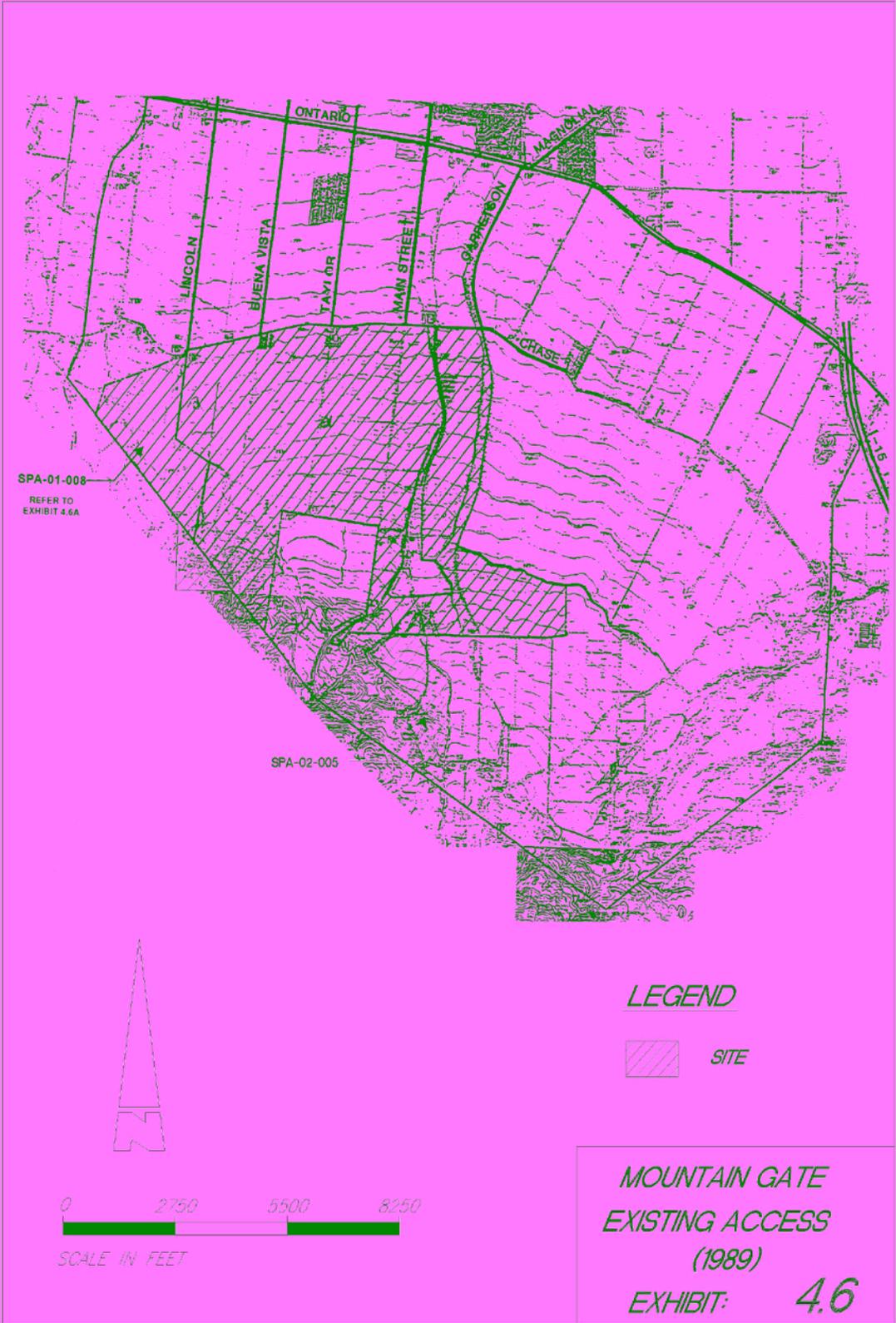


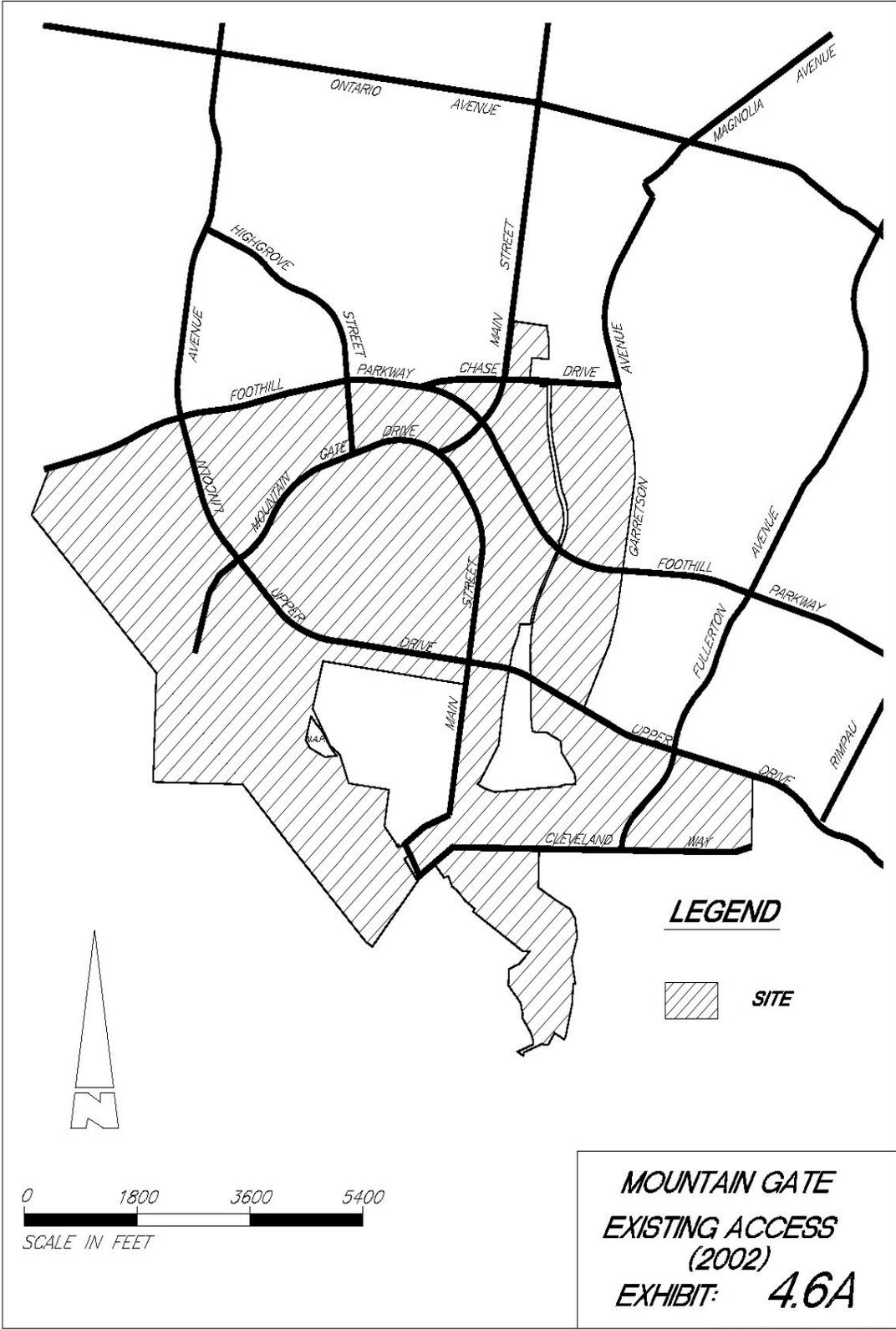
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 CONTRACT IDENTIFICATION
 ACREAGE
 EXPIRATION DATE</p> | <p>AG 72-1
 78.9 AC.
 1990.</p> |

**MOUNTAIN GATE
 ZONING /
 AGRICULTURE**

EXHIBIT: 4.5





LEGEND

 **SITE**

**MOUNTAIN GATE
EXISTING ACCESS
(2002)
EXHIBIT: 4.6A**

© [DATE] [FIRM] [REV. 5/04] CHARTS/LEAD-4.6A.DWG 1/3/2008

Chase Drive: This two-lane east-west road presently is only partially improved, and has no curbs, gutters or sidewalks. A duster of single family homes with direct driveway access has been built along Chase Drive east of Garretson.

4.8 Archaeological Resources

An archaeological records search and field investigation has been conducted for Mountain Gate. This research revealed no identifiable prehistoric sites or artifacts. This lack of sites or artifacts is attributed to prehistoric cultural practices, and to the many years of agricultural production on the site.

4.9 Utilities

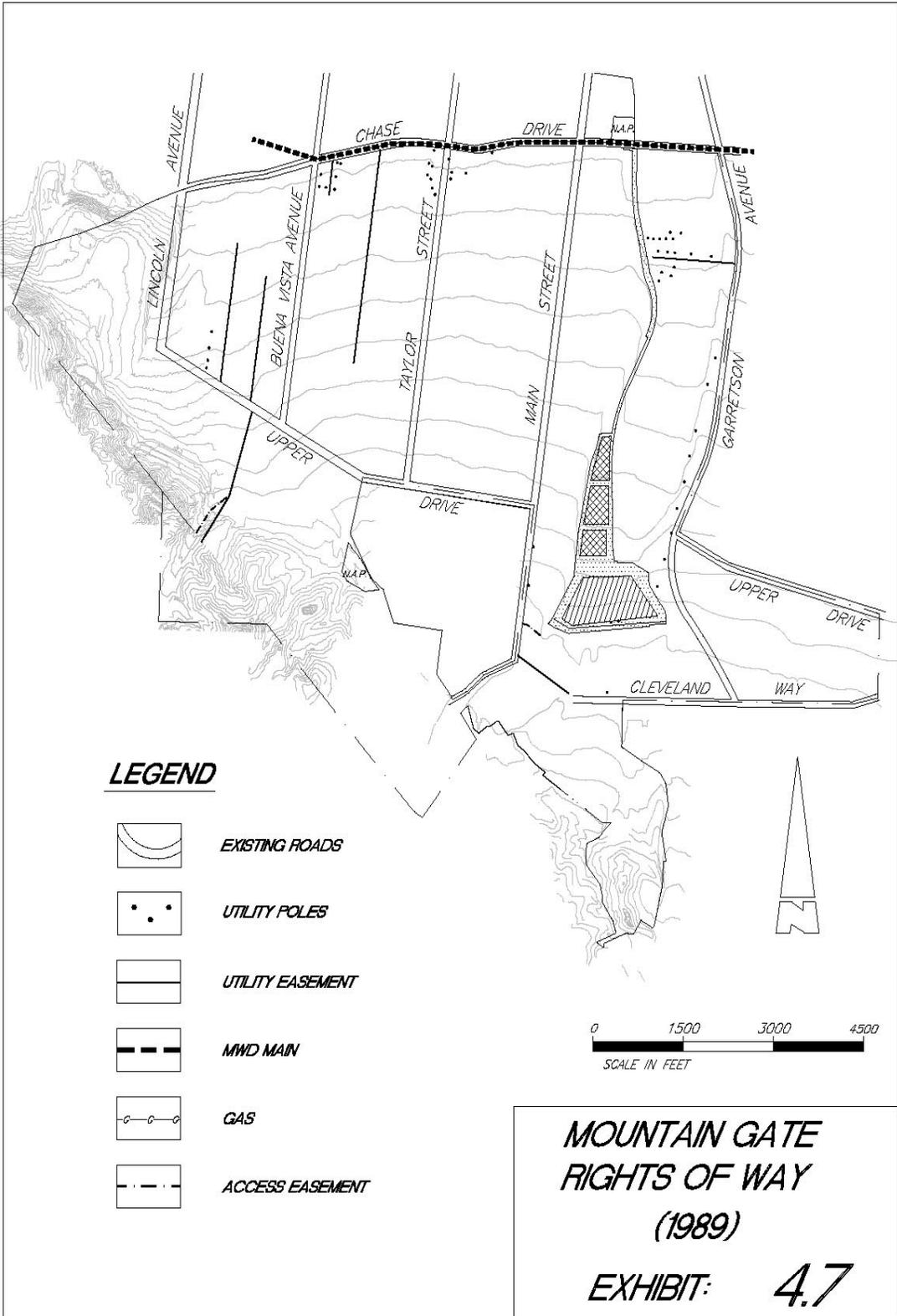
Public utilities that will provide service to Mountain Gate are indicated in Table 4.1. Master plans for the extension of water and sewer to serve the site are presented in chapter 7 of this Specific Plan. Plans for the extension of electricity, natural gas and telephone service will be coordinated with the utilities.

**Table 4.1
Public Utilities Serving Mountain Gate**

Utility	Servicing Agency
Sanitary sewer	City of Corona
Water	City of Corona
Electricity	Southern California Edison
Natural gas	Southern California Gas
Telephone	Pacific Telephone

4.10 Rights-of-Way/Easements

Major rights-of-way, easements, and encumbrances affecting Mountain Gate are depicted in exhibit 4.7; additional easements exist which are not shown on the exhibit. Existing rights-of-way, easements and encumbrances on Mountain Gate include public street rights-of-way offered for dedication as part of Tract 14792, as well as various utility, drainage and access easements. When Tract 14792 was recorded, the City of Corona accepted the offer of dedication for the improved existing roads on the site. Other road rights-of-way shown on Tract 14792 were not accepted by the City. Existing street rights-of-way will be vacated or relocated, as may be required, to allow for development of the street system presented in this Specific Plan. This is proposed to be accomplished through right-of-way vacations and through the recordation of a new subdivision map which will supersede Tract 14792.



LEGEND

-  *EXISTING ROADS*
-  *UTILITY POLES*
-  *UTILITY EASEMENT*
-  *MWD MAIN*
-  *GAS*
-  *ACCESS EASEMENT*

**MOUNTAIN GATE
RIGHTS OF WAY
(1989)**

EXHIBIT: 4.7

Existing utility easements generally do not present significant constraints to development, and will be vacated or relocated as may be required to accommodate development activities. The exception to this is Pacific Bell's fiber optic cable system in existing Main Street, Lincoln Avenue and portions of Orange Heights Lane. The Specific Plan has been designed to minimize the potential need for relocation of this improvement. Access easements to individual residences to the south of the property will be respected.

4.11 Summary of Site Features and Conditions

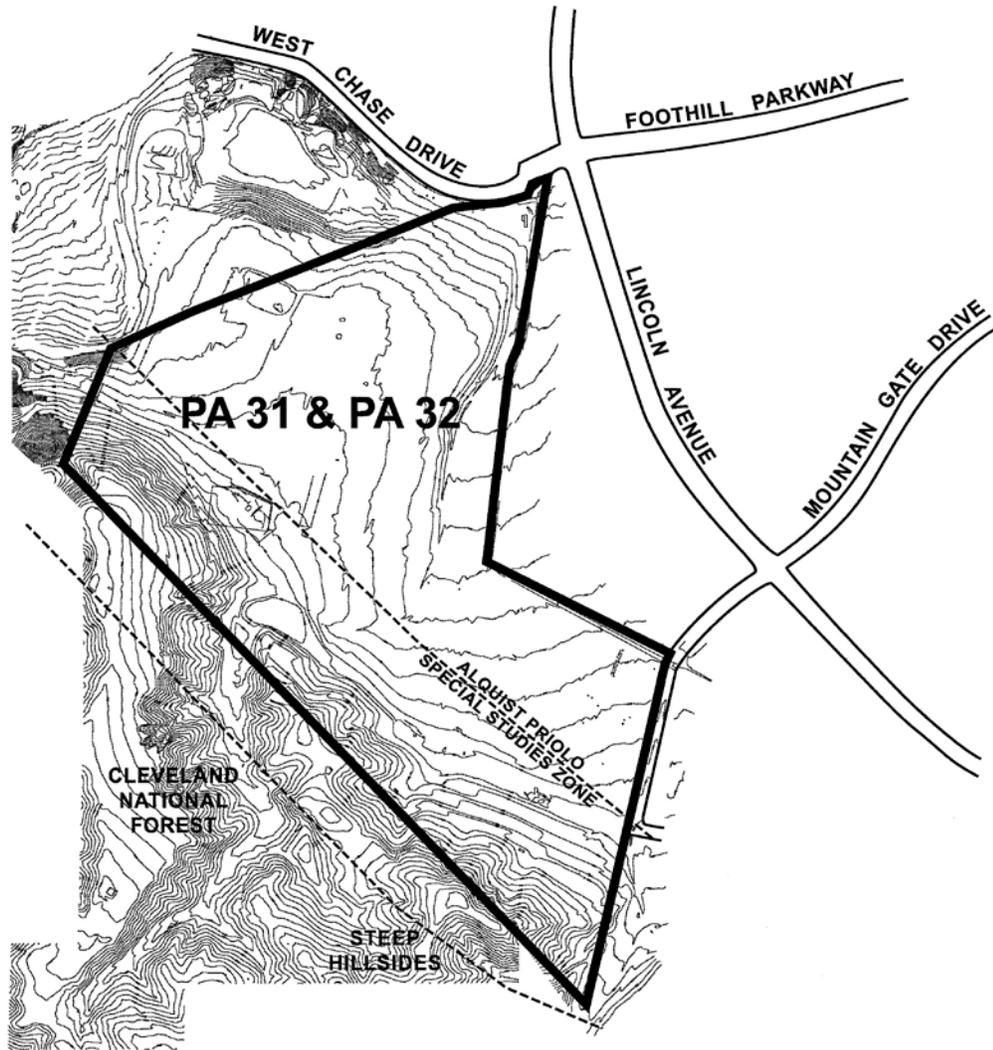
Exhibit 4.8 presents a summary of the site features and existing conditions that characterize Mountain Gate and adjacent properties. These features and conditions represent both constraints and opportunities for development of the site.

Opportunities for Development

- The gentle topography of the site means that minimal grading will be required to construct roads and streets. Gentle grades also allow for the development of a variety of housing types.
- No significant stands of rare or endangered plants or wildlife habitat are found on the site. Riparian woodland and southern oak woodland areas in the upper Main Street wash could enhance the visual quality and amenity value of the open space in the wash.
- Main Street, Lincoln Avenue, and the proposed new Foothill Parkway arterial highway will provide good access and traffic circulation capacity connecting the site with the developed areas of Corona and the freeway system.
- The adequate capacity of the existing Main Street channel and debris basin minimizes the need for off-site drainage improvements to serve the easterly part of the site. Opportunities to construct new storm water basins make it possible to reduce downstream run-off in the largely unimproved Lincoln Avenue and Oak Street storm drain systems.
- The terrain offers excellent views to the north of the city, the Santa Ana river valley, and the west San Bernardino valley. The site also offers excellent views of the hillsides and canyons of the Santa Ana Mountains. These view amenities present opportunities to design view-oriented residential neighborhoods.

Constraints for Development

- The hillside area in the southwest part of Mountain Gate contains steep slopes and a potentially active fault trace. These will require careful site planning and grading design for proper development of this area.



SITE SUMMARY (2001: PAs 31 & 32)
MOUNTAIN GATE
LYON COMMUNITIES INC.



ED&A

EXHIBIT 4.8A

Chapter 4.0 Existing Conditions

- The Main Street wash itself and the steep slopes that exist along the escarpment that follows the wash are constraints on the design of road crossings, and site planning and grading must take into account the grade changes in this area.
- Existing roadway rights-of-way and utility easements need to be vacated or relocated to fit proposed development plans.
- Major off-site water system improvements will be required in order to start development of Mountain Gate.
- Major off-site drainage improvements and the construction of basins will be required for development of the westerly part of the site.

5.0 LAND USE PLAN

The Land Use Plan for Mountain Gate is shown in Exhibit 5.1. The plan proposes a maximum of 3,593 homes within the site, in eight different residential density categories. The plan also proposes 15.1 acres of commercial development; 15.8 acres of quasi-public uses; one 10.3 - acre elementary school site; one 21.7 -acre intermediate school site; 41.3 acres of park sites; and 24 acres of open space. Table 5.1 presents a statistical summary of the Land Use Plan. Table 5.2 presents a detailed allocation of dwelling units among the planning areas of the Specific Plan.

5.1 Residential Land Uses

The Specific Plan establishes six land use categories for single family detached development. The ER 1 and ER 2 categories are intended to accommodate residential estates at gross densities of one unit per acre and two units per acre, respectively. These designations provide for density transition zones adjacent to existing estate residential homes.

The SFD land use designations are intended for development of single-family detached residential neighborhoods at a density range of 0-4 du/ac, depending on the General Plan designation of the property. The SFD-14.4 category is intended to accommodate residential development with a minimum lot size of 14,400 square feet, the SFD-9.6 category is intended to accommodate residential development with a minimum lot size of 9,600 square feet, and the SFD-7.2 category with a minimum lot size of 7,200 square feet. The range of densities is designed to permit the various neighborhoods to be differentiated from one another through lot area, lot width, streetscape character, architectural style, pricing and amenity-orientation. In this way, each enclave of single family detached homes can be given its own neighborhood character.

The Estate Residential Cluster designation is a special category applied to planning areas 5 and 26 in the southern portion of Mountain Gate. This designation is intended to permit the use of clustering of single family detached and single family attached homes together with compensating open space. This will allow the design of a “planned unit development” with internal open space in each of these areas. Consistent with the CFP, the overall density within each of these planning areas is limited to three units per gross acre. Within each Estate Residential Cluster area, larger estate lots and/or landscaped buffer zones will be utilized to achieve compatible transitions to adjacent estate residential areas. Specific development concepts will be developed for each of the Estate Residential Cluster areas concurred with the preparation of subdivision maps for these areas.

The SFA Single Family Attached Designation is intended to accommodate duplex homes. The single family attached enclaves are located within or adjacent to the village core. The densities for the SFA areas are designed to accommodate high-end, amenity-oriented duplex homes that will allow for the creation of a single family character in each neighborhood. The densities in the SFA areas will permit lot widths that can support distinctive architectural designs. To further enhance the character of the SFA neighborhoods, the development standards require builder-installed front yard landscaping.

**TABLE 5.1
Land Use Plan Summary**

CFP	Specific Plan Use	Description	Total Acres	Total Units	% of Acres	% of Units
E	ER 1	Estate Residential 1 Unit per Acre	22.9	23	2.3%	0.6%
E	ER 2	Estate Residential 2 Units per Acre	72.9	57	7.3%	1.6%
E	ER Cluster	Estate Cluster (*) 3 Units per Acre	290.2	559	29.1%	15.6%
E	SFD-14.4	Single-Family Detached 14,400 sq. ft. lots	75.0	52	7.5%	1.5%
L	SFD-9.6	Single-Family Detached 9,600 sq. ft. lots	96.2	238	9.7%	6.7%
L	SFD-7.2	Single-Family Detached 7,200 sq. ft. lots	171.9	602	17.2%	16.8%
LM	SFA	Single-Family Attached Duplexes, 6-8 Units per Acre	227.2	1,452	22.8%	40.5%
M	MDR	Medium Density 10-15 Units per Acre	40.0	600	4.0%	16.8%
RESIDENTIAL SUBTOTAL			996.3	3,583		
C	C	Commercial	15.1	0	1%	
OP	QP	Quasi-Public(Office and Restricted Commercial	15.8	0	1%	
LM	ES	Elementary School(**)	10.3	10	1%	
LM	IS	Intermediate School	21.7	0	2%	
LM/M	P	Community, Historic and Neighborhood Parks(*)	30.0	0	3%	
M	GP	Greenbelt Park	11.3	0	1%	
L/E	OS	Open Space	24.0	0	2%	
L	Fire Station Site		1.3	0	1%	
	Arterial and Collector Roads		79.6	0	7%	
PROJECT TOTAL			1,205.4	3,593		

(*) The Estate Cluster area includes a requirement for additional open space, which may include additional park land, should the City Council decide to accept such park land.

(**) Ten (10) dwelling units are reserved for the Elementary School site in the event the site is not acquired for school purposes.

Chapter 5.0 Land Use Plan

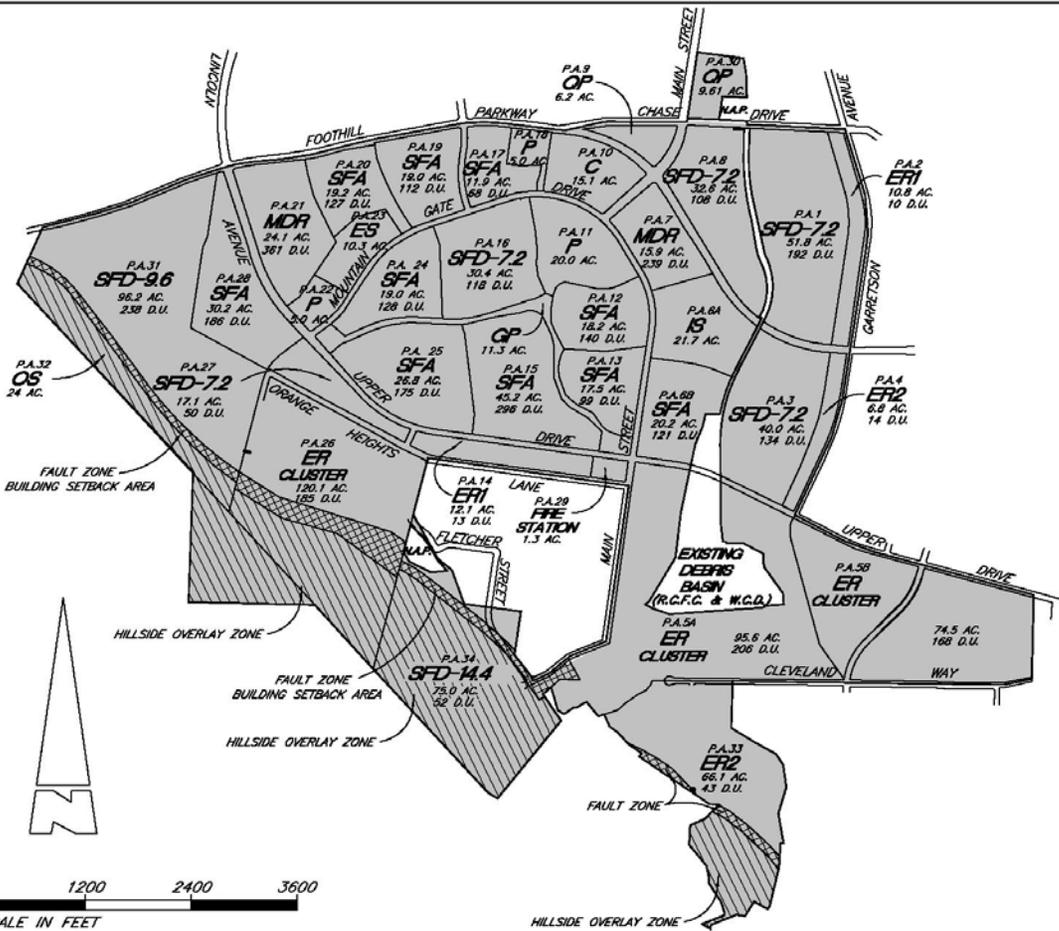
**TABLE 5.2
Allocation of Dwelling Units among Planning Areas**

Planning Area	Community Village-Area	Facilities Plan Land Use	Specific Plan Land Use	Approximate Gross Acres	DU Objectives	Dwelling Unit Low (90%)	Range High (110%)
1	4-220	L	SFD-7.2	51.8	192	172	211
2	3-230	E	ER 1	10.8	10	9	11
3	4-320	LM	SFD-7.2	40.0	134	121	147
4	3-341	LM	ER 2	6.8	14	13	15
5A	4-333	E	ER CLUSTER	95.6	206	185	227
5B	3-400 / 410	E	ER CLUSTER	74.5	168	151	185
6A	4-320	LM	IS	21.7	0	0	0
6B	4-320	LM	SFA	20.2	121	109	161
7	4-320	M	MDR	15.9	239	215	239
8	4-220	L	SFD-7.2	32.6	108	97	119
9	4-092	OP	QP	6.2	0	0	0
10	4-300	C	COMMERCIAL	15.1	0	0	0
11	4-310	P	PARK	20.0	0	0	0
12	4-310	LM	SFA	18.2	140	126	146
13	4-310	LM	SFA	17.5	99	89	109
14	4-331	L	ER 1	12.1	13	11	14
15	4-310	LM	SFA	45.2	296	268	328
16	4-290	LM	SFD-7.2	30.4	118	106	130
17	4-300	LM	SFA	11.9	68	61	75
18	4-300	P	PARK	5.0	0	0	0
19	4-280	LM	SFA	19.0	112	101	123
20	4-280	LM	SFA	19.2	127	114	140
21	4-280	M	MDR	24.1	361	325	397
22	4-280	P	PARK	5.0	0	0	0
23	4-280	ES	ES	10.3	10	9	11
24	4-290	LM	SFA	19.0	128	115	141
25	4-290	M	SFA	26.8	175	161	194
26	4-332	E	ER CLUSTER	120.1	185	166	203
27	4-274	LM	SFD-7.2	17.1	50	45	55
28	4-272	LM	SFA	30.2	186	168	206
29	4-280	FS	FS	1.3	0	0	0
30	4-000	OP	QP	9.6	0	0	0
31	4-271/272/273	E/L/LM	SFD-9.6	96.2	238	220	256
32	4-273	E/L	OS	24.0	0	0	0
33	4-421	E	ER 2	66.1	43	40	97
34	4-332	E	SFD-14.4	75.0	52	47	57
SUBTOTAL				1,114.5	3,593		
	4-290/310	P	GP	11.3			
	ROADS / LANDSCAPE EASEMENT			79.6			
TOTAL				1,205.4	3,593		

The total number of dwelling units in the Specific Plan area may not exceed 3,593 units.

Areas 5A and 5B are shown separately solely for comparison to the CFP village boundaries. For the purpose of dwelling unit allocation and possible density transfers, areas 5A and 5B shall be treated as a single planning area 5, with a dwelling unit objective of 374 units, and a dwelling unit range of 336 to 412 units.

Density Transfers: 7 units transferred from PA 33 to PA 5 per SPA05-006



STATISTICAL SUMMARY

LAND USE CATEGORY	ACRES	DWELLING UNITS
ER1 ESTATE RESIDENTIAL 1 UNIT PER GROSS ACRE	22.9	23
ER2 ESTATE RESIDENTIAL 2 UNITS PER GROSS ACRE	72.9	57
ER3 ESTATE CLUSTER 3 UNITS PER GROSS ACRE	290.2	559
SFD14.4 SINGLE FAMILY DETACHED 14,400 SQ. FT. MIN. LOT	75.0	52
SFD9.6 SINGLE FAMILY DETACHED 9,600 SQ. FT. MIN. LOT	96.2	238
SFD7.2 SINGLE FAMILY DETACHED 7,200 SQ. FT. MIN. LOT	171.9	602
SFA SINGLE FAMILY ATTACHED	227.2	1,452
MDR MEDIUM DENSITY RESIDENTIAL	40.0	600
IS INTERMEDIATE SCHOOL	21.7	0
ES ELEMENTARY SCHOOL	10.3	10
P PUBLIC PARKS	30.0	
C COMMERCIAL	15.1	
QP QUASI PUBLIC	15.8	
FS FIRE STATION	1.3	
GP GREENBELT PARK	11.3	
OS OPEN SPACE	24.0	
ROADS/LANDSCAPE EASEMENT	79.6	
TOTAL:	1205.4 AC.	3,598 DU.

**MOUNTAIN GATE
LAND USE PLAN**

EXHIBIT: 5.1

The MDR Medium Density Residential designation is proposed for two planning areas: one at Main Street and Foothill Parkway, and one at Lincoln Avenue and Foothill Parkway. These areas are intended for the development of town homes. The two medium density areas adjacent to Foothill Parkway reflect an extension of the village core outward from Mountain Gate Drive.

5.2 Commercial and Quasi-Public Land Uses

The Land Use Plan designates 15.1 acres at the southwest corner of Foothill Parkway and Main Street, located in planning area 10, for neighborhood commercial development. On the northwest corner of this same intersection, the plan designates planning area 9 as a “Quasi-Public” site covering 6.2 acres. The intersection where these uses will be located is the primary entry to the Mountain Gate planned community. An additional, 9.6 acres at the northeast corner of Main Street and Chase Drive, in planning area 30, are designated for “Quasi-Public” uses. Planning area 30 is not subject to the Mountain Gate phasing plan and will be developed individually with separate engineering and infrastructure requirements.

The commercial site is planned to meet a range of neighborhood-serving, daily shopping needs. Typical uses would likely include a supermarket, convenience store, drugstore, and various small retail and personal services establishments.

The purpose of the quasi-public site is to establish a location for government or other public uses that are necessary to serve the day-to-day needs of the residents. The range of uses which might be provided on this site include a fire station, a post office, a branch library, a government center, child care facilities, a home for the aged, a church, or professional offices.

The commercial land use designation for planning area 10 is based upon the recommendations of a commercial market analysis which addressed the need for additional commercial development in south Corona, and also the specific commercial development potential of this particular site. This commercial market analysis reached the following conclusions:

1. The CFP land use plan for south Corona provides limited opportunities for commercial development which are concentrated along Ontario Avenue. The commercial areas designated on the CFP are not adequately distributed within south Corona to support the local need and demand for convenient neighborhood shopping throughout the area.
2. The addition of a neighborhood commercial area in Mountain Gate is desirable to provide convenient daily shopping for the future residents of the south part of south Corona.
3. There will exist sufficient demand for neighborhood commercial uses in south Corona to support the proposed commercial development on the Mountain Gate site as well as the proposed commercial development on Ontario Avenue.
4. The specific site proposed, at the corner of Main Street and Foothill Parkway, is ideally located to provide highly accessible and convenient commercial access for residents of the immediate area, and also for residents of other areas in south Corona traveling along Main Street and Foothill Parkway. Future traffic volume projections for this intersection fit the typical profile required for successful convenience commercial development.

The Specific Plan establishes development regulations for the commercial and quasi-public districts that will ensure an appropriate mix of commercial, quasi-public and office uses consistent with the planned residential character of the surrounding area. The CFP further establishes standards and guidelines for architecture, building and parking setbacks, landscaping and design to ensure the quality of future commercial development.

In the event that Planning Area 9 (Quasi-Public District) is developed with other than public, non-profit facilities, the underlying residential units may not be assigned to other planning areas within the Specific Plan, and are thereby considered transferred to the City for reallocation through the target density provisions of the south Corona Community Facilities Plan.

5.3 Park, Greenbelt, School and Fire Station Sites

Park, greenbelt and school sites are a central feature of the family-oriented; amenity oriented development concept for the Mountain Gate. For public safety, 1.3 acre fire station site is designated for Planning Area 29, at the corner of Main Street and Upper Drive.

The Specific Plan designates planning area 23 as a 10.3 acre elementary school site adjacent to the village core. This site is located along Mountain Gate drive, immediately adjacent to a proposed five-acre neighborhood park site. This site is consistent with the criteria for school site location as established in the CFP. This school site will be easily accessible from the bicycle paths to be constructed in the greenbelt. Staffs of the Corona Norco Unified School District have indicated that this school site appears to meet the district's preliminary requirements.

The school district staff has also expressed an interest in planning area 6A as a possible junior high school site. This site is located in one of the earlier phases of development, and is accessible from Main Street.

Sections 10.4 and 10.5 of this Specific Plan present more detailed information and drawings, which depict the proposed parks, greenbelts and trails system for Mountain Gate. A summary of this system is presented in the following paragraphs.

The Land Use Plan for Mountain Gate identifies four park sites with a total area of 41 to 45 acres. Additional open space areas are proposed within the two estate residential cluster areas. Each of these parks would be distinctive in design and function:

Community Park Consistent with the CFP, 20 acres of land in planning area 11 are designated as a community park site. This park will be both highly visible and highly accessible at this location where the Main Street entrance to the community ends at the Mountain Gate Drive. The community park is planned to accommodate a community recreation center, a community pool, sports fields, court sports, play equipment, picnic areas, and trails. Off-street parking and restrooms will also be provided.

Neighborhood Park Consistent with the CFP, 5 acres of land in planning area 22 are designated as a neighborhood park site. This park is located at the intersection of Lincoln Avenue and Upper Drive with Mountain Gate Drive. A concept plan for this park is shown in Exhibit 10.9 in the Community Design chapter of this plan. The park is immediately adjacent to the elementary school site, and is intended to meet the neighborhood recreation needs for

the western part of village four in the south Corona plan. The park will be developed as a joint use park/detention facility. The park will include sports fields; open play areas, trails and picnic areas in the lower detention basin area of the site. Court sports, play equipment and restrooms will be located on the higher part of the site above the detention area.

Greenbelt Park The plan designates 11 to 15 acres of land within the village core as a greenbelt park. The Greenbelt Park will provide an off-street bicycle and pedestrian path that will link together the residential neighborhoods with the community and neighborhood parks and the elementary school. The two legs of the Greenbelt Park will begin at the edges of the village core, where landscaped recreation areas will be provided at the intersections of Mountain Gate Drive with Lincoln Avenue and Upper Drive. As the greenbelt extends through the village core, a series of recreation areas will be improved with facilities including picnic areas; children's play equipment, and a volleyball court. Along the length of the greenbelt, cul-de-sacs in the residential neighborhoods will open onto the greenbelt, providing access for the residents, views of the greenbelt open space landscaping, and convenient access for police security. The recreation facilities and continuous trail system in the Greenbelt Park are central elements in the family-oriented, amenity-oriented community concept for Mountain Gate.

Historic Park A five-acre historic park site is proposed in planning area 18, at the intersection of Foothill Parkway and Taylor Avenue. A number of structures that are part of the agricultural history are still found on the site. These include a headquarters complex, the ranch manager's house and original agricultural worker housing. The site complex as a whole is historically significant because it is the best preserved and the largest example of functioning citrus site in the region.

The historic park is intended to illustrate, through the preservation of buildings and artifacts, the important role of citrus farming in the growth and development of Corona. Located at the site of the site headquarters, the historic park site anticipates the preservation and reuse of several of the existing structures for a museum, branch library, recreational activities or other civic purposes. Additionally, the park will offer the opportunity to relocate other historic structures from within the City. Adjacent citrus groves could be preserved within the boundaries of this park. A concept plan of the Foothill Ranch Heritage Park is shown on page 101A. The park would be connected to the village Loop Road and the 15-acre community park via a trail to be established along the westerly boundary of the proposed 15-acre commercial center. The historic park and this trail would also serve to connect the community park and Mountain Gate trail network with the planned off-street bicycle trail designated in the CFP for Chase Drive, Foothill Parkway, and the MWD right-of-way.

The Phasing chapter of this plan proposes that the historic park be dedicated during the final phase of site development, when the last agricultural operations are ended. Through the mutual agreement of the city and landowner, aspects of the historic park may be implemented earlier. It is anticipated that a non-profit organization will operate the proposed Heritage Park and may include limited retail or special events to provide financial support for the park operations.

Additional open space lands will be provided in planning areas 5 and 26, to the extent that development in these areas makes of the "estate cluster" provisions.

New Citrus Grove Area
Made Vacant by Bldg.
Removal - Depicting
Fruit Production Methods
From 1800's to Present

Shrub & Groundcover
Planted on Slope

New Concrete Walk

Gallery House

Gift House

New Parking +/- 122
Cars Total on Site

Heritage Museum

Curator Annex

Standpipe Fountain

Overflow Parking

OCELOT

CIRCLE

LANE

PANTHER

Wedding Gazebo

Possible Existing Building
Relocated to Enclose
Courtyard

Studio

Hampton House

Standpipe Fountain

Packing Label Mosaic

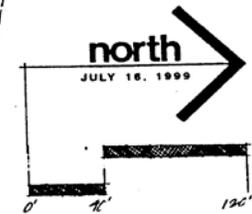
Drop off Turn Around

Victoria Hotel

Entry Plaza and Fountain

PARKWAY

FOOTHILL



KAMMEYER & ASSOCIATES
2837 KELLOGG AVENUE
CORONA, CALIFORNIA 92644
Phone: 909.371.2444 Fax: 909.371.4719

**CONCEPTUAL FOOTHILL
RANCH HERITAGE PARK**

EXHIBIT 5.3

The Elementary School site is designated on the land use plan to have an underlying residential density of one dwelling unit per acre. In the event that this site is not acquired by the school district, then the site may be developed for residential uses, and the density transfer provisions of this Specific Plan may be used to transfer additional residential units from other planning areas into this site.

6.0 CIRULCATION PLAN

6.1 Arterial and Collector Streets

The Mountain Gate circulation master plan identifies the arterial and collector streets that will serve the planned community. Alignments and cross-sections for these arterial and collector streets are shown in exhibit 6.1, the circulation plan. For reference purposes, this exhibit also shows the locations of certain local streets. Exhibit 6.2 shows the Mountain Gate circulation plan in the context of adjacent existing streets. More detailed cross-sections of these streets, including parkway designs, are presented in Exhibit 10.2 A through G in chapter 10 of this Specific Plan. The following text describes the arterial and collector streets that will serve Mountain Gate.

Foothill Parkway This arterial is identified in the south Corona CFP as “Chase/Pacific,” the major east-west transportation corridor through the south Corona area. Through Mountain Gate to approximately 1,700 linear feet west of Lincoln Avenue, Foothill Parkway will be a four lane divided arterial road in an 88-foot right-of-way, with emergency parking lanes, a 14-foot landscaped median, and two 5-foot parkways. A 20-foot wide landscape lot will be added to each side of the right-of-way, providing a total parkway width of 25 feet on each side. Extending westerly from this point, Foothill Parkway adjacent to the Specific Plan boundary will be a four-lane divided arterial road in a 102-foot right-of-way, which includes a 14-ft landscaped median and two 12-foot parkways. A 5-foot wide sidewalk will be located within the 12-foot parkway.

Consistent with the CFP’s goal of minimizing local street intersections along this arterial, the Mountain Gate Specific Plan limits local street access to the following locations:

- One local street access to planning area 20, between Lincoln Avenue and High grove Street;
- One local Street access to planning area 18, at existing Taylor Avenue;
- One local street access to planning area 8, between Main Street and the Main Street channel;
- One local Street access to planning areas 1 and 3, between the Main Street channel and Garretson Avenue;
- One local street access to planning area 31, west of Lincoln Avenue.

Driveway and private street access to Foothill Parkway is limited to the following locations:

- One right in, right out access to planning area 21, east of Lincoln Avenue;
- One full left turn and right turn access to each of planning areas 9 and 10, with additional right in, right out access points to be established during site plan review for the commercial and quasi-public developments on these sites;

- One full left turn and right turn access to planning area 7 with additional right in, right out access points to be established during site plan review for the medium density residential development on this site.

A traffic study shall be submitted in conjunction with the tentative tract maps for these planning areas along Foothill Parkway, to address the traffic safety and circulation aspects of these intersections. Local Street, driveway and private Street intersections with Foothill Parkway, in addition to those identified above, may be permitted subject to the approval of the City Engineer.

Main Street This arterial will serve as the primary entry to Mountain Gate. The circulation plan proposes a 100 foot right-of-way, with four travel lanes, emergency parking lanes, a 14 foot raised landscaped median, and two 10 foot wide parkways. A 20 foot wide landscape lot will be added to each side of the right-of-way, providing a total parkway width of 30 feet on each side. Main Street will terminate as an arterial roadway at its intersection with the Mountain Gate Drive, at the proposed community park site. Main Street shall continue southerly from the intersection with Mountain Gate Drive to the intersection with Upper Drive with a special ‘enhanced collector’ street section. This section provides a 68-foot right-of-way which includes two travel lanes, emergency parking lanes and two twelve foot parkways. An 18-foot wide landscape lot will be added along the outer edge of Main Street, providing for a total parkway width of 30 feet on this side. A 4-foot landscape lot will be added on the inner edge of Main Street, providing a total parkway width of 16 feet. From the intersection with Upper Drive, Main Street shall continue southerly as a collector street, providing access to that portion of Planning Area 5 located west of the Main Street wash, and further southwesterly as a local street, providing access to Planning Area 34. Direct driveway access to the Main Street collector will be permitted for estate density lots.

Mountain Gate Drive Mountain Gate Drive will be the primary traffic circulation element in the center of the planned community. Mountain Gate Drive will collect and distribute traffic through its intersections with Lincoln Avenue, Highgrove Street, and Main Street. A special “enhanced collector” street section is proposed for Mountain Gate Drive. This section provides a 68-foot right-of-way which includes two travel lanes, emergency parking lanes and two 12 foot parkways. An 18 foot wide landscape lot will be added along the outer edge of Mountain Gate Drive, providing a total parkway width of 30 feet on this side. A 4 foot landscape lot will be added on the inner edge of Mountain Gate Drive, providing a total parkway width of 16 feet on this side. This enhanced collector street section will also be provided along that portion of Upper Drive which is adjacent to the village core, between the terminus of Lincoln Avenue at the intersection with Mountain Gate Drive and the intersection of Upper Drive with Main Street. Extending southwesterly from Lincoln Avenue to Orange Heights Lane, Mountain Gate Drive will be a two-lane collector road in a 68-foot right-of-way, with emergency parking lanes and two 12-foot parkways. From Orange Heights Lane to Windy Ridge Drive, Mountain Gate Drive will be a local street with a 64- foot right-of-way, providing access to the southwestern section of Mountain Gate.

Lincoln Avenue The Community Facilities Plan and the Mountain Gate circulation plan proposes to relocate Lincoln Avenue south of its existing intersection with Foothill Parkway (formerly Chase Drive). From this point, Lincoln would be extended southeast and east, to join in a continuous alignment with a westerly extension of Upper Drive. Lincoln Avenue will

provide a second, westerly major entrance to the Mountain Gate planned community. The Mountain Gate circulation plan proposes that north of its intersection with Mountain Gate Drive, Lincoln Avenue will have a 100 foot right-of-way including four travel lanes, a striped median, emergency parking lanes and two 10 foot parkways. A 10 foot wide landscape lot will be added to each side of the Lincoln right-of-way, providing a total parkway width of 20 feet on each side. At its intersection with Mountain Gate Drive, Lincoln will transition and continue as the enhanced collector street section for Upper Drive.

Upper Drive Upper Drive is proposed as a 68-foot collector street that would depart from its existing alignment at Garretson Avenue, swinging north approximately 200 feet as it crosses the Main Street channel and Flood Control District spreading basin, to an intersection with Main Street. The Mountain Gate circulation plan proposes a 68 foot right-of-way with two travel lanes, emergency parking lanes, and two 12 foot parkways.

East of its intersection with Main Street, an additional 12.5-foot wide landscape lot will be added along the south side of the right-of-way to provide a total parkway width of 24.5 feet, consistent with the Community Facilities Plan. Along this same reach of upper Drive, a 2.5-foot wide landscape lot will be added on the north side of the right-of-way to provide a total parkway width of 14.5 feet. West of its intersection with Main Street, Upper Drive will have an enhanced collector section, with added landscape lots on both sides of the right-of-way, identical to the section for Mountain Gate Drive.

Existing Orange Heights Lane (formerly Upper Drive) will be retained as a local street along its existing alignment from existing Main Street west along the southerly edge of planning area 14. This part of Orange Heights Lane will provide access to the estate lots in planning area 14 and to the existing residences south of this area. This will buffer these estate homes from the higher traffic volumes expected along the new Lincoln Avenue/Upper Drive connection.

Highgrove Street and Fullerton. These streets are designated as collectors with two travel lanes, emergency parking lanes, and two 12 foot parkways within a 68 foot right-of-way. Highgrove Street provides a secondary entrance to the north side of Mountain Gate between Main Street and Lincoln Avenue, connecting Foothill Parkway with Mountain Gate Drive. Highgrove Street follows the new alignment called for in the Community Facilities Plan, intersecting Foothill Parkway at a point east of its present intersection with Chase Drive. Fullerton Avenue is located in the southeast part of Mountain Gate is Planning Area 5. Fullerton will serve to connect existing Fullerton Avenue (formerly Garretson Avenue) south of existing Cleveland Way (the Crown Ranch area) with the planned village core area in village three.

Cleveland Way (easterly of Fullerton Avenue). Cleveland Way (formerly Foothill Drive) is a semi-improved local street which extends along the southerly boundary of Mountain Gate adjacent to Planning Areas 5 and 33. The portion of Cleveland Way easterly of Fullerton Avenue shall be upgraded to a collector street in conformance with the approved South Corona Community Facilities Plan. This will provide access to the existing and planned estate lots along both sides of the Street.

6.2 Selected Local Street

Conditions relating to existing Main Street, existing Orange Heights Lane and the southwestern segment of Mountain Gate Drive were addressed in the preceding discussion of the arterial and collector streets. Additional special conditions relating to certain local streets are described below. Exhibit 63 illustrates certain aspects of Chase Drive and Garretson Avenue as discussed below.

Chase Drive. Chase Drive will extend as a local street east from its intersection with Main Street. The CFP designates Chase Drive as a scenic road, and an off-street bicycle trail is planned along the north side of this road. The Mountain Gate specific plan designates Chase Drive as a non-frontage road that will provide a northerly point of access to the local streets serving the single-family detached subdivisions in planning areas 1 and 8. The CFP also calls for Chase to terminate in a cul-de-sac west of its existing intersection with Garretson Avenue. Termination of the road will occur at the time of the development of the property northerly of Chase Drive.

As shown in Exhibit 6.3, west of its intersection with Main Street, Chase Drive will be extended as a local street providing secondary points of access to the commercial center in planning area 9. The phasing plan proposes that this be accomplished in phase 4. The road will terminate in a cul-de-sac providing access to the existing home located north of the west end of planning area 9. The off-street bicycle trail will extend along this section of Chase Drive to join the off-street bicycle trail planned along the north side of Foothill Parkway east of the MWD right-of-way. A special landscape setback area will be established along the south side of Chase Drive, terminating in a landscape buffer area at the west end of planning area 9, to provide a buffer for the existing home to the north.

Garretson Avenue. Within the Mountain Gate specific plan, Garretson Avenue will continue along its existing alignment between Chase Drive and Upper Drive, providing direct access to the estate lots proposed for planning 2 and 4. South of Upper Drive, Garretson Avenue may be realigned within the estate residential cluster development area in planning area 5. Access will be maintained at all times to Garretson's existing point of intersection with Cleveland Way, where it provides access to the Crown Ranch area.

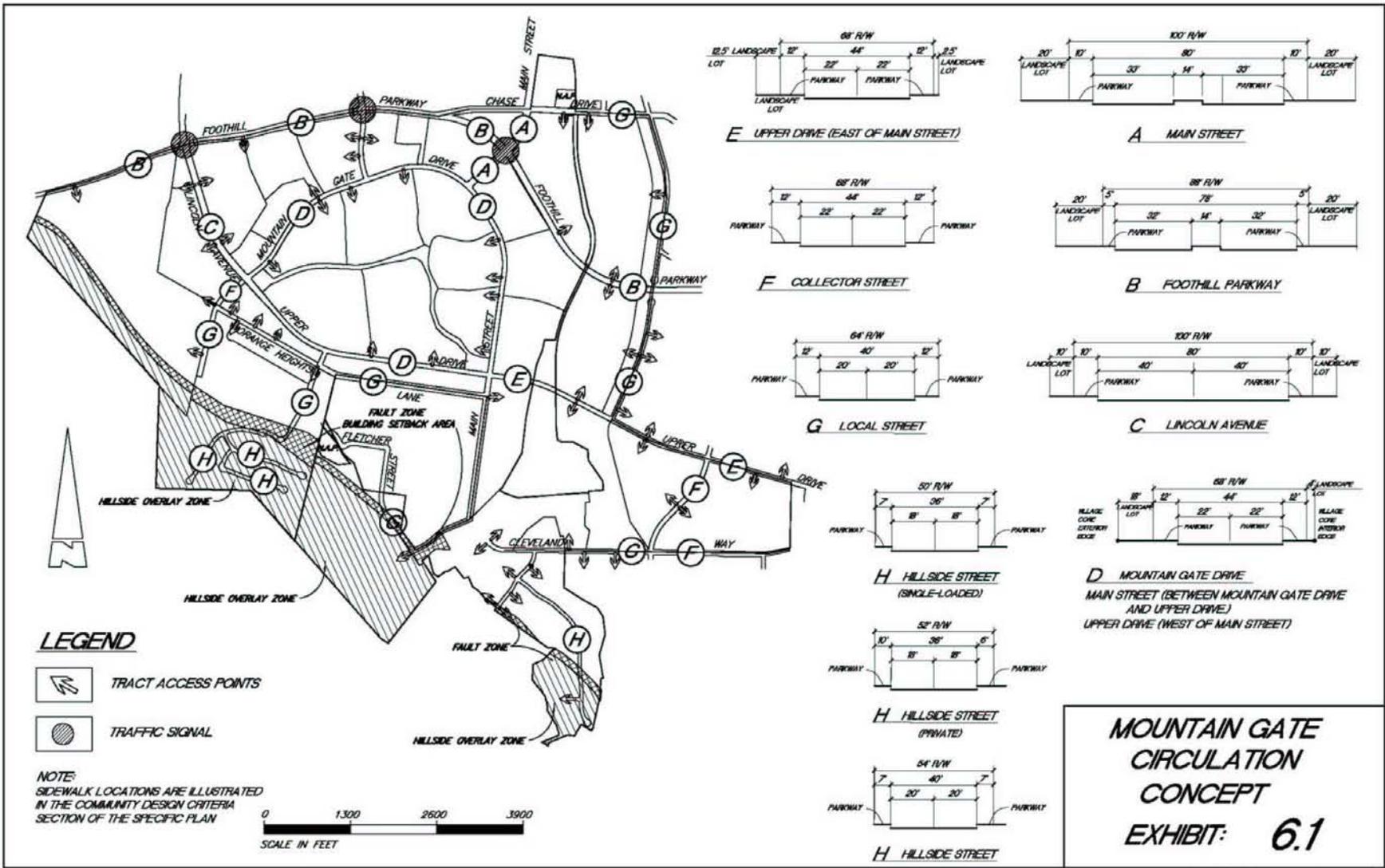
A plan for severing Garretson as a through street at Foothill Parkway is shown in exhibit 6.3. The phasing plan proposes to accomplish this in phase 3. This will prevent excessive through traffic from using this road as an alternative to Main Street. This plan would extend Garretson south from Chase Drive to provide access to the estate lots in planning area 2; then terminate the road in a cul-de-sac on the south at a point immediately north of the new Foothill Parkway alignment. Garretson would also end in a cul-de-sac on the south side of Foothill Parkway. In both cases, the cul-de-sacs in Garretson would be connected to other local streets prior to severing through access to Foothill Parkway. The maximum length of any cul-de-sac shall be limited to 500 feet.

Cleveland Way (Westerly of Fullerton Avenue). Cleveland Way (formerly Foothill Drive) is a semi-improved local street which extends along the southerly boundary of Mountain Gate adjacent to planning areas 5 and 33. The single family cluster development plan for planning area 5 shall include the continuation of Cleveland Way as a local Street along the frontage of

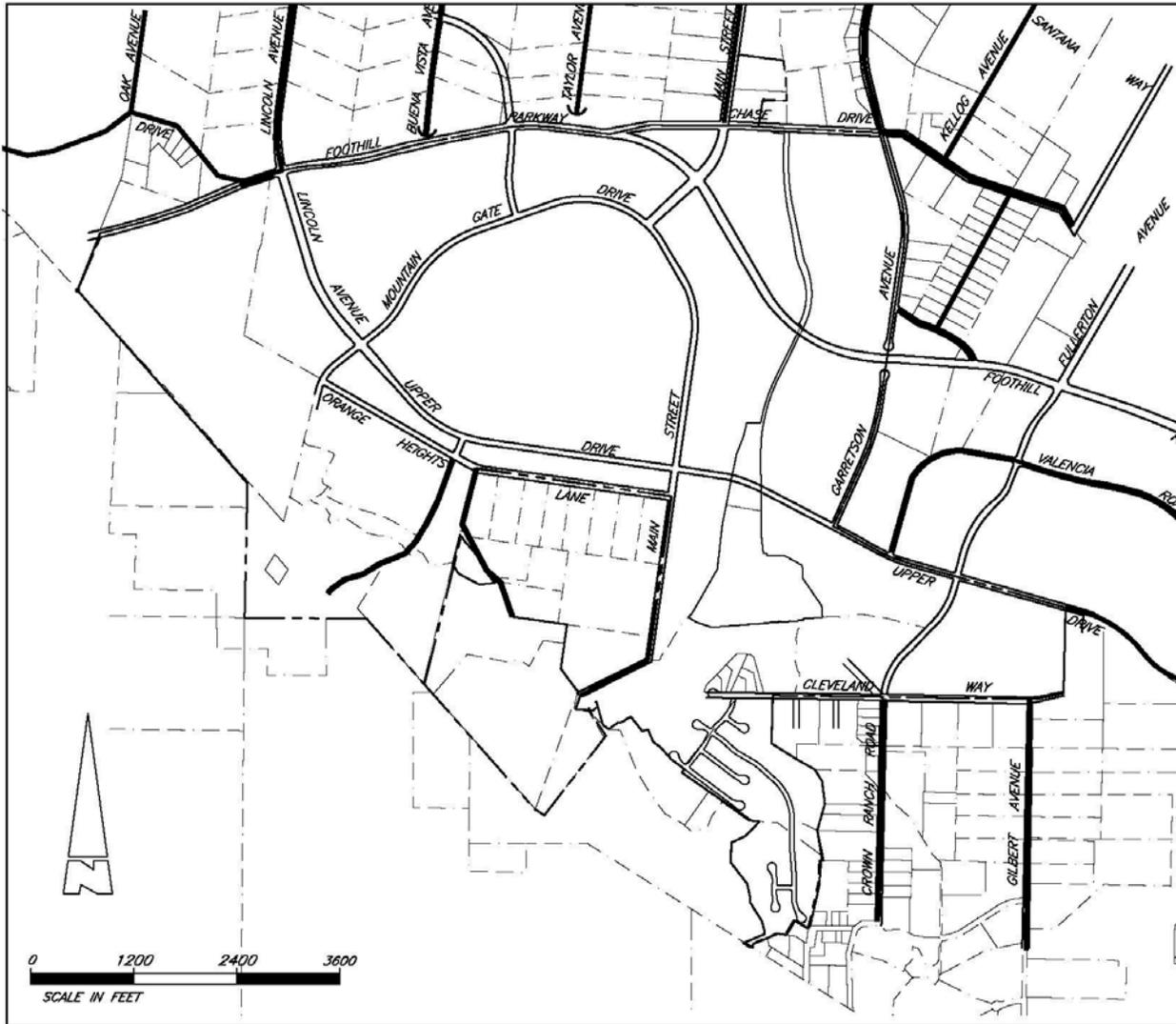
Planning Area 33 east to the intersection with Fullerton Avenue. This will provide access to the existing and planned estate lots along both sides of the street.

6.3 Hillside Street Standards

Hillside Street standards may be used within the designated hillside development area within planning areas 26 and 33, per the approval of the Director of Public Works and the Fire Chief. See exhibit 9.1 in the Development Standards chapter for the location of the hillside development area in planning area 26 and 33.



**MOUNTAIN GATE
CIRCULATION
CONCEPT
EXHIBIT: 6.1**



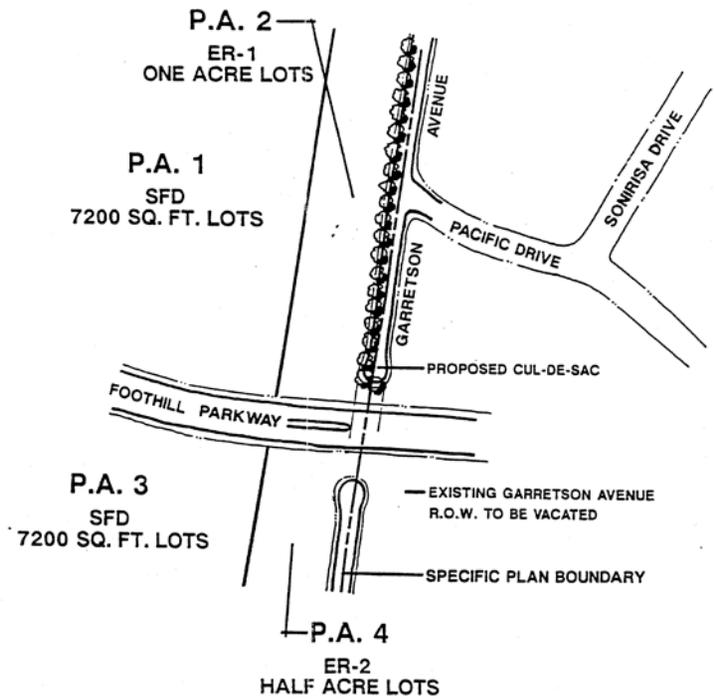
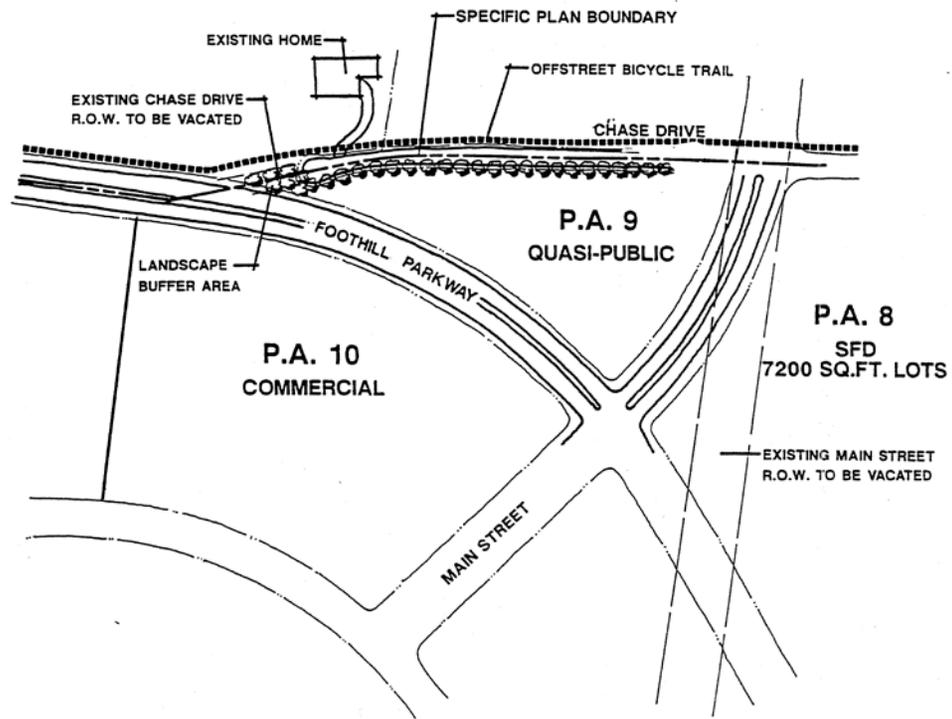
LEGEND



NO ACCESS TO THROUGH STREET

**MOUNTAIN GATE
SURROUNDING
ACCESS**

EXHIBIT: 6.2



SELECTED LOCAL STREETS
MOUNTAIN GATE
 LYON COMMUNITIES INC.

7.0 UTILITY PLAN SUMMARY

Master plans for water, wastewater and storm drainage have been prepared in support of the land planning program.

These plans are included in the following separate technical supplement reports:

- John S. Murk Engineers and Church Engineering. *Mountain Gate Water System Master Plan*. Revised July 1989.
- John S. Murk Engineers and Church Engineering. *Mountain Gate Sewer System Master Plan*. Revised July 1989.
- Church Engineering. *Preliminary Hydrology Report for Mountain Gate*. Revised July 1989.

7.1 Water Facilities Plan Summary

It is anticipated that Mountain Gates will have an ultimate population of approximately 11,937 people. Water demand for this population has been estimated to be approximately 2.78 million gallons per day (3,118 acre feet per year). This is based upon a per capita water demand figure of 220 gallons per day for low-medium density (SFD, SFA, and MDR) areas, 280 gallons per day for Estate Cluster areas, and 400 gallons per day for Estate Residential areas.

In order to properly serve the needs of the project area, the water supply and improvement plan must be evaluated according to the design criteria established by the City. These criteria include the ability of the system to deliver maximum day demands of fire storage is provided in reservoirs, maximum day peaking factors, a minimum water pressure of 35 psi, fire flows consistent with City standards, and the storage requirements of the pressure zones in which Mountain Gate is located. Mountain Gate is located within pressure zone 4 (920-1120 ft.), zone 5 (1120- 1280 ft.), and zone 6 (1280-1450 ft.).

Mountain Gate will utilize a variety of methods to furnish the water system improvements necessary for the project: direct construction of water facilities, participation in construction with other developers, and/or payment of water facility fees for improvements constructed by others. Any variation or alternatives shall be approved by the Utilities Director for compliance with the water master plan, the Community Facilities Plan and subsequent conditions of development placed on development within the boundaries of the Mountain Gate project.

Water supply to meet the additional project demand will be provided through an expansion of the City's water sources. This may include transfer to the City of existing wells which serve the Mountain Gate area; expansion of the existing water treatment plant at Chase Drive and Lester Avenue; drilling and equipping new wells; or from a combination of these sources and/or alternative sources.

It is anticipated that all of the source alternatives will make water available into the system at the City’s Lester Reservoir site where pumps will be constructed to raise water into water pressure zone 5.

Water storage requirements are based upon operational, emergency and fire demands established in the water system plan for the south Corona Agricultural Area. Table 7.1 summarizes the storage requirements for Mountain Gate.

**Table 7.1
Storage Requirements for Mountain Gate
(in Million Gallons)**

STORAGE	ZONE 4	ZONE 5	ZONE 6	TOTAL
Operating	0.49	0.67	0.19	1.35
Emergency	0.73	2.01	0.57	3.31
Fire	0.06	0.13	0.03	0.22
TOTAL	1.28	2.81	0.79	4.88

The ultimate south Corona Area storage requirements for zones 4, 5 and 6 are 11.4 mg, 7.6 mg, and 3.5 mg, respectively. The Mountain Gate Water Master Plan proposes to obtain a site and construct an initial 4.1 mg reservoir for zone 5. Design of the facility and placement of the reservoir will incorporate provisions for later expansion to approximately 8 mg as lands in zone 5 are developed. This reservoir would serve zone 5 directly and serve zone 4 lands within the site through pressure reducing stations. If zone 6 water facilities are not available, zone 6 demands for Mountain Gate would be provided in the zone 5 reservoir utilizing hydropneumatic systems incorporating pumped storage for domestic, emergency and fire demands. The more easterly pumping facility near future Fullerton Avenue would be designed to complement the permanent zone 6 pumping facilities when it is later expanded by others. Should pumped storage facilities be used for zone 6, the zone 5 reservoir must have sufficient reserve capacity to also fulfill the zone 6 demands.

As noted in table 7.1, Mountain Gate’s total demand on water system storage facilities, 4.88 mg, may exceed that constructed by Mountain Gate in the zone 5 reservoir (4.1 mg). Should additional storage capacity be required, development of later phases will be dependent upon the construction of additional storage by Mountain Gate, either alone or in conjunction with other developers. Project water system demands will not exceed the available capacity in the various water system infrastructure.

As the adjacent areas in south Corona are developed and construction of zone 4 or zone 6 water system infrastructure (pumping, transmission and storage) makes capacity available, Mountain Gate's zone 4 and zone 6 demands would be transferred to the appropriate facilities. As this transfer occurs, the released capacity in the zone 5 system would be made available for additional development within zone 5. Reimbursements for facilities used by other development would depend upon Mountain Gate's construction / participation in water system improvements and compliance with applicable ordinances and resolutions governing such reimbursements in the south Corona Area.

The distribution system planned for Mountain Gate is consistent with the system proposed by the south Corona Community Facilities Plan and is illustrated in exhibit 7.1. Final pipe sizes and alignments will be based on additional technical reviews by the Public Works Department.

7.2 Wastewater Facilities Plan Summary

Wastewater generation for the projected ultimate population of Mountain Gate has been estimated to be approximately 1.19 million gallons per day (mgd). This is based upon a per capita wastewater generation figure of 100 gallons per day.

The sewer component of the South Corona Community Facilities Plan divides Mountain Gate into six tributary drainage areas. These areas are generally divided along major streets including Lincoln Avenue, Buena Vista Avenue, Taylor Avenue, Main Street, Valencia Road/Fullerton Avenue and along the east side of the Main Street Channel. The Mountain Gate Sewer Master Plan identifies on-site sewer trunk lines which have been designed to connect to the South Corona Community Facilities Plan sewer trunk lines. The points of connection to the off-site sewer trunk lines will be located at the intersections of Foothill Parkway with Lincoln, Buena Vista and Taylor Avenues, at the intersection of Main Street and Chase Drive at the intersection of Chase Drive, and the Main Street Channel and at the intersection of Upper Drive and Valencia Road.

Exhibit 7.2 illustrates the proposed location of the Mountain Gate on-site sewer mains. The Mountain Gate sewer plan proposes to amend the Sewer System Master Plan for the South Corona Agricultural Area by relocating the existing or designating an additional master plan gravity sewer which is needed to serve the land in the Main Street wash area, between the bluffs separating Main Street and Garretson Avenue. This sewer main would extend north from Upper Drive through Mountain Gate to Chase Drive. From this point, the sewer main would follow the Main Street channel right-of-way north to Ontario Avenue. Final determination as to whether this change constitutes relocation or an additional master plan sewer will be made by review of detailed engineering studies and approved by the Utilities Director prior to recordation of Phase 1 master tentative tract map.

This new master plan sewer main could either follow the Main Street channel right-of-way all of the way to Ontario, or else it could be turned to follow the alignment of the Magnolia Avenue extension at the point where this new road will cross the Main Street channel.

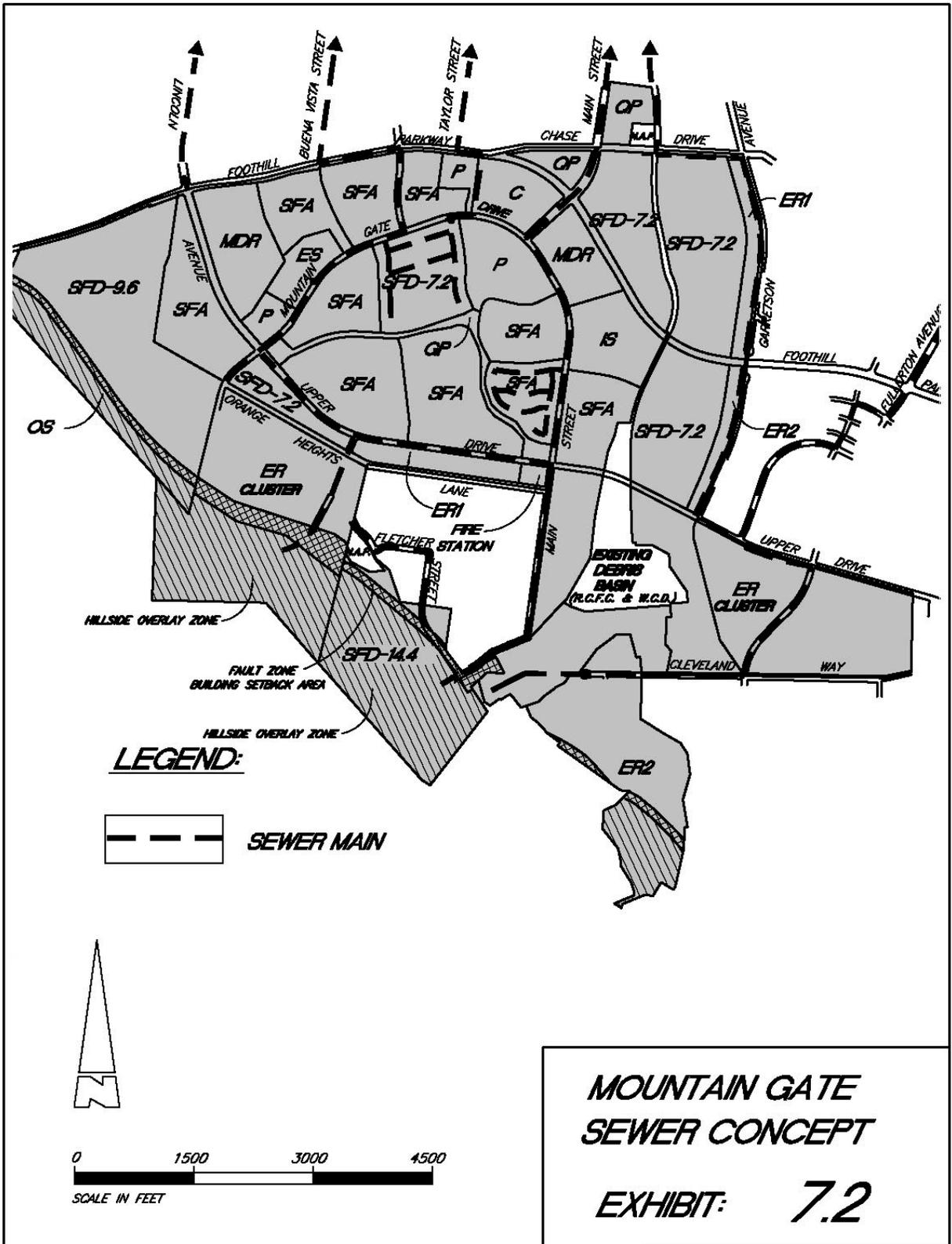


Exhibit 8.5 also shows an alternative off-site sewer connection in Magnolia Avenue, which could be utilized if the City desired to divert sewer flows from phases 1, 2, 3, 4 and 7 of the Mountain Gate project to the Sunkist Wastewater Treatment Plant, as opposed to the Wastewater Treatment Plant No. 1.

7.3 Storm Drainage Facilities Plan Summary

Mountain Gate is divided into two major watershed systems. The east part of the site is located at the mouths of Eagle and Main Street Canyons in the Main Street watershed, with a combined tributary area of over 2,500 areas. The west part of the site is located below three smaller unnamed canyons, located southwest of the site.

The flows from Main Street and Eagle Canyons are combined just upstream of the existing debris basin and discharge into the Main Street channel. In the event of a major storm, the flows from the unnamed canyons to the southwest overflow existing facilities and sheet flow northward toward Foothill Parkway and Lincoln Avenue.

The Mountain Gate storm drainage master plan proposes to develop a drainage system that is consistent with the south Corona Community Facilities Plan. The Mountain Gate drainage master plan is based on the following guidelines adopted in the CFP:

1. Any master plan drainage facility abutting or passing through a specific development area must be constructed with the development.
2. A development which does not increase peak run-off rates more than the excess capacity of existing downstream facilities may proceed without construction of downstream master plan facilities.
3. A development which does not increase run-off may proceed without construction of downstream master plan facilities, regardless of the capacity of downstream facilities.

The proposed drainage master plan for Mountain Gate is shown in exhibit 7.3. Phases 1, 2, 3, 4 and 7 of the Mountain Gate development plan are tributary to the Main Street channel. Development of these phases will require the construction of master plan storm drain lines that will discharge into the Main Street channel.

Phase 7 includes planning area 5 which is designated for single family cluster development. Upstream from the Main Street debris basin, the Eagle Canyon and Main Street Canyon washes are either unimproved or only partially improved. If the single family cluster development plan for this area does not locate residential development next to these washes, then these channels will not need to be improved. If the development plan for this area does include development next to the washes, improvement of certain reaches of these channels will be required in order to provide adequate flood protection.

Portions of Phase 5 and Phases 6, 8, 9 and 10 of the Mountain Gate development plan are tributary to the master planned Buena Vista and Lincoln Avenue storm drain systems, which in turn are tributary to the Oak Street storm drain

A portion of Phase 5 of the Mountain Gate development plan is tributary to the master plan storm drain system in Taylor Avenue which is inadequate to fully convey developed run-off from this watershed. The City of Corona has designated a permanent detention basin facility to be constructed along Taylor Avenue, near Ontario Avenue, to limit run-off to the north of Ontario Avenue at pre-south Corona development levels.

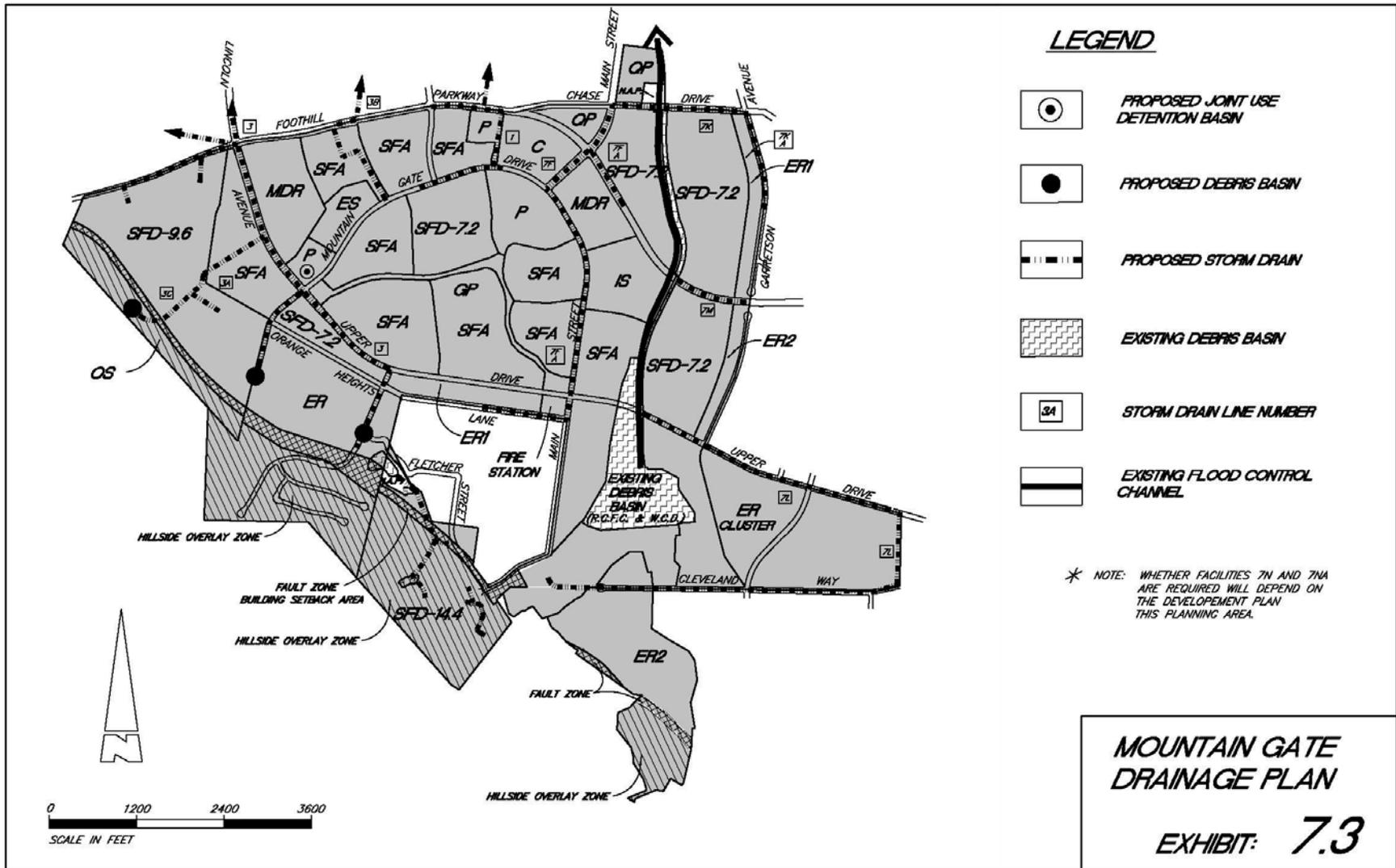
To address the problem of inadequate downstream drainage improvements in the Lincoln Avenue watershed, several types of drainage detention facilities are included in the Mountain Gate storm drain master plan:

1. Permanent detention facilities. The Community Facilities Plan calls for the construction of a permanent flow-by detention basin to be developed as a joint use facility in conjunction with the neighborhood park site located at the intersection of Lincoln Avenue and Mountain Gate Drive. The Mountain Gate Specific Plan includes provisions for this joint use detention facility. Exhibit 10.9 of this Specific Plan shows a concept plan for this joint use park/detention basin.
2. Temporary detention for phases 5 and 6. In order to develop portions of phases 5 and 6, downstream improvements will be required for the Buena Vista storm drain system and portions of the Lincoln Avenue storm drain system. If these downstream facilities are not improved when phases 5 and 6 are ready to proceed, a temporary detention basin will be provided at the north end of phase 5, near Foothill Parkway within planning area 19 or within planning area 20. This temporary basin would be sized at a minimum to serve drainage from Mountain Gate, being designed so that existing downstream run-off will not be increased. Landscaping and maintenance requirements for the detention basin are addressed in section 10.6 of this Specific Plan.
3. Detention for areas tributary to Oak Street Channel. Improvement of the Oak Street channel is included in the Corps of Engineers' Santa Ana River project, but the timing of future construction of this project is uncertain. In the event that development of areas tributary to the Oak Street channel is ready to proceed prior to the start of construction on this channel, storm water detention facilities will be provided in the west part of Mountain Gate so that peak run-off rates into the Oak Street channel will not be exceeded, consistent with City policy. Should this detention capacity be required, it may be accommodated in the temporary detention basin described in item no. 2 above.

To address the problem of inadequate downstream improvements in the Taylor Avenue watershed, the developer shall choose from one of the following proposed mitigation measures:

1. Construction of the master planned permanent detention basin along Taylor Avenue, near Ontario, with reimbursement to the Mountain Gate developer for construction of facilities in excess of the "fair share" portion of this basin.
2. Construction of the "fair share" portion of the master planned permanent detention basin along Taylor Avenue, near Ontario Avenue, to mitigate the increase in run-off generated by the proposed development within Mountain Gate.
3. Construction of a temporary detention basin within Phase 5 designed so that existing downstream run-off will not be increased. Landscaping and maintenance requirements for the detention basin are addressed in Section 10.6 of this Specific Plan.

As part of the Tentative Tract Map review process for new development within the Mountain Gate Specific Plan area a Project-specific Water Quality Management Plan (WQMP) shall be submitted by the project applicant to the City for review and approval. The Project-specific WQMP shall provide an overview of the proposed development plan, identify potential sources of storm water pollution of the land use, and recommended appropriate Best Management Practices (BMPs) or pollution control measures to reduce the discharge of pollutants in storm water runoff. The Riverside County WQMP shall serve as the guidance document for preparation of the Project-specific WQMP.



LEGEND

-  PROPOSED JOINT USE DETENTION BASIN
-  PROPOSED DEBRIS BASIN
-  PROPOSED STORM DRAIN
-  EXISTING DEBRIS BASIN
-  STORM DRAIN LINE NUMBER
-  EXISTING FLOOD CONTROL CHANNEL

* NOTE: WHETHER FACILITIES 7N AND 7NA ARE REQUIRED WILL DEPEND ON THE DEVELOPMENT PLAN THIS PLANNING AREA.

MOUNTAIN GATE DRAINAGE PLAN

EXHIBIT: 7.3

8.0 DEVELOPMENT FINANCING AND PHASING

8.1 Public Facilities Financing Plan

The developers, builders and future residents of the Mountain Gate planned community will participate in the financing of public facilities as established by State law, City ordinances, and conditions of approval.

8.2 Public Facilities Operation and Maintenance

It is proposed that a landscape maintenance district be established for Mountain Gate. The landscape maintenance district will be responsible for the following improvements:

- Landscaping within the parkways and medians of the arterial and collector streets, including the additional landscape lots that are proposed to be dedicated along a number of the major streets.
- The structure and exterior face of the project theme walls located along the major arterial and collector streets in the planned community.
- Entry monuments and special landscape areas located at the major project entries, secondary project entries, individual subdivision entries, and special landscape areas identified on the Mountain Gate landscape master plan.
- The landscaping, bicycle paths, recreational improvements and open space areas which may be developed and dedicated to the public within the designated estate residential duster development areas, exclusive of public park areas.
- Other improvements which may be accepted by the City Council for maintenance by the landscape maintenance district.

Either a homeowners association or a landscape maintenance district will maintain the landscaping within the designated fuel modification areas in the hillside overlay zone of planning area 26. These areas may include dedicated lands as well as easements granted to the homeowners association or the landscape maintenance district within private lots.

Other public facilities operation and maintenance costs will be funded from local tax revenues; water, sewer, and trash collection user fees; and other methods as established by the City Council pursuant to the south Corona Community Facilities Plan.

8.3 Phasing Plan Summary

The development phasing concept for Mountain Gate is based on the principle of orderly expansion of the infrastructure that is needed to serve the development of the various planning areas on the site. The development phasing concept plan for Mountain Gate is illustrated in exhibit 8.1. This phasing concept plan does not establish a required sequence of development for the site; instead, it depicts the current estimate of the most likely phasing scenario based upon infrastructure and market conditions.

Development is anticipated to commence in planning area 1, in the northeast portion of the site. This area is outside of the agricultural preserve, and is closest to the existing roads, sewer, and water systems that must be improved to serve the new development. The principal off-site improvements required to serve the initial phases of development are the water system and the sewer system. Off-site reservoir and transmission line improvements are needed for the water system and an off-site sewer will be required to connect with the existing sewer system to the north.

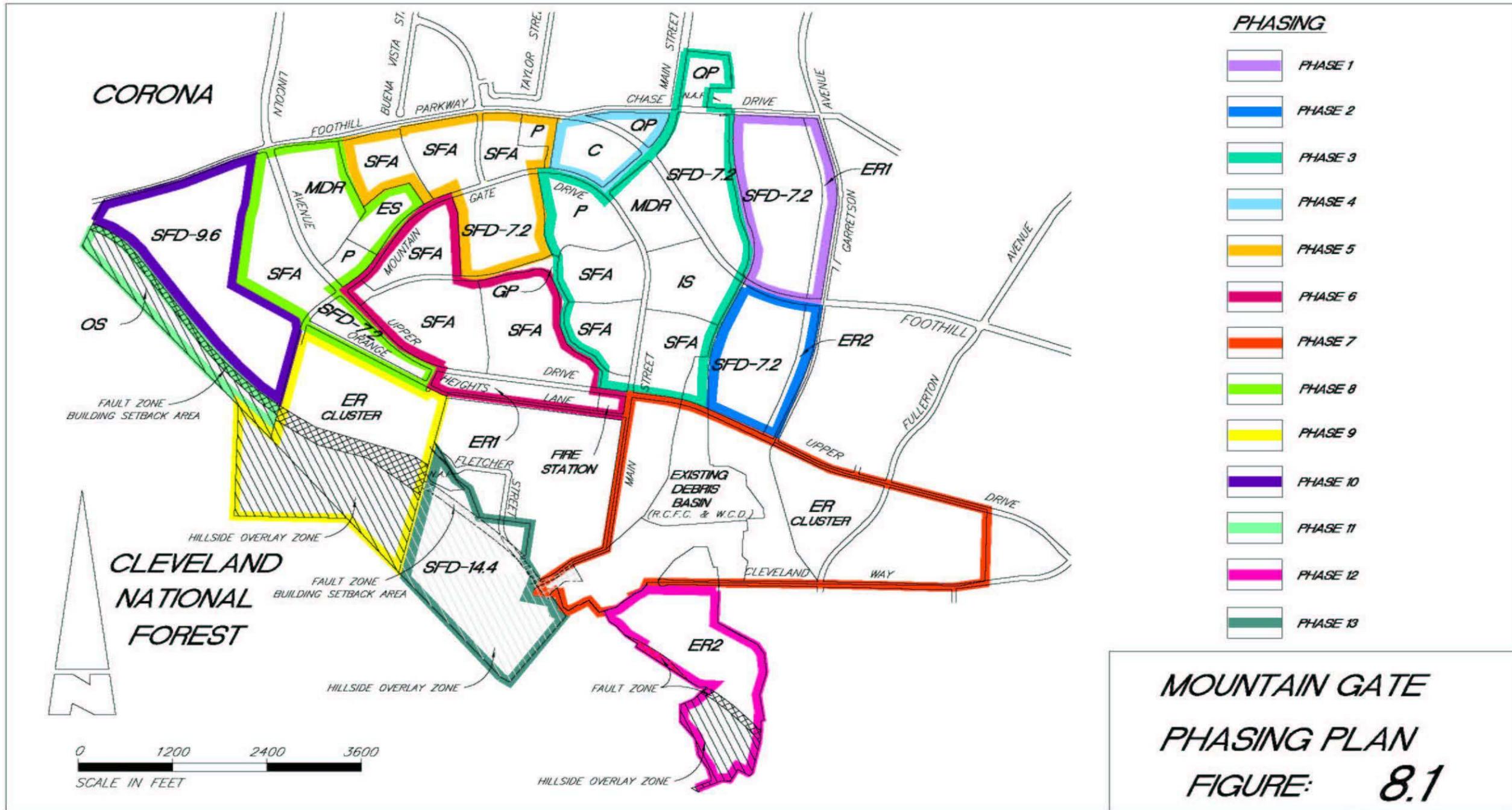
Development is then anticipated to extend to the west and to the south in a logical and incremental manner. Infrastructure systems will be extended and improved to serve the various increments of development. The final phases of development are expected to include the westerly part of the site near Lincoln Avenue and Foothill Parkway, and also the southerly parts of the site at the higher elevations that will be served by the zone 6 water system. The Board of Zoning Adjustment shall have the authority to hear and decide applications for modifications to the infrastructure phasing plans. The Board shall be required to make the following findings:

1. That the modification is consistent with the General Plan;
2. That the proposed changes will not adversely affect the implementation of the Specific Plan or the Community Facilities Plan; and
3. That it will not be detrimental to the public health, safety, and general welfare.
4. That the proposed modification will not delay the construction of master plan improvements necessary to serve development.

8.4 Infrastructure Phasing Program

This section of the Specific Plan proposes a schedule for the phasing of construction of the on-site and off-site infrastructure anticipated for the development of Mountain Gate. Map conditions of the individual builder maps are to be referenced for the detailed construction requirements for each development.

The schedule is organized on a phase by phase basis. For each phase of development, the individual infrastructure elements proposed to be constructed in that phase are listed. On-site improvements (those located within the Specific Plan boundaries) are shown separately from off-site improvements (those located outside of the Specific Plan boundaries) for each phase.



Development of individual planning areas within each phase will require construction of portions of the infrastructure required for the phase. The extent of infrastructure required for each planning area shall be approved by the City of Corona as part of the tentative map review process for each planning area.

The following exhibits included at the end of this section summarize the proposed phasing of construction for each of the major infrastructure systems:

- 8.2. Streets and landscaping phasing
- 8.3. Water system phasing
- 8.4. Sewer system phasing, on-site
- 8.5. Sewer system phasing, off-site
- 8.6. Drainage system phasing
- 8.7. Park land dedication phasing

The sizing of water, sewer and storm drain facilities are estimates based upon the Community Facilities Plan and the technical engineering studies which have been prepared in support of the Mountain Gate Specific Plan. Final sizing and alignment details will be determined on the basis of further technical studies prepared at the time of final design.

Refer also to Table 3.4.6, Arterial Street Program, and Table 3.8-1, Phasing Opportunities for Drainage, in the Community Facilities Plan.

PHASE 1- ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase one:

Streets

- 1. One-half width of Chase Drive, from realigned Main Street to Garretson Avenue.
- 2. West side of Garretson, from Chase to the location of the future cul-de-sac just north of Foothill Parkway.
- 3. Full width of Foothill Parkway, from Garretson to Main Street channel.

Water

- 1. Chase Drive, from Garretson to Main Street channel, 20" main (zone 4).
- 2. Garretson Avenue, from existing zone 4 line south of Chase to Foothill Parkway, 8" main (zone 4 and 5).
- 3. Foothill Parkway, from Garretson Avenue to the Main Street channel, 12" main (zone 5).

Sewer

- 1. Through planning area 1, from Foothill Parkway to off-site sewer located at Chase and Main Street channel, 8" main.
- 2. Garretson Avenue, from Foothill Parkway to Chase, 8" main.
- 3. Chase Drive, from Garretson to Main Street wash, 10" main.

Storm Drainage

1. In Garretson Avenue, from upstream storm drain terminus to Chase, portion of Line 7-KA, 30” RCP.
2. In Chase Drive, from Garretson to Main Street channel, portion of Line 7-K, 48” RCP.

Landscaping

1. South parkway, Chase Drive, from Main Street channel to Garretson.
2. West parkway, Garretson Avenue, from Chase to the location of the future cul-de-sac just north of Foothill Parkway.
3. North parkway, Foothill Parkway, from Garretson to Main Street channel.
4. North side, entry monument, Foothill Parkway at Garretson.

PHASE 1- OFF-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase one:

Water

1. Additional pumping capacity is to be added to zone 4 to provide sufficient water to phase one.

Sewer

1. Parallel to Main Street channel, preferably within the Flood Control District right-of-way, from Chase to Ontario, 10” main.
2. Ontario Avenue, from Main Street channel to e sewer at Garretson 10” main.

PHASE 2- ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase two:

Streets

1. West side of Garretson, from Foothill Parkway to Upper Drive.
2. Full width of Upper Drive, from Garretson to east side of Main Street channel.

Water

1. Upper Drive, from east of Fullerton Avenue to Main Street channel, 16” main (zone 5).
2. Garretson, from Foothill Parkway to Upper Drive, 12” main (zone 5).
3. Fullerton Avenue, from Foothill Parkway to Old Foothill Drive, 16” main (zone 5).

Sewer

1. Through planning area 3, from Upper Drive to Foothill Parkway, 8” main to join southerly terminus of sewer main through planning area 1.
2. Garretson Avenue, from Foothill Parkway to Upper Drive, 8” main.

Storm Drainage

1. In Foothill Parkway, from west of Garretson to Main Street channel, portion of Line 7- M, 42” RCP.

Landscaping

1. South parkway and median landscaping Foothill Parkway, from Garretson to Main Street channel.
2. West parkway, Garretson, from Foothill Parkway to Upper Drive.
3. North parkway, Upper Drive, from Garretson to Main Street channel.
4. South side, entry monument, Foothill Parkway at Garretson.
5. North side, entry monument, Upper Drive at Garretson.

PHASE 2- OFF-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase two: *

Water

1. Reservoir to be located south of Cleveland Way at a site to serve pressure zone 5. Said reservoir to be sized initially at 4.7 mg capacity to serve Mountain Gate directly from zone 5, and indirectly through pressure reduction for zone 4.
2. Pump station to boost water to reservoir that will serve zones 4, 5 and 6 of Mountain Gate.
3. Dedicate to the City of Corona all or some portion of the capacity of the existing wells which serve the site and construct water transmission pipelines and pumping facilities necessary to deliver water to the south Corona Area. Alternatively, if plant expansion is used for water supply, construct or participate in the construction of water treatment facilities.

4. Assuming water source is the Lester treatment plant, construct the following water transmission mains:
 - a. Rimpau, from Chase to Foothill Parkway, 24” main.
 - b. Rimpau, from Foothill Parkway to Upper Drive, 20” main.
 - c. Upper Drive, from Rimpau to east of Fullerton Avenue, 16” main.

PHASE 3-ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase three:

Streets

1. Full section of Foothill Parkway, from Main Street channel to Main Street.
2. Full width of Main Street, from Chase Drive to Mountain Gate Drive.
3. Full section of Main Street, from Upper Drive to Mountain Gate Drive.
4. South side of Mountain Gate Drive, from Main Street to west boundary of planning area 10.
5. North side of Upper Drive, from Main Street channel to Main Street.
6. Cul-de-sac on Garretson, immediately north of the intersection with new Foothill Parkway.
7. Cul-de-sac on Garretson, immediately south of the intersection with new Foothill Parkway.

Channel Crossings

1. South side of Chase Drive crossing of Main Street channel.
2. Full Foothill Parkway crossing of Main Street channel.
3. Full Upper Drive crossing of Main Street channel and Flood Control District water spreading basin.

Water

1. Chase Drive, from Main Street channel to Main Street, 20” main (zone 4).
2. Main Street, from Chase Drive to Mountain Gate Drive, 20” main (zone 4).
3. Mountain Gate Drive and Main Street, from west boundary of planning area 10 east and south to zone 4 reservoir site, 16” main (zone 4).
4. Upper Drive, from Main Street channel to Main Street, 16” main (zone 5).
5. Foothill Parkway, from Main Street channel to Main Street, 12” main, including pressure reduction station (zones 4 and 5).
6. Main Street, from Upper Drive to zone 5 terminus, 12” main (zone 5).

Sewer

1. Main Street, from Upper Drive to Mountain Gate Drive, 8” main.
2. Main Street, from Mountain Gate Drive to Chase Drive, 8” main.
3. Foothill Parkway, from sewer terminus west of Main Street channel to Main Street, 8” main.

Storm Drainage

1. Chase Drive, from Main Street to Main Street channel, portion of Line 7-F, 72” RCP.2. Main Street, from Mountain Gate Drive to Chase Drive, portion of Line 7-F, 60” RCP.
2. Foothill Parkway, from terminus to Mountain Gate Drive, portion of Line 7-FB, 24” RCP.
3. Main Street, from Upper Drive to Mountain Gate Drive, portion of Line 7-FA, 30” RCP.

Landscaping

1. South parkway, Chase, from Main Street to Main Street channel
2. Full parkway and median landscaping, Foothill Parkway, from Main Street channel to Main Street.
3. East parkway, Main Street, from Chase Drive to Mountain Gate Drive.
4. Full parkway, Main Street, from Upper Drive to Mountain Gate Drive.
5. South parkway, Mountain Gate Drive, from Main Street to west boundary of planning area 10.
6. North parkway, Upper Drive, from Main Street channel to Main Street.
7. East side, entry monument, Main Street at Foothill Parkway.

Parks

1. Dedication of 20 acre community park site, planning area 11, in partial satisfaction of park dedication requirements.
2. Dedication of the easterly leg of the greenbelt park, adjacent to planning areas 12 and 13.

PHASE 3-OFF-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase three:

Sewer

1. Main Street, from Chase Drive to the southerly terminus of the existing Main Street sewer, 10” main.
2. New sewer parallel to existing sewer, in the following streets:
 - a. Ontario Avenue, from Main Street to Taylor, 10” main.
 - b. Taylor Avenue, from Ontario to Olive, 10” and 12” main.
 - c. Olive Street, from Taylor to Sheridan, 15” main.

PHASE 4- ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase four:

Streets

1. South side of the extension of Chase Drive from Main Street to its new terminus near the west boundary of planning area 9.
2. Full section of Foothill Parkway, from Main Street to west boundary of planning area 10.
3. Completion of Mountain Gate Drive, north side, from Main Street to west boundary of planning area 10.

Traffic Signals

1. Traffic signal at Main and Foothill Parkway; to be constructed when warranted.

Water

1. Foothill Parkway, Main Street to west boundary of planning area 10, 12k' main (zone 4).

Landscaping

1. South parkway, Chase Drive, from Main Street to westerly terminus.
2. Landscape buffer area at west terminus of Chase Drive in planning area 9.
3. Full parkway and median landscaping Foothill Parkway, from Main Street to west boundary of planning area 10.
4. West parkway and median landscaping Main Street, from Chase Drive to Mountain Gate Drive.
5. North parkway, Mountain Gate Drive, from Main Street to west boundary of planning area 10.
6. West side, entry monument, Main Street at Foothill Parkway.

PHASE 5-ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase five:

Streets

1. South side of Foothill Parkway, from west boundary of planning area 10 to west boundary of planning area 20.
2. Full section of Mountain Gate Drive, from west boundary of planning area 11 to east boundary of planning area 23.
3. Full section of Highgrove Street, from Mountain Gate Drive to Foothill Parkway.

Traffic Signals

1. Traffic signal at the intersection of Foothill Parkway and Highgrove Street, to be constructed when warranted.

Water

1. Mountain Gate Drive, from west boundary of planning area 11 to east boundary of planning area 23, 16” main (zone 4).
2. Highgrove Street, from Mountain Gate Drive to Foothill Parkway, 16” main (zone 4).

Sewer

1. Mountain Gate Drive, from the east entrance into planning area 16 to the east boundary of planning area 17.
2. Along the east boundary of planning areas 17 and 18, from Mountain Gate Drive to Foothill Parkway, 8” main.
3. Foothill Parkway, from east boundary of planning area 18 to Taylor Avenue, 10” main.
4. Foothill Parkway, from Taylor Avenue to 400’ west.
5. Through planning area 19, from Mountain Gate Drive to Foothill Parkway.
6. Foothill Parkway, from Buena Vista to 200’ east.
7. Foothill Parkway, from Buena Vista to 400’ west.

Storm Drainage

1. Mountain Gate Drive, from west entry of planning area 16 to east boundary planning area 17.
2. Along the east boundary of planning areas 17 and 18, from Mountain Gate Drive to Foothill Parkway.
3. Foothill Parkway, from east boundary planning area 18 to Taylor Avenue.
4. Foothill Parkway, from Highgrove Street to Taylor Avenue.
5. Mountain Gate Drive, from east entry to planning area 24 to east boundary of planning area 23.
6. Through planning area 20, from Mountain Gate Drive to Foothill Parkway.
7. Temporary detention basin may be permitted in planning areas 19 or 20, near Foothill Parkway, designed so that runoff from phases 5 and 6 will not exceed the existing quantities of runoff.
8. Foothill Parkway, from Buena Vista Avenue to 200’ east.
9. Foothill Parkway, from Buena Vista Avenue to. 400’ west.

Landscaping

1. South parkway, Foothill Parkway, from west boundary of planning area 10 to west boundary of planning area 20.
2. Full parkway, Mountain Gate Drive, from west boundary of planning area 11 to east boundary of planning area 23.
3. Full parkway, Highgrove Street, from Mountain Gate Drive to Foothill Parkway.
4. South side, entry monument, Highgrove Street at Foothill Parkway.

Parks

1. Dedication of the central part of the west leg of the greenbelt park, adjacent to planning area 16.

PHASE 5- OFF-SITES

The developers of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase five:

Storm Drainage

1. Permanent detention basin at Taylor Avenue and Montoya Drive (formerly Bismark), or contribute a fair share amount to the construction of the permanent detention basin, or construct an interim detention basin within phase 5.

PHASE 6- ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase six

Streets

1. Full section of Upper Drive, from Main Street to east boundary of planning area 27.
2. North side of Upper Drive, from east boundary of planning area 27 to Mountain Gate Drive at Lincoln.
3. Full width of Mountain Gate Drive, from east boundary of planning area 20 to Lincoln.
4. West side of Main Street, from Upper Drive to Orange Heights Lane.
5. North side of Orange Heights Lane, from Main Street to the west boundary of planning area 14.

Water

1. Upper Drive, from Main Street to the zone 4/5 boundary in new Lincoln Avenue alignment, 12" and 16" main (zone 5).
2. Pressure reduction station in Lincoln Avenue, zone 4/5 boundary.
3. Mountain Gate Drive and through planning areas 22 and/or 23, from pressure reduction station in Lincoln Avenue to west boundary of planning area 19, 16" main (zone 4).

Sewer

1. Mountain Gate Drive, from Lincoln Avenue to the west boundary of planning area 19, 8" main.
2. Upper Drive, from upstream terminus of sewer, near east boundary of planning area 25, to Mountain Gate Drive at Lincoln Avenue, 8" main.

Landscaping

1. Full parkway, Upper Drive, from Main Street to the east boundary of planning area 25.
2. North parkway, Upper Drive, from east boundary of planning area 25 to Mountain Gate Drive at Lincoln Avenue.
3. East parkway, Mountain Gate Drive, from west boundary of planning area 19 to Lincoln Avenue.
4. West parkway, Main Street, from Upper Drive to Orange Heights Lane.
5. North parkway, Orange Heights Lane, from Main Street to west boundary of planning area 14.

Parks

1. Dedication of the western part of the west leg of the greenbelt park, adjacent to planning area 24.

PHASE 6- OFF-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase six:

Streets

1. Widening of Main Street from two lanes to four lanes, from Chase Drive north to its intersection with the extension of Magnolia Avenue. See also Table 3.4.6 of the Community Facilities Plan and related traffic studies.

PHASE 7- ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase seven:

STREETS

1. South side of Upper Drive, from east boundaiy of planning area 5 to Garretson.
2. Completion of Upper Drive, south side, from Main Street to channel to Main Street.
3. Full section of Fullerton, from Upper Drive to Cleveland Way.
4. East side of Main Street, from Upper Drive to its southerly terminus.
5. North side of Cleveland Way, from Gilbert to its westerly terminus.

WATER

1. Zone 6 pump station and hydropneumatic tank in planning area 5.
2. Fullerton Avenue from Zone 6 pumps station to Cleveland Way, 16 main.
3. Main Street, from Upper Drive to Cleveland Way, 16" main.
4. Cleveland Way, Main Street to Fullerton, 16" main.
Alternatives for serving Water to Planning Area 33:
 - a. Extend the existing 16" water main in Cleveland Way west and north to connect to the existing Main Street water line, per the original master plan described above.
 - b. Extend the existing 16" line in Cleveland Way westerly to the project entry of Planning Area 33, and then loop the system through the Planning Area and back to Cleveland Way.
 - c. Extend the existing 16" line in Cleveland Way westerly to Planning Area 33, then southeast through the property to the southerly end of Crown Ranch Road and connecting to the Zone 6 reservoir.

SEWER

1. Fullerton, from Upper Drive to Cleveland Way, 8” main.
2. Upper Drive, from Fullerton to Garretson, 8” main.

Storm Drainage

1. In Cleveland Way, from Fullerton Avenue to the eastern boundary of planning area 5B.
2. Along the eastern boundary of planning area 5B, from Cleveland Way to Upper Drive.
3. Upper Drive, from the eastern boundary of planning area 5B to the Main Street Channel.

Landscaping

1. South parkway, Upper Drive, from east boundary of planning area 5 to Main Street.
2. Full parkway, Fullerton from Upper Drive to Cleveland Way.
3. East parkway, Main Street, from Upper Drive to its southerly terminus.
4. North parkway, Cleveland Way, from Gilbert to its westerly terminus.

PHASE 8-ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase eight:

Streets

1. South side of Foothill Parkway, from west boundary of planning area 20 to Lincoln Avenue.
2. Completion of Upper Drive, south side, from east boundary of planning area 27 to Mountain Gate Drive at Lincoln Avenue.
3. Full section of Lincoln Avenue, from Foothill Parkway to Mountain Gate Drive.
4. Full section of Mountain Gate Drive, separating planning areas 27 and 28 (the “Hillcrest Connector”), from Lincoln Avenue to Orange Heights Lane.

Water

1. Lincoln, from Foothill Parkway to pressure reduction station at southerly terminus zone 4 system, 12” main (zone 4).
2. Hillcrest Connector, from Lincoln/Upper Drive to south boundary of planning areas 27 and 28, 12” main (zone 5).

Sewer

1. Lincoln, from Hillcrest Connector to Foothill Parkway, 8” main.
2. Foothill Parkway, from planning area 20 to Lincoln, 8” main.
3. Hillcrest Connector, Lincoln to Old Upper Drive at south boundary of planning areas 27 and 28, 8” main.

Storm Drainage

1. In Lincoln, from Mountain Gate Drive at Upper Drive to Foothill Parkway, portion of Line 3.
2. Debris basin at upstream end of Line 3 in planning area 26.
3. In Hillcrest Connector and in planning areas 26 and 27, from upstream debris basin to Lincoln at Mountain Gate Drive, portion of Line 3.
4. Debris basin at upstream end of Line 3-D in planning area 26.
5. Along the west boundary of planning area 26, from upstream debris basin to Lincoln, portion of Line 3D.
6. In planning area 28, from west boundary to Lincoln, portion of Line 3C.
7. Detention basin at joint use park site, in planning area 22.

Landscaping

1. South parkway, Foothill Parkway, from west boundary of planning area 20 to Lincoln.
2. South parkway, Upper Drive, from east boundary of planning area 25 to Mountain Gate Drive at Lincoln Avenue.
3. West parkway, Mountain Gate Drive, from west boundary of planning area 19 to Lincoln Avenue.
4. Full parkway, Lincoln, from Foothill Parkway to Mountain Gate Drive.
5. Full parkway, Hillcrest Connector, from Lincoln to Orange Heights Lane at south boundary of planning areas 27 and 28.
6. South side, entry monument, Lincoln at Foothill Parkway.

Parks

1. Dedication of the neighborhood park site in planning area 22, in partial satisfaction of park dedication requirements.

PHASE 8-OFF-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase eight:

Streets

1. Widening of Lincoln Avenue from two lanes to four lanes, from Foothill Parkway north to Ontario. See also Table 3.4.6 of the Community Facilities Plan and related traffic studies.

Sewer

1. Lincoln, from Foothill Parkway to existing sewer in Lincoln, 8" main.

Storm Drainage

1. Contribute to the construction of the downstream storm drain system in the following streets:
 - a. In Lincoln, from Ontario Avenue to Oak Street channel, portions of Line 3, 90” to 126” RCP.

PHASE 9- ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following improvements concurrent with the development of phase nine:

Water

1. Zone 6 facilities to service planning area 26, in accordance with the approved zone 6 master plan of water facilities.

Parks

1. Dedication of the historic park site in planning area 18, in partial satisfaction of park dedication requirements.

PHASE 10-ON-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following infrastructure improvements concurrent with the development of phase ten:

Streets

1. West side of Mountain Gate Drive, from Orange Heights Lane to Windy Ridge Drive.

Traffic Signals

1. Traffic signal at Lincoln Avenue and Foothill Parkway; to be constructed when warranted.
2. Pay a fair share toward the traffic signal at Foothill Parkway and project entrance.

Water

1. Hillcrest Connector, from Mountain Gate to western specific plan boundary in Foothill Parkway, 16” main (zone 5).

Sewer

1. Through planning area 31 to off-site sewer located at Foothill Parkway and Lincoln Avenue.

Storm Drainage

1. Debris basin at upstream end of Line 3-C in planning area 32.
2. From debris basin to east boundary of planning area 31, portion of Line 3-C, 66" RCP.

Landscaping

1. West parkway, Mountain Gate Drive, from Orange Heights Lane to Windy Ridge Drive.

PHASE 10- OFF-SITES

The developer of Mountain Gate shall construct or contribute its fair share toward the construction of the following infrastructure improvements concurrent with the development of phase ten:

Streets

1. South side of Foothill Parkway adjacent to the specific plan area, from Lincoln Avenue to western specific plan boundary.

Water

1. Foothill Parkway adjacent to the specific plan boundary, from Lincoln Avenue to approximately 1,700 linear feet westerly, 12" main (zone 4).

Sewer

1. Foothill Parkway to existing sewer in Lincoln Avenue, 8" main.

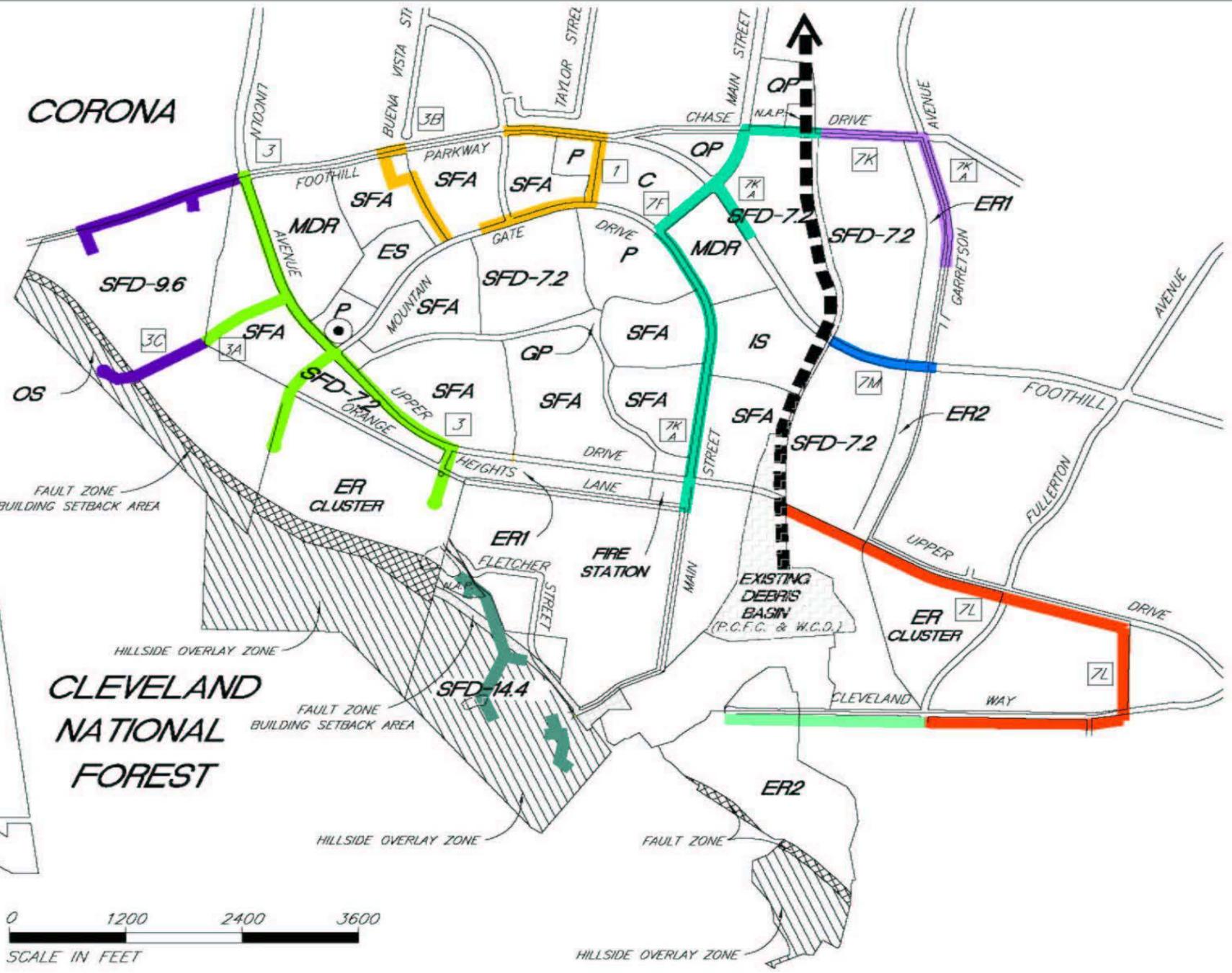
Storm Drainage

1. Foothill Parkway adjacent to the specific plan boundary, from Lincoln Avenue to approximately 1,700 linear feet westerly.
2. Contribute to the construction of the downstream storm drain system in Chase Drive, from Foothill Parkway to Oak Avenue basin, 54" RCP.

Landscaping

1. South parkway, Foothill Parkway adjacent to the specific plan area, from Lincoln Avenue to western specific plan boundary.

CORONA



CLEVELAND NATIONAL FOREST

LEGEND

- PROPOSED JOINT USE DETENTION BASIN
- PROPOSED DEBRIS BASIN
- EXISTING DEBRIS BASIN
- EXISTING FLOOD CONTROL CHANNEL
- STORM DRAIN LINE NUMBER

PHASING

- PHASE 1
- PHASE 2
- PHASE 3
- PHASE 5
- PHASE 6
- PHASE 7
- PHASE 8
- PHASE 10
- PHASE 11
- PHASE 12
- PHASE 13

**MOUNTAIN GATE
DRAINAGE SYSTEM
PHASING
EXHIBIT: 8.6**

9.0 DEVELOPMENT STANDARDS

The Mountain Gate Specific Plan establishes five residential districts, a commercial district, a quasi-public district; a hillside overlay district and standards for the improvement of open space and public lands. The standards contained herein shall serve as minimum regulations unless specified otherwise. They are subject to modification through the amendment and modification procedures contained herein or within Section 11.0, Implementation Plan.

9.0.1 General Provisions

- a. Unless excepted herein, the south Corona Community Facilities Plan and other relevant zoning ordinances, municipal codes, and City regulations shall govern the design, development and construction of Mountain Gate.
- b. The Land Use Plan for Mountain Gate is shown in exhibit 5.1. The Land Use Plan identifies individual planning areas and assigns each planning area to a land use district.
- c. The number of units allowed within each planning area shall be within the range established in table 5.2, except where density transfers are approved as provided in Section 11.2, Density Transfers. The total number of dwelling units in the entire Specific Plan area shall not exceed the total number permitted for all planning areas as shown in table 5.2.
- d. The following land use districts are established:

ER:	Estate Residential 1 - 2 units per gross acre
ER Cluster:	Estate Residential Cluster 3 units per gross acre
SFD:	Single Family Detached Residential 0 - 4 units per gross acre
SFA:	Single Family Attached Residential 6 - 8 units per gross acre
MDR:	Medium Density Residential 10 - 15 units per gross acre
C:	Commercial
QP:	Quasi-Public
P, GP, OS and ES:	Parks, Greenbelt Park, Open Space and Elementary School
GP:	Greenbelt Park

ES: Elementary School

HO: Hillside Overlay District

Fire Station: Fire Station Site

- e. Processing of Precise Plans in accordance with Section 11 of this Specific Plan, or design review as established by City ordinance, shall be required for town home and multi-family development within the “MDR” Medium Density Residential district, and for any development within the “ER Cluster” Residential district, the “C” Commercial district, and the “OF” Quasi-Public district.
- f. Agricultural operations consistent with the permitted uses in the “A” zone (CMC Section 17.06.020) shall be permitted as interim uses on lands where the construction of buildings has not yet commenced.
- g. No building permit shall be issued for any lot or parcel of land unless said lot has frontage on a dedicated or City approved Private Street and said Private Street is improved with access determined adequate by the Public Works Director.
- h. Construction of wood framing will not be permitted until temporary water system improvements are completed to each area to provide an adequate fire flow, and all- weather surface vehicular access determined adequate for fire suppression equipment is available as approved by the City Fire Department.
- i. Setbacks from street rights-of-way shall be as noted herein, unless additional separation is required to mitigate traffic noise per the Mountain Gate E

9.0.2 Special Setback, Lot Area, Lot Width and Fencing Provisions adjacent to Existing Estate Residential Neighborhoods

The following special standards shall apply only in those situations where new development is proposed adjacent to certain existing residential neighborhoods in south Corona; as such existing residential neighborhoods were defined at the time of adoption of the south Corona Community Facilities Plan by the Corona City Council.

- 1. Where new single family detached residential units are developed on lots that immediately abut existing single family residential housing, or where the new housing is located directly across from and fronts on the street where existing residences occur, these new lots shall be similar in area and width to the existing lots, but need not exceed 130 feet in width nor one acre in area. The new units shall be restricted to a maximum height of two stories.
- 2. For new residential development that is located on the same block with existing residential lots, the lots for the new residences shall be generally the same average areas as the existing lots (within 90 percent) within the block but need not exceed one acre in area. This applies to frontage on both sides of the street.

3. Where new residential lots back directly onto the rear of existing developed residential lots that are one-third acre or greater, the minimum area of the new residential lots shall be generally the same as the existing lots, but not to exceed one acre. If the existing residential lots are less than one-third acre, then the new lots shall be at least the same area as the existing lots.
4. On a back-on condition as described in item 3 above, the minimum rear yard setback adjacent to the existing developed residential lot shall be 30 feet. If a side yard occurs on a new residential lot that immediately abuts the rear property line of an existing residence, then the minimum side yard dimension shall be 15 feet as shown in Exhibit 3.1-2A of the CFP.
5. For item 4 above, new residential development will require a minimum six foot decorative high opaque wall between the new and existing residential lots unless protest is received from the property owner of the existing residence.

9.1 RESIDENTIAL DEVELOPMENT STANDARDS

9.1.1 ER - Estate Residential District (1-2 units per gross acre)

1. Purpose
2. Permitted Uses
3. Conditional Uses
4. Prohibited Uses
5. Lot Area, Lot Width, Lot Depth
6. Building Height
7. Front Yard Setbacks
8. Side Yard Setbacks
9. Rear Yard Setbacks
10. Coverage
11. Walls and Fences
12. Off-Street Parking
13. Hillside Development Standards

1. **Purpose** - The ER Estate Residential district is intended for single-family homes on large, suburban size lots with one dwelling and customary accessory buildings upon one lot. ER areas have been located on the Land Use Plan to buffer existing land uses of a similar or lesser density. The ER land use district proposes an urban subdivision at the following densities: Except as specifically provided elsewhere, any and every building and premises or land in the ER district shall be used for or occupied and every building shall be erected, constructed, established, altered, enlarged, maintained, moved into or within said ER district exclusively and only in accordance with the following standards set forth.

In the event that portions of this district are located adjacent to existing residential neighborhoods existing and identified as such at the time of adoption of the south Corona Community Facilities Plan (July 1988), the requirements of Section 9.0.2 of this document shall take precedence over other requirements herein.

2. **Permitted Uses** - The following uses shall be permitted in the ER district:

One-family detached dwelling of a permanent character placed in a permanent location.

A secondary residential unit pursuant to Corona Municipal Code Section 17.85.

Parks and playgrounds (public and private).

Small family day care.

Large family day care is permitted with Board of Zoning Adjustment approval.

Private recreational facilities, owned and maintained by an individual homeowner, a homeowners' association or other form of community association or maintenance district

Animals to maximum numbers as follows:

- Three weaned cats;
- Three weaned dogs.

Home occupations, in accordance with CMC Chapter 17.80.

Model home complex including sales office subject to the review of the Board of Zoning Adjustment according to the procedures described in Chapter 17.98 of the Corona Municipal Code.

Uses customarily incidental to any of the above uses and accessory buildings, including a private garage, and servants' quarters, provided no kitchen or kitchen facilities shall be included in any such accessory building.

Similar uses permitted by Commission determination: The Commission, may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district for procedure, see Chapter 17.88 of the Corona Municipal Code.

3. **Conditional Uses** - The following uses may be permitted subject to conditional use permits provided for in Chapter 17.92 of the Corona Municipal Code:

- Churches;
- Educational Facilities, Public and Private;
- Senior citizen residences

4. **Prohibited Uses** - Uses prohibited in the ER district are as follows:

- Commercial uses;
- Manufacturing uses;

5. **Lot Area, Lot Width and Lot Depth** - The minimum area of any lot intended for residential development shall be 40,000 square feet within areas designated "ER 1" and 20,000 square feet within areas designated "ER 2." The minimum lot width shall be 130 feet within areas designated "ER 1" and 100 feet within areas designated "ER 2." The minimum lot depth shall be 120 feet. Number of dwelling units permitted shall be subject to density allocation found on Table 5.2, Allocation of Dwelling Units among Planning Areas.

For those portions of Planning Area 33 which directly abut the existing Crown Ranch residential neighborhood (as designated in the South Corona Community Facilities Plan), or are within the Hillside Development Overlay Zone, the lot area shall be a minimum of one-acre. The balance of Planning Area 33 shall have a minimum lot area of 20,000 square feet pursuant to the ER 2 standards of this section.

6. **Building Height** - The height of buildings or structures shall not exceed two stories or thirty (30) feet.

7. **Front Yard Setback** - The front yard shall be a minimum of thirty (30) feet. Within Planning Area 33, porte-cocheres associated with the main driveway entrance to a residence may extend within the front yard setback a maximum of 15 feet.
8. **Side Yard Setback** - Lots shall have minimum side yards of 15 feet within areas designated "ER 1," and 15 feet and 10 feet within areas designated "ER 2." Fencing erected consistent with Section 9.1.1.11 along local street side yards may be permitted to encroach into the required side yard setback to the property line. Corona Municipal Code Chapter 17.70 Ct. seq. standards applicable to corner cut-offs, key lots and reverse corner lots shall apply.
9. **Rear Yard Setbacks** - Lots shall have level rear yards of not less than 30 feet within areas designated "ER 1" and 20 feet within areas designated "ER 2" for the primary dwelling unit, and 10 feet for any accessory structures.
10. **Coverage** - Lot area coverage by buildings or structures shall not exceed twenty-five (25) percent of the total lot area within areas designated "ER 1" and thirty (30) percent within areas designated "ER 2."
11. **Walls and Fences** - The provisions of Chapter 17.70 of the Corona Municipal Code shall apply except that solid walls and fences or non-climbable wrought iron fences up to six feet in height shall be permitted where on lot depth blocks are created, and provided that said wall or fence is separated from the adjacent sidewalk of said street by a minimum of four feet of landscaping, consistent with the appropriate street cross section of this Specific Plan, subject to design approval by the Planning Director.
12. **Off-Street Parking** - The provisions of Chapter 17.76 of the Corona Municipal Code shall apply.
13. **Hillside Development Standards:**
 - a. **Purpose** - The purpose of the hillside overlay criteria is to provide regulations for the development of those areas of Mountain Gate which, due to their topography, require special consideration to assure that they are developed in a way that will substantially maintain their natural character and environmental and aesthetic values.
 - b. **Applicability** - Prior to or concurrent with the approval of a development plan for the designated hillside overlay zone, specific hillside development standards shall be adopted to govern development within this area. Said hillside development standards are to be used in conjunction with the specific residential development standards established by the district designation illustrated on the Land Use Plan. If such standards are not adopted into the specific plan, development shall adhere to the standards in CMC Section 17.62.110, Hillside Overlay Zone.

9.1.2. ER Cluster - Estate Residential Cluster District (3 units per gross acre)

1. Purpose
2. Permitted Uses
3. Conditional Uses
4. Prohibited Uses
5. Development Standards
6. Open Space Area Requirement
7. Processing of Plans
8. Hillside Development Standards

1. **Purpose** - The ER Cluster residential district is intended for single-family detached and attached homes not to exceed three (3) dwelling units per gross acre developed consistent with the provisions of the south Corona Community Facilities Plan. Town homes may also be permitted subject to a conditional use permit. Development in the ER Cluster shall be within a well designed planned residential development which combines open space, greenbelts, and/or trails with a range of single family detached and attached units including estates, conventional single family detached and/or single family attached.

Except as specifically provided elsewhere, any and every building and premises or land in the ER Cluster district shall be used for or occupied and every building shall be erected, constructed, established, altered, enlarged, maintained, moved into or within said ER Cluster district exclusively and only in accordance with the following standards set forth.

In the event that portions of this district are located adjacent to existing residential neighborhoods existing and identified as such at the time of adoption of the south Corona Community Facilities Plan (July 1988), the requirements of Section 9.02 of this document shall take precedence, over other requirements herein.

2. **Permitted Uses** - The following uses shall be permitted in the ER Cluster district:

One-family detached or cluster unit of a permanent character placed in a permanent location.

A cluster unit is defined as a single family detached, single family attached or multiple family dwelling that is assembled with similar units, either on separate lots or on a single lot, to preserve open space features within the planning area, and having a combined unit density no greater than that permitted by the CFP.

A secondary residential unit pursuant to Corona Municipal Code Section 17.85.

Parks and playgrounds (public and private).

Small family day care.

Large family day care is permitted with Board of Zoning Adjustment approval.

Private recreational facilities owned and maintained by an individual homeowner, a homeowners' association or other form of community association or maintenance district.

Animals to maximum numbers as follows:

- Three weaned cats;
- Three weaned dogs.

Home occupations, in accordance with CMC Chapter 17.80.

House trailer, may be used as a subdivision construction project office during time of construction when a valid building permit is in force.

Manufactured housing in conformance with provisions established in Chapter 17.81 of the Corona Municipal Code

Model home complex including sales office subject to the review of the Board of Zoning Adjustment according to the procedures described in Chapter 17.98 of the Corona Municipal Code.

Uses customarily incidental to any of the above uses and accessory buildings, including a private garage, and servants' quarters, provided no kitchen or kitchen facilities shall be included in any such accessory building.

Similar uses permitted by Commission determination: The Commission may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district. For procedure, see chapter 17.88 of the Corona Municipal Code.

3. **Conditional Uses** - The following uses may be permitted subject to conditional use permits provided for in Chapter 17.92 of the Corona Municipal Code:

- Churches;
- Educational Facilities, Public and Private;
- Day care facilities, except family day care which does not require a conditional use permit;
- Senior Citizen Residences;
- Residential condominium;

A residential condominium is a single family detached, single family attached or multiple family dwelling where property owners have title to a dwelling unit, and title to a fractional portion of land held in common by all property owners.

Town homes developed pursuant to the standards of Section 9.1.5 of this Specific Plan.

4. **Prohibited Uses** - Uses prohibited in the ER Cluster district are as follows: Commercial uses; Manufacturing uses.
5. **Development Standards** - The standards for residential development in the ER Cluster designation shall be as follows:
 - a. single family detached residential at a maximum density of 2 dwelling unit or less per gross acre the development standards of Sections 9.1.1.5.-I inclusive;
 - b. single family detached residential with a minimum lot size of 7,200 square feet - the development standards of Sections 9.13.5.- 12., inclusive;
 - C. single family attached residential - the development standards of Sections 9.1.4.5.- 13., inclusive;
 - d. town homes - the development standards of Sections 9.1.5.S.-17., inclusive.

The number of dwelling units permitted shall be subject to density allocation found on Table 5.2, Allocation of Dwelling Units among Planning Areas.

6. Open Space Area Requirement

- a. Within each separate planning area which is designated for development within the ER Cluster district, qualifying open space areas shall be provided in a total amount equal to a minimum of 1,500 square feet per dwelling unit within said planning area.
- b. As used in this section 9.1.2.6, “qualifying open space areas” shall include park lands, greenbelts, recreation areas and open space areas which are dedicated or offered for dedication to the City or other public agency, or which are owned in common by an association of homeowners or property owners, and for which provisions have been made to ensure the ongoing maintenance of the open space lands. An improvement plan for the open space areas shall be submitted in conjunction with the tentative tract map for the Estate Cluster District. The open space areas shall preserve mature trees and natural landforms and vegetation wherever feasible. Existing stands of Oak Woodland shall be preserved per the requirements of the Mountain Gate EIR.

7. **Processing of Plans** - In order to achieve cohesive development and continuity of open space within each of the ER Cluster planning areas, one or more tract maps shall be processed concurrently, so that for each of planning areas 5 and 26, maps covering the entire planning area shall be considered by the Planning Commission at the same time.

8. **Hillside Development Standards.**

- a. **Purpose** - The purpose of the hillside overlay criteria is to provide regulations for the development of those areas of Mountain Gate which, due to their topography, require special consideration to assure that they are developed in a way that will substantially maintain their natural character and environmental and aesthetic values.
- b. **Applicability** - Exhibit 9.1 of this Specific Plan designates a portion of planning area 26 as a “hillside overlay zone.” Prior to or concurrent with the approval of a development plan for this designated hillside overlay zone, specific hillside development standards shall be adopted to govern development within this area. Said hillside development standards are to be used in conjunction with the specific residential development standards established by the district designation illustrated on the Land Use Plan.

9.1.3 SFD - Single Family Detached Residential District (0 - 4 units per gross acre)

1. Purpose
2. Permitted Uses
3. Conditional Uses
4. Prohibited Uses
5. Lot Area, Lot Width, Lot Depth
6. Building Height
7. Front Yard Setbacks
8. Side Yard Setbacks
9. Rear Yard Setbacks
10. Coverage
11. Walls and Fences
12. Off-Street Parking

1. **Purpose** - The SFD single family residential district is intended for single-family homes with not more than one dwelling and customary accessory buildings upon one lot. The SFD land use category proposed an urban subdivision at a density range of 0 to 4 units per acre, which corresponds with the City's respective General Plan designation of the property. Except as specifically provided elsewhere, any and every building and premises or land in the SFD district shall be used for or occupied and every building shall be erected, constructed, established, altered, enlarged, maintained, moved into or within said SFD district exclusively and only in accordance with the following standards set forth.

In the event that portions of this district are located adjacent to existing residential neighborhoods existing and identified as such at the time of adoption of the south Corona Community Facilities Plan (July 1988), the requirements of Section 9.0.2 of this document shall take precedence over other requirements herein.

2. **Permitted Uses** - The following uses shall be permitted in the SFD district:

One-family detached dwelling of a permanent character placed in a permanent location. A secondary residential unit pursuant to Corona Municipal Code Section 17.85.

Parks and playgrounds (public and private), subject to provisions of Chapter 12.24 of the Corona Municipal Code.

Small family day care.

Large family day care is permitted with Board of Zoning Adjustment approval.

Private recreational facilities owned and maintained by an individual homeowner, a homeowners' association or other form of community association or maintenance district.

Animals to maximum numbers as follows:

- Three weaned cats;
- Three weaned dogs.

Home occupations, in accordance with CMC Chapter 17.80.

House trailer, may be used as a subdivision construction project office during time of construction when a valid building permit is in force.

Manufactured housing in conformance with provisions established in Chapter 17.81 of the Corona Municipal Code.

Model home complex including sales office subject to the review of the Board of Zoning Adjustment according to the procedures described in Chapter 17.98 of the Corona Municipal Code.

Uses customarily incidental to any of the above uses and accessory buildings, including a private garage, and servants' quarters, provided no kitchen or kitchen facilities shall be included in any such accessory building.

Similar uses permitted by Commission determination: The Commission may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district. For procedure, see Chapter 17.88 of the Corona Municipal Code.

3. **Conditional Uses** - The following uses may be permitted subject to conditional use permits provided for in Chapter 17.92 of the Corona Municipal Code:

Churches;

Education Facilities, Public and Private;

Day Care Facilities, except family day care which does not require a conditional use permit;

Senior Citizen Residences.

4. **Prohibited Uses** - Uses prohibited in the SFD district are as follows:

Commercial uses;

Manufacturing uses;

5. **Lot Area, Lot Width and Lot Depth** - The minimum area, width and depth of any lot intended for residential development shall be as follows:

SFD-9.6¹: Lot Area - 9,600 square feet

Lot Width - 70 feet; with an average lot width of 75 feet per residential subdivision

Lot Depth - 105 feet

SFD-7.2: Lot Area - 7,200 square feet Lot Width - 60 feet

Lot Depth - 80 feet

Lot width to depth ratios may vary from 1:1 to 1:3 within areas designated "SFD-7.2." Number of dwelling units permitted shall be subject to density allocation found on Table 5.2, Allocation of Dwelling Units among Planning Areas.

¹ To accommodate a creative subdivision design per the recommendations listed in the City's Residential Design Guidelines, Planning Area 31 may have up to 18 percent of the lots within a subdivision reduced to 8,400 square feet. Said lots shall have the same minimum lot width and depth of the SFD-9.6 designation.

6. **Building Height** - The height of buildings or structures shall not exceed two stories or thirty (30) feet, whereas a single-story building shall not exceed twenty-five (25) feet.

7. **Front Yard Setback** - The minimum front yard setback shall be provided as follows:

SFD-9.6: The front yard shall be a minimum of eighteen (18) feet to dwelling unit living area or side entry garage, provided that the average setback to the dwelling unit living area or side entry garage shall be no less than twenty (20) feet within a single subdivision. The front yard shall be twenty-three (23) feet to a garage opening directly onto the street. Front yard setbacks shall be staggered to provide more aesthetically pleasing street scene exhibit. No more than two adjacent units shall have the same setback.

SFD-7.2: The front yard shall be a minimum of fifteen (15) feet to dwelling unit living area or side entry garage, provided that the average setback to the dwelling unit living area or side entry garage shall be no less than eighteen (18) feet within a single subdivision. The front yard shall be twenty (20) feet to a garage opening directly onto the street.

Where a single-family detached home fronts on a street where single-family attached homes front on the opposite side of the same street, the front yard setback for said single-family detached home shall be 25 feet. Architectural projections into the required front yard setback are permitted.

8. **Side Yard Setback** - Lots shall have side yards of not less than 5 feet and 10 feet on interior yards, 15 feet adjoining a local street and 20 feet adjoining a collector, secondary or major Street. Fencing erected consistent with Section 9.1.3.11 along local Street side yards may be permitted to encroach into the required side yard setback to the property line. Corona Municipal Code Chapter 17.70 et. seq. standards applicable to corner cut-offs, key lots and reverse corner lots shall apply.

9. **Rear Yard Setbacks** - Lots shall have level rear yards of not less than fifteen (15) feet in the SFD-9.6 and SFD-7.2 designations. Lots adjacent to collector and secondary streets in the SFD-9.6 designation shall have a rear setback of twenty (20) feet. Patio covers, open on three sides, may encroach up to three feet of the rear yard property line.

10. **Coverage** - Maximum lot area coverage by buildings or structures shall be as follows:

SFD-9.6: Single-story buildings/structures - 40% Two-story buildings/structures - 35%

SFD-7.2: Single-story buildings/structures - 45% Two-story buildings/structures - 35%

11. **Walls and Fences** - The provisions of Chapter 17.70 of the Corona Municipal Code shall apply except that solid walls and fences or non-climbable wrought iron fences up to six feet in height shall be permitted where on lot depth blocks are created, and provided that said wall or fence is separated from the adjacent sidewalk of said Street by a minimum of four (4) feet of landscaping, consistent with the appropriate street cross section of this Specific Plan, subject to design approval by the Planning Director.

New residential developments that do not have approved precise plan applications shall comply with residential tract fencing requirements prescribed in Chapter 17.70 of the Corona Municipal Code.

12. **Off Parking** - The provisions of Chapter 17.76 of the Corona Municipal Code shall apply.

9.1.4 SFA - SINGLE FAMILY ATTACHED RESIDENTIAL DISTRICT

1. Purpose
2. Permitted Uses
3. Conditional Uses
4. Prohibited Uses
5. Development Standards for Single-Family Attached
 - A. Density, Lot Area,, Lot Width, Lot Depth
 - B. Building Height
 - C. Front Yard Setbacks
 - D. Side Yard Setbacks
 - E. Rear Yard Setbacks
 - F. Coverage
 - G. Landscaping, Walls and Fences
 - H. Parking
 - I. Minimum Dwelling Unit Area
6. Development Standards for Single-Family Detached Condominiums
 - A. Density
 - B. Building Height
 - C. Setbacks
 - D. Special Setbacks Adjacent to Development of Different Densities
 - E. Coverage
 - F. Landscaping, Walls and Fences
 - G. Parking
 - H. Open Space
 - I. Minimum Dwelling Unit Area

1. **Purpose** - The Single-Family Attached residential district is intended as a planned residential district of single-family attached homes, town homes (by Conditional Use Permit) and/or, single-family detached condominiums, with either one or more dwellings on the same lot. Each and every building and premises or land in the SFA district shall be used for, or occupied and every building shall be, erected, constructed, established, altered, enlarged, maintained, moved into and within said SFA district exclusively and only in accordance with the standards set forth herein.

In the event that portions of this district are located adjacent to residential neighborhoods existing and identified as such at the time of adoption of the South Corona Community Facilities Plan (July 1988), the requirements of Section 9.0.2 of this document shall take precedence over other requirements herein.

2. **Permitted Uses** - The following uses shall be permitted in the SFA district:

Single-family detached dwelling, based upon the provisions of Section 9.1.3 of this Specific Plan.

Single-family detached condominiums, based upon the provisions of Section 9.1.4.6 of this Specific Plan.

Duplex dwellings.

Parks and playgrounds (public and private).

Small family daycare operations.

Large family daycare is permitted with Board of Zoning Adjustment approval.

Private recreational facilities owned and maintained by an individual homeowner, a homeowners' association or other form of community association or maintenance district.

Animals to maximum numbers as follows:

Three weaned cats.

Three weaned dogs.

Home occupations, in accordance with CMC Chapter 17.80.

House trailer may be used as a subdivision construction project office during time of construction when a valid building permit is in force.

Manufactured housing in conformance with provisions established in Chapter 17.81 of the Corona Municipal Code.

A model home complex, including sales office, subject to the review of the Board of Zoning Adjustment according to the procedures described in Chapter 17.98 of the Corona Municipal Code.

Uses customarily incidental to any of the above uses and accessory buildings, including private garages, and servants' quarters, provided no kitchen or kitchen facilities shall be included in any such accessory building.

Similar uses permitted by Commission determination: The Commission may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district. For procedure, see Chapter 17.88 of the Corona Municipal Code.

3. **Conditional Uses** - The following uses may be permitted subject to Conditional Use Permits provided for in Chapter 17.92 of the Corona Municipal Code:

Churches.

Educational Facilities, Public and Private.

Daycare facilities, except family daycare which does not require a conditional use permit.

Senior Citizen's residences.

Town homes of more than two units attached subject to the standards in Section 9.13 of this Specific Plan.

4. **Prohibited Uses** - Uses prohibited in the SFA district are as follows:

Commercial uses;

Manufacturing uses;

5. **Development Standards for Single-Family Attached Dwelling Units**

- A. **Density Lot Area, Lot Width, Lot Depth** - A maximum of eight (8) dwelling units per gross acre shall be permitted in the SFA areas. Number of dwelling units permitted shall be subject to density allocation found on Table 5.2, Allocation of Dwelling Units among Planning Areas.

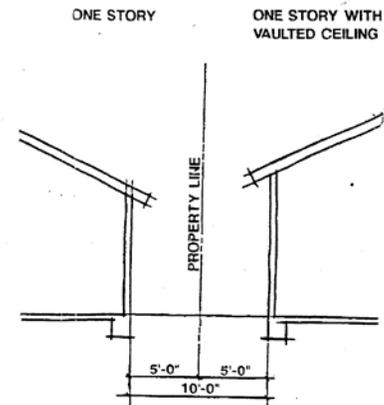
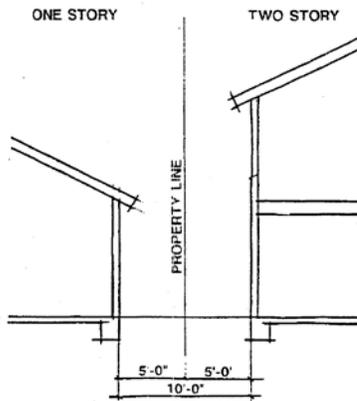
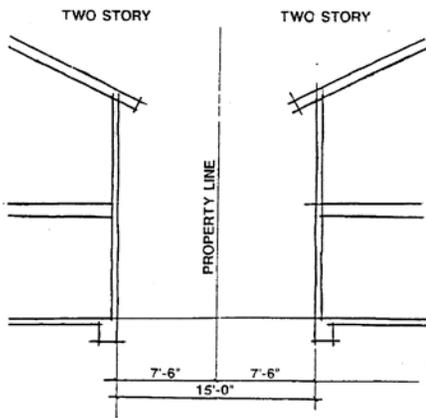
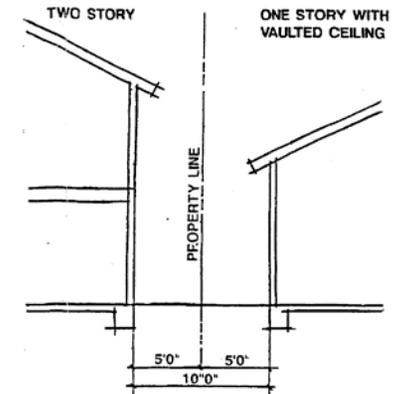
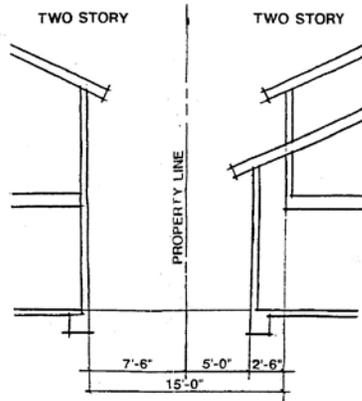
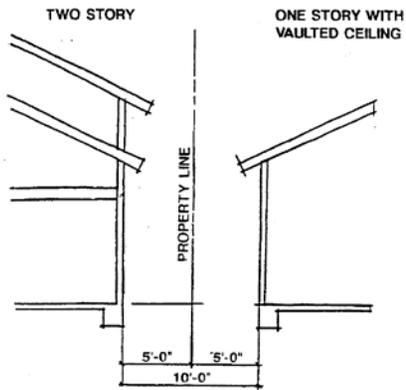
Minimum lot area, width and depth for single-family attached shall be a minimum of 3,600 square feet, 40 feet and 80 feet, respectively.

- B. **Building Height** - the height of buildings or structures shall not exceed forty (40) feet from finished grade, provided that in areas of fire department access at front and rear and to base of windows of living areas, the height shall not exceed thirty (30) feet from finished grade.

- C. **Front Yard Setbacks** - The front yard shall be a minimum of fifteen (15) feet on a local public street or ten (10) feet on a private street when no on-street parking is permitted to private unit living area or side entry garage and/or twenty (20) feet to a garage opening directly onto the street. For purposes of this paragraph, front yard setback from a private street shall be measured from the back of sidewalk. Variable setbacks, as provided for in Corona Municipal Code Section 17.64.040A.3 may also be permitted based upon the provisions of said section.

Where a single-family attached home fronts on a street where single-family detached homes front on the opposite side of the same street, the front yard setback for said single-family attached home shall be 25 feet.

- D. **Side Yard Setbacks** - Lots shall have a minimum first story side yard of five (5) feet, exclusive of all architectural projections except roof overhangs. Roof overhangs may extend up to two-and- one-half (2.5) feet into the required side yard area. Adjacent second story living areas shall be separated by a minimum of fifteen (15) feet. This is illustrated in Exhibit 9.2.



SFA DISTRICT
 SIDE YARD SETBACKS
MOUNTAIN GATE
 LYON COMMUNITIES INC.

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Street side yards shall be a minimum of ten (10) feet on local or private streets, ten (10) feet on a collector street, and fifteen (15) feet on a secondary or major street. Corona Municipal Code Chapter 17.70 et. seq. standards applicable to corner cutoffs, key lots and reverse corner lots shall apply.

- E. **Rear Yard Setbacks** - The intent of this rear yard setback standard is to encourage articulation of the rear elevation of the architecture, and to achieve a useable and livable yard area. The minimum rear yard setback shall be an average of fifteen (15) feet, with no building area, except accessory buildings, closer than ten (10) feet to the rear yard property line.

Where the rear property line of a single-family attached home abuts the property line of a single-family detached home, a 20-foot minimum rear yard is required.

Patio covers, open on three sides, may encroach up to three feet of the rear yard property line.

- F. **Coverage** - Lot area coverage by buildings or structures shall not exceed fifty (50) percent of the total lot area.

- G. **Landscaping, Walls and Fences** - Front yard landscaping shall be installed with an irrigation system by the project developer prior to Certificate of Occupancy, according to the following minimum standards:

1. Builder to submit plans and specifications prepared by a landscape architect to the City of Corona for approval.
2. Front yard landscaping for each home to include permanent automatic irrigation system.
3. Two 15-gallon trees to be planted in the front yard behind the sidewalk, in addition to any required Street trees in the parkway.
4. Entire front yard including parkway (where applicable) to be soil prepared and seeded with turf in the designated turf areas.
5. A minimum of ten (10) percent of the front yard landscaping area shall be soil prepared and irrigated for shrub planting.
6. For purposes of this section, the front yard area which is required to be landscaped shall exclude any courtyard or patio area adjacent to the house which is separated from the rest of the front yard by an opaque wooden fence or masonry wall which screens said courtyard or patio area from view from the street.

The provisions of Chapter 17.70 of the Corona Municipal Code shall apply except that solid walls and fences or non-climbable wrought iron fences up to six feet in height shall be permitted where one lot depth blocks are created, and provided that said wall or fence is separated from the sidewalk of said street by a minimum of four (4) feet of

landscaping consistent with the appropriate street cross-section of this specific plan, subject to design approval by the Planning Director.

Wood, masonry or non-climbable wrought iron fencing, not to exceed six (6) feet in height, shall be constructed along all interior lot lines excluding lot lines in front yards. Materials and design of the fencing shall be subject to the approval of the Planning Director.

- H. **Parking** - The provisions of Chapter 17.76 of the Corona Municipal Code shall apply except that 2.0 off-street parking spaces per unit within an enclosed garage shall be provided and 0.25 guest spaces shall be provided. Guest parking may be provided on the street or in parking bays subject to the approval of the Public Works Director.
- I. **Minimum Dwelling Unit Area** - There shall be a minimum of 1,100 of floor area for senior citizen's residences and family residences, in each dwelling exclusive of open porches, patios and garages.

6. **Development Standards for Single-Family Detached Condominiums**

- A. **Density** - A maximum of eight (8) dwelling units per gross acre shall be permitted in the SFA areas. The number of dwelling units permitted shall be subject to density allocation found on Table 5.2 Allocation of Dwelling Units Among Planning Areas.
- B. **Building Height** - The height of buildings or structures shall not exceed 30 feet from finished grade and two stories.
- C. **Setbacks** - The setbacks shall be measured from the project boundary, street right-of-way, edge of private street easements, or boundary of exclusive use area which is to include front, rear and side yard .areas associated with each dwelling unit, as appropriate according to the following specifications:
 - 1) **Street/Dwelling Unit Setback:** Setbacks shall be a minimum of ten (10) feet for a private street, fifteen (15) feet for a local or collector, and twenty (20) feet for a secondary or major arterial. Fencing erected consistent with Section 9.16.10 along local street yards may be permitted to encroach into the required yard setback to the edge of the property right-of-way or private Street easement if adjacent to a landscaped lot or parkway. Architectural projections such as porches and balconies may extend three (3) feet into the setback where such setback is a minimum of 20 feet.
 - 2) **Garage/Street Setback:** Front-on garages shall have a minimum setback of twenty (20) feet.
 - 3) **Rear yard setbacks** shall be a minimum of ten (10) level feet useable yard area. Rear yard setbacks shall average fifteen (15) level feet. Patio covers, open on three sides may encroach up to three (3) feet of the rear yard boundary.

Building Separation: Ten (10) feet shall separate residential buildings. Roof overhangs may extend up to two-and-one-half (25) feet onto the required yard area. No architectural projections are permitted into the minimum five (5) foot side yard between the unit and fence line.

- D. **Special Setbacks Adjacent to Residential Development of Different Densities** - Units which abut or are adjacent to fee simple single-family detached units shall be separated by a setback of not less than twenty (20) feet.
- E. **Coverage** - Lot area coverage by buildings or structures shall not exceed forty (40) percent of the total exclusive use area.
- F. **Landscaping, Walls and Fences** - Front yard landscaping shall be installed with an irrigation system by the project developer prior to Certificate of Occupancy, according to the following minimum standards.
- 1) The builder is to submit plans and specifications prepared by a licensed landscape architect to the City of Corona for approval.
 - 2) Front yard landscaping for each home is to include permanent automatic irrigation system
 - 3) Two 15 gallon trees are to be planted in the front yard behind the sidewalk, in addition to any required street trees in the parkway.
 - 4) The entire front yard, including parkway (where applicable), is to be soil prepared and seeded with turf in the designated turf areas.
 - 5) A minimum of ten percent of the front yard landscaping area shall be soil-prepared and irrigated for shrub planting.
 - 6) For purposes of this section, the front yard area required to be landscaped shall exclude any courtyard or patio area adjacent to the house which is separated from the rest of the front yard by an opaque wooden fence or masonry wall which screens said courtyard or patio area from view from the street.

The provisions of Chapter 17.70 of the Corona Municipal Code shall apply except that solid walls and fences or non-climbable wrought iron fences up to six feet in height shall be permitted where one dwelling unit depth blocks are created, and provided that said wall or fence is separated from the sidewalk of said street by a minimum of four (4) feet of landscaping consistent with the appropriate street cross-section of this Specific Plan, subject to design approval by the Planning Director.

Wood, masonry or non-climbable wrought iron fencing, not to exceed six (6) feet in height, shall be constructed along all side and rear perimeters of all dwelling units (excluding front yards). Material and design of the fencing shall be subject to the approval of the Planning Director.

- G. **Parking** - The provisions of Chapter 17.76 of the Corona Municipal Code shall apply except that two (2) off-street parking spaces per unit within an enclosed garage shall be provided and 0.25 guest spaces shall be provided. Guest parking may be provided on the street or in parking bays subject to the approval of the Public Works Director.
- H. **Open Space** - A combination of useable private and common open space shall be provided in the minimum average amount of 2,460 square feet per dwelling unit. Usable private open space includes landscaped yard areas at grades of less than 10 percent. Private space associated with each unit on the front, side or rear yards may be counted as open space.

Common open space includes community recreation facilities such as swimming pools, spas, tennis courts, putting greens and similar outdoor recreation spaces; bikeways and pedestrian walkways, greenbelts and other open space. This may include deed restricted or use restricted common property or property on greenbelts or on open space property dedicated to the Landscape Maintenance District. Of these 2,460 square feet of common/private open space, a minimum of 100 square feet per unit shall be provided in common recreational facilities; such as recreation buildings, pools, spas, children's playgrounds, tennis or other court sport areas and ancillary use areas. Where perimeter landscaping is adjacent to an arterial street and is the responsibility of the Landscape Maintenance District, such area shall not be counted in the minimum average requirement for 2,460 square feet per unit.

Open space associated with each dwelling unit, including front, rear and side yards as defined in the purchase agreements as "Exclusive Use Areas" shall be maintained by the homeowner. Other common areas such as greenbelts, parkway or median landscaping and community recreation areas shall be the responsibility of the homeowners association. Greenbelt areas and landscape lots adjacent to arterial streets may be acceptable for maintenance by Landscape Maintenance District subject to review and approval of the Director of Public Works. The Planning Commission shall have discretion regarding the location, type of amenities, and appropriate combination of private and common open space areas.

- I. **Minimum Dwelling Unit Area** - There shall be a minimum of 1,000 square feet of floor area for each dwelling exclusive of open porches, patios and garages.

9.1.5. MDR. Medium Density Residential District

- 1. Purpose
- 2. Permitted Uses
- 3. Conditional Uses
- 4. Prohibited Uses
- 5. Density
- 6. Building Height
- 7. Special Setbacks adjacent to Residential Development of Different Densities
- 8. Front Yard Setbacks
- 9. Side Yard Setbacks
- 10. Rear Yard Setbacks
- 11. Distance Between Buildings
- 12. Coverage
- 13. Parking
- 14. Minimum Dwelling Unit Area
- 13. Open Space Requirement
- 16. Trash Pick-up Area
- 17. Enclosed Storage Area
- 18. Landscaping

1. **Purpose** - The Medium Density Residential District is intended as a planned residential district of medium density multiple family dwellings with either one or more dwellings on the same lot. Except as specifically provided elsewhere in the title, any and every building and premises or land in the MDR district shall be used for or occupied and every building shall be erected, constructed, established, altered, enlarged, maintained, move into and within said MDR district exclusively and only in accordance with the standards set forth.

In the event that portions of this district are located adjacent to existing residential neighborhoods existing and identified as such at the time of adoption of the south Corona Community Facilities Plan (July 1988), the requirements of Section 9.0.2 of this document shall take precedence over other requirements herein.

2. **Permitted Uses** - The following uses shall be permitted in the medium density residential district:

Duplex dwellings based upon the SFA standards in section 9.1.4 of this Specific Plan.

Town home dwellings no more than eight ground floor units attached.

Cluster housing, as defined in section 9.1.2 of this Specific Plan, and multiple family dwellings.

Small family day care.

Large family day care is permitted with Board of Zoning Adjustment approval.
Parks and playgrounds (public and private).

Private recreational facilities, owned and maintained by an individual homeowner, a homeowners' association or other form of community association or maintenance district.

Animals to maximum numbers as follows:

Two weaned cats;

Two weaned dogs.

Home occupations, in accordance with CMC Chapter 17.80.

House trailer, may be used as a subdivision construction project office during time of construction when a valid building permit is in force.

Manufactured housing in conformance with provisions established in Chapter 17.81 of the Corona Municipal Code.

Model home complex including sales office subject to the review of the Board of Zoning Adjustment according to the procedures described in Chapter 17.98 of the Corona Municipal Code.

Uses customarily incidental to any of the above uses and accessory buildings, including a private garage, and servants' quarters, provided no kitchen or kitchen facilities shall be included in any such accessory building.

Similar uses permitted by Commission determination: The Commission may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district. For procedure, see Chapter 17.88 of the Corona Municipal Code.

3. **Conditional Uses** - The following uses may be permitted subject to conditional use permits provided for in Chapter 17.92 of the Corona Municipal Code:

Churches;
Educational Facilities, Public and Private;
Day care facilities, except family day care which does not require a conditional use permit;
Senior Citizen's Residences;
Congregate Care Facilities;
Health Care Facilities.

4. **Prohibited Uses** - Uses prohibited in the MDR district are as follows:

Commercial uses;
Manufacturing uses;

5. **Density** - A maximum of 13 units per gross acre shall be permitted. Number of dwelling units permitted shall be subject to density allocation found on Table 5.2, Allocation of Dwelling Units among Planning Areas.
6. **Building Height** - The height of buildings or structures shall not exceed forty (40) feet from finished grade, provided that in areas of fire department access at front and rear and to the base of the windows of living areas, the height shall not exceed thirty (30) feet from finished grade.
7. **Special Setbacks adjacent to Residential Development of Different Densities** - Development herein shall conform to the standards included within Section 3.1.3.5 and Figure 3.1-2 of the south Corona Community Facilities Plan.
8. **Front Yard Setbacks** - The perimeter front yard setback shall be a minimum of 20 feet; no parking shall be allowed within the required front yard setback.
9. **Side Yard Setbacks** - Perimeter side yard setbacks shall be 10 feet. Perimeter street side yards shall be a minimum of ten (10) feet on local, private, or collector streets, and fifteen (15) feet on a secondary or major street.
10. **Rear Yard Setbacks** - Perimeter rear yard setbacks shall be 10 feet.
11. **Distance Between Buildings** - The minimum distance between dwellings shall be not less than ten (10) feet for one-story buildings, 15 feet for two-story buildings and 20 feet for three-story buildings.
12. **Coverage** - Lot area coverage by buildings or structures shall not exceed fifty (50) percent of the total lot area.

13. **Parking** - The provisions of Chapter 17.76 of the Corona Municipal Code shall apply except that 2.0 off-street parking spaces per unit shall be provided for two or more bedroom units, 1.5 spaces off-street parking per unit shall be provided for one bedroom units, and 0.25 guest spaces shall be provided for each unit. One covered space shall be provided for each unit. Detached covered parking spaces shall be located within 200 feet of the assigned dwelling unit.

Townhouse units shall include a two car enclosed garage for each unit and provide 0.25 guest spaces per unit.

14. **Minimum Dwelling Unit Area** - There shall be a minimum of six hundred (600) square feet for studio units and nine hundred (900) square feet for all others of floor areas in each dwelling exclusive of open porches, patios and garages.

15. **Open Space Requirements**

- a.. A total of 200 square feet of open space shall be established per dwelling unit. A minimum of fifty (50) square feet of this total shall be private open space for the personal and exclusive use of the dwelling occupant it is designed to serve. Said private open space shall include any of the following

Patios	Decks
Balconies	Others deemed similar
Porches	by the Planning Director

- b. Of the remaining required open space, a minimum of one hundred (100) square feet of active common open space shall be established for the joint use of, and to be shared equally by the dwelling occupants it is intended to serve. This active common open space shall include swimming pool(s) and tot lot(s); it may optionally include tennis courts, mini-parks, recreational rooms, or others deemed similar by the Planning Director.

16. **Trash Pickup Area** - If attached garage is not provided, trash pickup areas shall be provided at a ratio of one trash enclosure for each ten dwelling units. The trash enclosure shall be to City standards.
17. **Enclosed Storage Area** - if attached garage is not provided, an enclosed storage area of not less than 100 cubic feet, shall be provided for each dwelling unit. The minimum dimension shall be three (3) feet.
18. **Landscaping** - Landscaping shall be provided in accordance with section 17.70 of the Corona Municipal Code.

9.2 PARKS (P), GREENBELT PARK (GP), OPEN SPACE (OS) and ELEMENTARY SCHOOL (ES) DISTRICTS

1. Purpose
2. Permitted Uses and Structures in Planning Areas 11, 18, 22 and 23
3. Permitted Uses in Planning Area 32
4. Conditional Uses and Structures
5. Lot Area, Lot Width and lot Depth
6. Setbacks
7. Building Height
8. Walls and Fences
9. Parking
10. Park Plans
11. Reversion to Residential Use

1. **Purpose** - The Parks, Greenbelt Park, Open Space and Elementary School Districts are established to provide for permanent open space by designating lands for public and private recreational facilities, elementary schools, and other public facilities.

The location of parks, schools and other permitted facilities such as water and wastewater storage or transmission facilities, electrical substations, communication facilities, etc., shall be in substantial conformance with the South Corona Community Facilities Plan.

2. Permitted Uses and Structures in Planning Areas 11, 18, 22 and 23

- a. Public recreation uses including but not limited to:
Parks,
Playgrounds, and athletic areas;
Sport activities;
Bicycle and pedestrian paths;
Picnic areas;
Swimming areas
- b. A Historic Park may be established in Planning Area 18, which may include, but not be limited to: museums, educational and cultural facilities, agricultural uses, recreational facilities, offices, restaurants, and related commercial ventures. The specific structures and uses shall be determined in conjunction with a precise plan for the property.
- c. Public facilities and service uses and structure including flood control areas not requiring continuous occupancy or use by the public.
- d. Similar uses permitted by Commission determinations: The Commission may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district. For procedure, see Chapter 17.88 of the Corona Municipal Code.

3. **Permitted Uses in Planning Area 32**

- a. Rural pedestrian paths.
- b. Access roads or streets that provide access to the property located south of PA 32
- c. Debris basins

4. **Conditional Uses and Structures**

- a. Public utility communication towers
- b. Elementary Schools, except in PA 32
- c. Satellite dish receiver stations
- d. Public facilities and service uses and structures requiring continuous occupancy or use by the public, except in PA 32.

5. **Lot Area, Lot Width and Lot Depth** - No minimums are established. Lot area, width and depth should be appropriate for the use contemplated, subject to the approval of the Planning Director.

6. **Setbacks** - Setbacks for the structure are as follows:

- a. Front yards shall be minimum of 20 feet; no parking shall be permitted within a required front yard.
- b. Side yard setbacks shall be a minimum of 10 feet,
- c. Rear yard setbacks shall be a minimum of 20 feet.

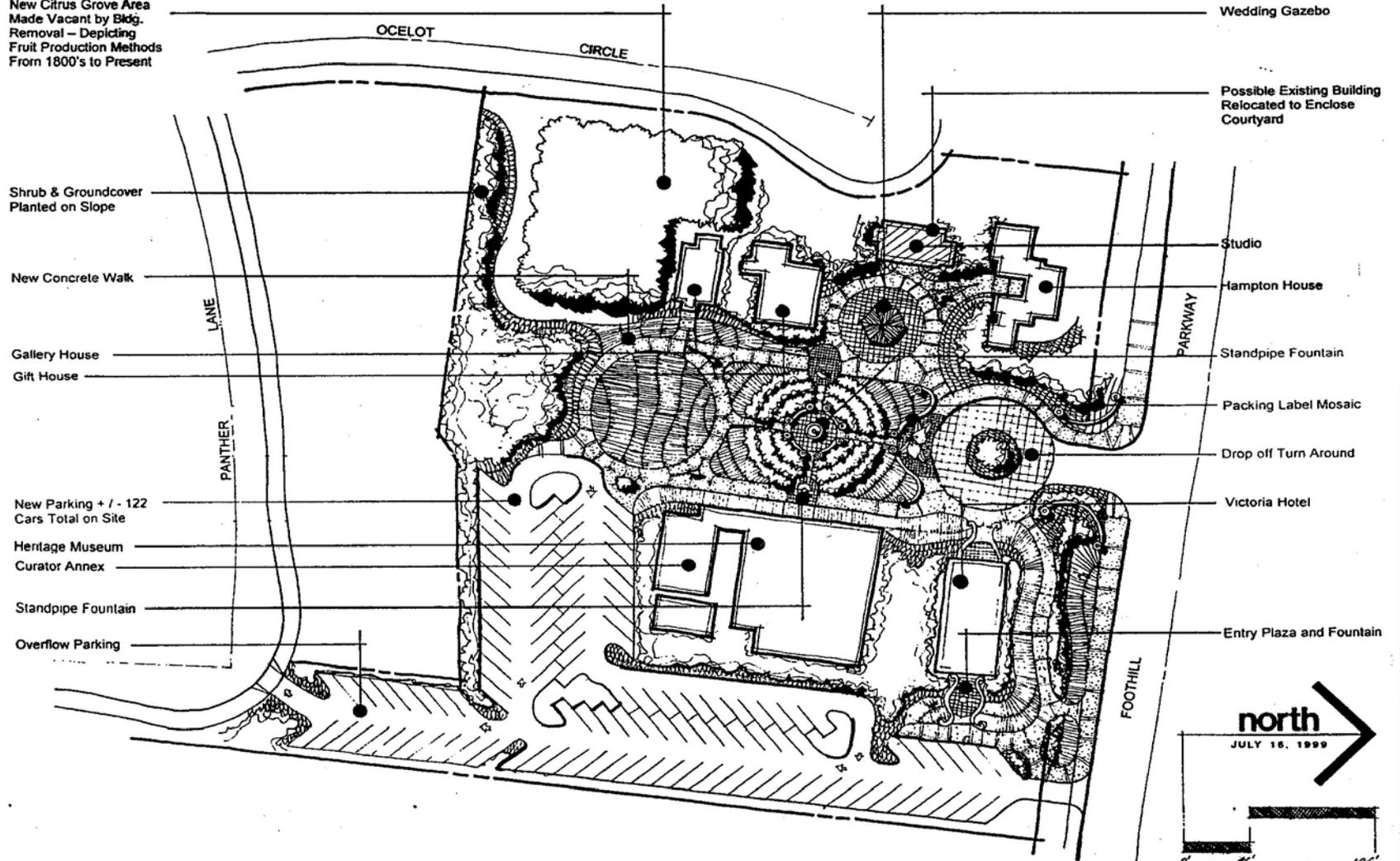
7. **Building Height** - Structures directly adjacent to residential districts shall not exceed two stories in height or 30 feet. Structures directly adjacent to commercial zones may be a maximum of 45 feet in height.

8. **Walls and Fences** - The provisions of this Specific Plan regarding theme walls and the Corona Municipal Code Chapter 17.70 shall apply.

9. **Parking** - The provisions of Corona Municipal Code Chapter 17.76 shall apply.

10. **Park Plans** - All park plans shall be reviewed by DPR and the Parks and Recreation Commission for approval prior to the submittal of building plan check. The Heritage Park will require a DPR review and precise plan approval by the Planning Commission and City Council. Specific uses, structures, hours of operation, programs and signage shall be identified as part of the precise plan submittal.

New Citrus Grove Area
Made Vacant by Bldg.
Removal - Depicting
Fruit Production Methods
From 1800's to Present



Shrub & Groundcover
Planted on Slope

New Concrete Walk

Gallery House

Gift House

New Parking +/- 122
Cars Total on Site

Heritage Museum

Curator Annex

Standpipe Fountain

Overflow Parking

Wedding Gazebo

Possible Existing Building
Relocated to Enclose
Courtyard

Studio

Hampton House

Standpipe Fountain

Packing Label Mosaic

Drop off Turn Around

Victoria Hotel

Entry Plaza and Fountain

**CONCEPTUAL FOOTHILL
RANCH HERITAGE PARK**

north

JULY 16, 1999



KAMMEYER & ASSOCIATES
2837 KELLOGG AVENUE
CORONA, CALIFORNIA 92644

9.3 COMMERCIAL DEVELOPMENT STANDARDS

1. Purpose
2. Permitted Use
3. Uses permitted subject to Conditional Use Permit
4. Prohibited Uses
5. Architectural Guidelines
6. Site Development Standards

1. **Purpose** - The purpose of these provisions is to guide the planning, design, development and operation of the Mountain Gate commercial areas. These standards are intended to develop a sense of place within the commercial areas, strengthen the interaction between commercial and residential areas in proximity to one another, and facilitate a more efficient use of adjoining and community circulation systems.
2. **Permitted Uses** - Uses permitted by Section 3.1.5 of the South Corona Community Facilities Plan; and Day Care Facilities.
3. **Conditional Uses** - Uses permitted by Section 3.1.5 of the South Corona Community Facilities Plan; and churches exceeding an overall combined floor area of 10,000 square feet by major conditional use (CUP); and churches not exceeding an overall combined floor area of 10,000 square feet by minor conditional use permit (MCUP).
4. **Prohibited Uses.** - Uses prohibited by Section 3.1.5 of the South Corona Community Facilities Plan.
5. **Architectural Guidelines** - The standards of Section 3.3.6 of the South Corona Community Facilities Plan.
6. **Site Development Standards** - The community design standards included with Section 3.3 of the South Corona Community Facilities Plan shall apply to commercial development within Mountain Gate. Where the South Corona Community Facilities Plan does not include specific site development standards, the following standards shall apply:
 - a. **Minimum front yard building setbacks:**
From streets:
The standards included within Section 3.1 (Figure 3.1-2) and 3.3.1 and 3.3.2 of the South Corona Community Facilities Plan;
From property line adjacent to residential uses:
The standards included within Section 3.1 (Figure 3.1-2) and 3.3.1 and 3.3.2 of the South Corona Community Facilities Plan;
 - b. **Minimum building separation:** 10 feet;
 - c. **Maximum building height:** Forty (40) feet;
 - d. **Fences and Walls:** All fences and walls shall comply with Corona Municipal Code Chapter 17.70;

- e. Off-street parking: Off-street parking shall comply with Corona Municipal Code Chapter 17.76.
- f. Landscaping: The standards included within Section 3.3.7 of the South Corona Community Facilities Plan shall apply.
- g. Lighting: Exterior lighting is required for all parking areas, walkways and building entrances. All lighting shall be designed and located to confine direct rays to the premises.
- h. Loading: All loading shall be performed on the site. Loading areas and platforms shall be screened from view from adjacent streets and residential areas.
- i. Trash and storage area: All storage, including cartons, containers or trash, shall be shielded from view by containment within a building or area enclosed by a wall a minimum of five feet in height. Materials and colors shall be consistent with that used for the structure. Trash enclosures shall comply with the Planning Director's standards.
- j. Screening: Shall be installed and maintained subject to the following requirements:
All building operating mechanical equipment shall be screened from view.

An opaque screen shall be installed along all exterior boundaries, other than streets, where the parcel abuts areas designated for residential uses. Said screens shall consist of one, or combination of, masonry wall, earthen berm or dense evergreen plant material, and shall have a total height of not less than 6 feet.

Screening shall be no higher than 3-1/2 feet within 20 feet of the point of intersection of any vehicular access way and a street, sidewalk or other vehicular access way measured from the property line.

7. **Commercial Sign Standards** - Signs shall be regulated and controlled as specified under Section 17.74 of the Corona Municipal Code for the Mountain Gate Specific Plan; except for modifications as herein stated:

- a. No signs shall be constructed or maintained on the roof of any buildings.
- b. Signs shall contain only such subject matter which refers to the name of the establishment or to the goods and services (no brand names) sold on the premises on which the sign is located.
- c. Center identification monument signs are permitted for the Commercial land use designation subject to the following standards:
 - 1. A monument sign not to exceed twelve (12) feet in height, with a maximum of sixty (60) square feet per copy face shall be permitted for both the Main Street and Foothill Parkway frontages.

2. A maximum of four tenants may be identified on each sign.
 3. The sign shall not be located in the public right-of-way or within the corner cut-off.
 4. The sign shall be architecturally compatible with the design of the shopping center.
- d. The maximum area of enterprise identification signs shall be two (2) square feet per lineal foot of leased frontage up to 200 square feet for major tenants (6,000 square feet of floor area or more). Minor tenants (less than 6,000 square feet of floor area) signage shall not exceed one (1) square foot per lineal foot of store frontage and shall not be taller than 24 inches in height.
- e. Monument style parcel identification signs shall be permitted subject to the following:
1. The sign shall identify the name of the shopping center.
 2. The building/use associated with the sign shall be located on a separate legal lot with a single tenant in the building.
 3. The building shall be located within 50 feet of the right-of-way.
 4. The sign shall not be located in the right-of-way or corner cut-off area.
 5. The sign shall not exceed a height of six (6) feet or a width of eight (8) feet.
 6. The sign shall contain only the name of the business in the associated building, with a maximum sign face area of 24 square feet.
 7. The sign shall incorporate the same architectural features as that of the main shopping center.
- f. Signs which move, or which have moving parts or flashing lights, shall not be permitted.
- g. In order to achieve continuity of signage, a sign program shall be developed for the planning area designated "Commercial" to be reviewed and approved by the Planning Commission or by design review, as provided by City ordinance.

9.4 QUASI-PUBLIC (QP) DISTRICT

1. Purpose
2. Permitted Uses and Structures
3. Conditional Uses and Structures
4. Lot Area, Lot Width and Lot Depth
5. Setbacks
6. Walls and Fences
7. Parking
8. Signage

1. **Purpose** - The purpose of the Quasi-Public (QP) District is to establish a location for government or other public-serving uses that are necessary to serve the day-to-day needs of the project and area residents. These uses often involve moderate traffic and thus may require better access than would be afforded by a local residential street. These include uses such as fire stations, post offices, libraries, government centers, child care facilities, churches and professional offices.

2. **Permitted Uses and Structures:**

- a. Professional offices including, but not limited to, medical offices, dental offices, professional services, financial institutions and real estate offices.
- b. Public and quasi-public facilities and service uses including, but not limited to the following:
 - Fire stations
 - Post offices
 - Libraries
 - Government centers
 - Day care facilities
 - Churches or other places of religious assembly
 - Recreation centers
 - Health clubs
 - Theaters
- c. Similar uses permitted by Commission determination: The Commission may, by resolution of record, permit any other uses which it may determine to be similar to those listed above, and not more obnoxious or detrimental to the public health, safety and welfare, or to other uses permitted in this district. For procedure, see Chapter 17.88 of the Corona Municipal Code.

3. **Conditional Uses and Structures:**

- a. Elementary Schools

4. **Lot Area, Lot Width and Lot Depth** - No minimums are established. Lot area, width and depth should be appropriate for the use contemplated, subject to the approval of the Planning Director.
5. **Setbacks** - Setbacks for structures are as follows:
 - a. Front yards shall be a minimum of 20 feet; no parking shall be permitted within a required front yard.
 - b. Side yard setbacks shall be a minimum of 10 feet.
 - c. Rear yard setbacks shall be a minimum of 20 feet.
6. **Walls and Fences** - The provisions of Corona Municipal Code Chapter 17.70 shall apply.
7. **Parking** - The provisions of Corona Municipal Code Chapter 17.76 shall apply.
8. **Signage:**
 - a. Monument style parcel identification signs shall be permitted on free-standing building lots adjacent to any public street; no such signs are permitted within the public right-of-way, but may be permitted within adjacent landscape lots. The signs shall not exceed 33 feet in height and 18 square feet in area per face. No other parcel identification signs shall be permitted with the exception of that permitted in subsection c above.
 - b. Enterprise signs are permitted in accordance with the provisions of section 93.7.d.
9. **Transfer of Underlying Residential Units**

In the event that planning area 9 (Quasi-Public District) is developed with other than public, non-profit facilities, the underlying residential units may not be assigned to other planning areas within the Specific Plan, and are thereby considered transferred to the City for reallocation through the target density provisions of the south Corona Community Facilities Plan.

9.5 SIGNAGE

9.5.1 Introduction

Subdivision signs are an important factor in establishing the overall character of a new community. Signage within Mountain Gate will complement the architecture and landscaping of the community.

In order to allow for the development of a unique identity for each neighborhood while maintaining design continuity throughout the Mountain Gate Community, a hierarchy of signs utilizing a consistent design theme has been developed.

9.5.2 Purpose and Intent

The purpose of the Sign Program is to provide uniform standards for builders and developers to use when obtaining sign permits within Mountain Gate. In addition, the program will require all signs to be harmonious with one another, as well as harmonizing with the architecture and character of the community.

It is the intention of the Mountain Gate Subdivision Sign Program to provide a balanced and attractive sign environment for the overall Mountain Gate Community.

Furthermore, the intent of the sign program is to:

- Provide a reasonable system for sign control, based on a set specific sign standards designed solely for Mountain Gate.
- Create sign forms, color, and designs which complement the setting and character of the community.
- Minimize visual clutter by keeping the number of signs and their size to a minimum.
- Maintain a continuity of design from one sign type to another.
- Prevent sign users from developing signs which may conflict with each other and not relate to the overall graphic design concept.
- Attract and direct people to various activities and projects in order to provide for maximum public convenience.
- Reduce possible traffic and safety hazards to motorists, pedestrians and animals through sound sign practices.
- Promote the public health, safety and general welfare of Mountain Gate and each of its various neighborhoods.

9.5.3 DEFINITIONS

- A. **Animated Sign**: A sign designed to attract attention through movement or the resemblance of movement of the whole or any part of the sign.
- B. **Apartment Sign**: A sign which identifies and provides leasing information for apartment complexes.
- C. **Flags**: A banner which contains colors/patterns/project names and/or logos.
- D. **Future Facility Sign**: A monument sign which directs attention to a future development (i.e. school, park, shopping center, church, etc.).
- E. **Identification Sign**: A freestanding sign which directs attention to a new community, subdivision or phase.
- F. **Information Sign**: Small double-post signs located at model complexes and containing information about the subdivision and/or models.
- G. **Logo**: A visual symbol identifying the builder, and/or the developer, and/or the project.
- H. **Parking Lot Sign**: Small double column signs located at model complexes which direct the public to the models.
- I. **Removable Directional Sign**: A small, portable sign providing temporary directions to a particular subdivision or phase being marketed.
- J. **Sales Center Identification Sign**: A monument sign which identifies the location of a sales office for a specific subdivision or phase.
- K. **Sign Face**: The area encompassing the outside of the lettered portion of the sign. The portion of the sign which encompasses the project name and/or message, excluding sign structure.
- L. **Sign Height**: The maximum vertical dimension of a sign or sign structure measured from the top of the sign to the ground directly below it.
- M. **Sign Width**: Maximum horizontal dimension of a sign or sign structure, excluding any wall to which it may be affixed.
- N. **Subdivision Directional Sign**: A temporary multi-paneled monument sign which directs the public to specific phases and subdivisions within the project area.
- O. **Total Sign Area**: The sign face and the sign base, excluding any wall upon which such sign is affixed.
- P. **Wall Monumentation Sign**: A permanent sign which is mounted on the exterior face of the perimeter wall at the community entrance.

9.5.4 PROHIBITED SIGNS

- A. Animated, flashing audible and intermittent signs.
- B. Portable signs other than those identified specifically herein to be used for directional purposes.
- C. Signs incorporating mechanical movement.
- D. Vehicle-mounted signs.
- E. Painted wall signs.
- F. Signs that by color, wording, design, location, or illumination resemble or conflict with any traffic control device or with the safe and efficient flow of traffic.
- G. Signs that create a safety hazard by obstructing clear view of pedestrian and/or vehicular traffic.

9.5.5 APPROVAL REQUIREMENTS

- A. These sign criteria shall be strictly enforced by the City of Corona, the master developer and builders of the project. Any non-conforming or illegally installed signs shall be removed by the builder or his sign contractor, at their expense, upon request by the City, per the Corona Municipal Code Section 17.74.280 and 17.74.300.
- B. All signs shall be subject to approval, in writing, from the master developer prior to submittal to the City.
- C. All signs will be limited to developer/builders trade name, logo, name of the project, public service information provided for the City, facility names, hours of operation, directional information and the like. All signs may be double- faced.
- D. The master developer and the builders shall be responsible for periodic inspection of signs. All signs and sign structures, as well as the immediate premises surrounding them, shall be well maintained by the master developer and the builders. Upon notice by either the City of Corona or the master developer, a builder shall be required to repair or refurbish any sign structure, or sign panel within seven (7) days.
- E. No sign or sign structure shall be erected at any location where, by reason of the position, shape, size, or color, it may obstruct or interfere with the view of any traffic control device or directional sign.
- F. All signs shall be built in accordance with the specifications of the Uniform Building Code and specifications of the Building and Safety department. Placement of signs shall be in accordance with all City permit requirements.
- G. Where these regulations differ from the City of Corona Municipal Code Section 17.74 "Signs," these regulations shall govern. Where these regulations do not discuss a specific condition, the non-conflicting portions of Section 17.74 shall apply.
- H. Liability insurance shall be maintained by the sign contractor as required by the City of Corona.
- I. Prior to construction of any sign included herein, a permit subject to the standard City approval process shall be obtained as follows:

Section 9.5.6 - Wall Monumentation Signs - Planning Department - Counter Permit

Section 93.7 - Subdivision Directional Signs - Board of Zoning Adjustment

Section 9.5.8 - Identification Signs - Planning Department - Counter Permit

Section 93.9 - Flags - Board of Zoning Adjustment - Model Home Permit

Section 9.5.10 - Sales Information Signs - Board of Zoning Adjustment - Model Home Permit

Section 93.11 - Future Facility Signs - Planning Department

Section 93.12 - Apartment Signs - Corona Municipal Code Requirements

Section 93.13 - City Kiosk Signs - Board of Zoning Adjustment - City Kiosk Program.

- J. Where signs are located on the right-of-way or Landscape Maintenance District (LMD) area, appropriate encroachment permits shall be obtained. Developer shall be responsible for maintenance and eventual removal of temporary signs and associated facilities and shall bond for such removal. Where permanent signs are located in the Landscape Maintenance District area, the design shall be such that signs are constructed of durable, low maintenance materials, without lighting, and placed on a public or private wall such that it does not create line of sight obstruction. Each sign design and approval shall be subject to review and approval of the Director of Public Works and the Director of Parks and Recreation.
- K. Signs and flags shall not be placed on public property, either the public right-of-way or Landscape Maintenance District lots, unless an encroachment permit is obtained pursuant to Corona Municipal Code Chapter 12 and City Council Resolution 85-145.

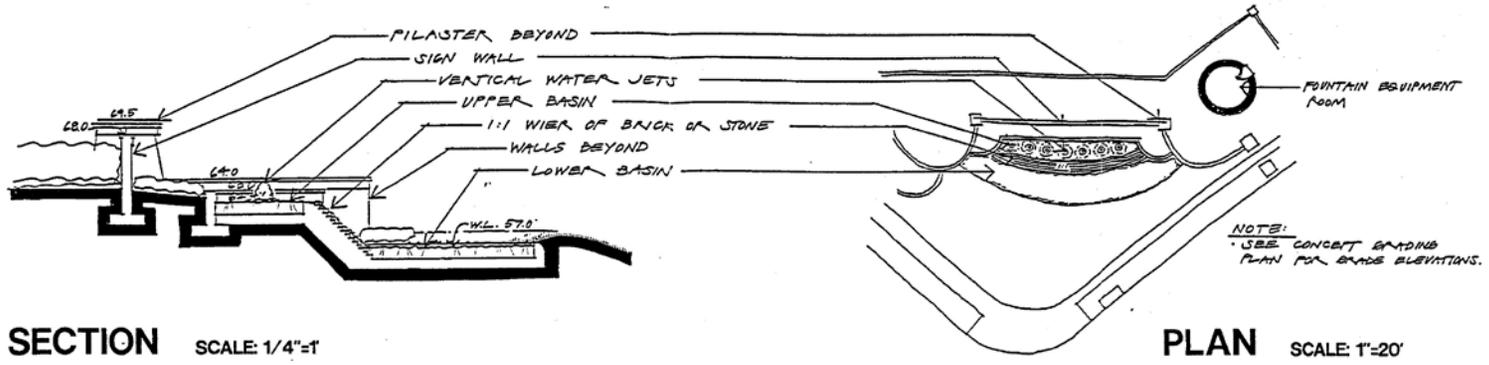
9.5.6 WALL MONUMENTATION SIGN REQUIREMENTS

A. Main Entryway Signs (Exhibit No. 9.5)

1. **PURPOSE:** The primary entryway statement which informs viewers of a major entrance to the planned community.
2. **LOCATION:** On both sides of the intersections of Foothill Parkway and Main Street and Foothill Parkway and Lincoln Avenue.
3. **SIZE:** Height = 7' to top of wall.
Sign Face = 6' in height, 22' in width.
Total Sign Face = Maximum 80 square feet.
4. **MATERIALS:** Materials shall be durable and may include concrete, stucco, decorative block, wood, stone, brick and composition shingle Sign letters may be cast, recessed, tile, marble or brass. All materials are subject to Community Development Director Approval.
5. **ILLUMINATION:** The entryway signs shall be externally illuminated, with flood lights, which are water and tamper-proof. Such lighting shall be removed 60 days after completion of 90 percent of the community.
6. **WATER FEATURES:** Where water is utilized, such water system shall be removed 60 days after completion of 90% of the community and said fountain area converted to a planter.

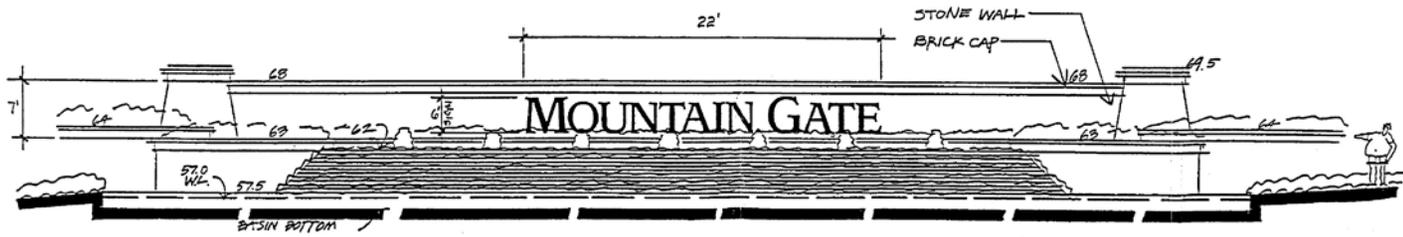
B. Secondary Entryway Sign (Smaller version without water, Exhibit No. 9.5)

1. **PURPOSE:** An entryway statement which informs the public of the secondary entrances to the planned community. These are permanent signs.
2. **LOCATION:** On both sides of the intersections of Garretson Avenue and Foothill Parkway, Garretson Avenue and Upper Drive and Highgrove Street and Foothill Parkway.
3. **SIZE:** Height = 7' to top of wall.
Sign Face = 5' in height, 19' in width.
Total Sign Face = Maximum 44 square feet.
4. **MATERIALS:** Materials shall be durable and may include concrete, stucco, decorative block, stone, and brick. Sign letters may be cast, recessed, tile, marble or brass. All materials are subject to Community Development Director.
5. **ILLUMINATION:** The entryway signs shall be externally illuminated with flood lights, which are water and tamper-proof. Such lighting shall be removed 60 days after completion of 90 percent of the community.



SECTION SCALE: 1/4"=1'

PLAN SCALE: 1"=20'



ELEVATION SCALE: 1/4"=1'

MAIN ENTRY SIGN
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/12



C. Subdivision Entryway Sign (See Exhibit No. 10.4.C)

- 1. PURPOSE:** Entryway signage which informs the public of the boundaries to a specific subdivision or neighborhood. These are permanent signs.
- 2. LOCATION:** Located on one and/or both sides of the entrances to new subdivisions.
- 3. SIZE:** Height = 7' to top of wall.
Sign Face = 5' in height, 19' in width.
Total Sign Face = Maximum 44 square feet.
- 4. MATERIALS:** Materials shall be durable and may include concrete, stucco, decorative block, stone, and brick. Sign letters may be cast, recessed, tile, marble or brass. A metal logo may be mounted on the base. All materials subject to Community Development Director Approval.
- 5. ILLUMINATION:** The entryway signs shall be externally illuminated with floods, which are water and tamper proof. Such lighting shall be removed 60 days after completion of 90 percent of the subdivision.

9.5.7 SUBDIVISION DIRECTIONAL SIGNS

A. Primary Directional Signs (Exhibit No. 9.5A)

1. **PURPOSE:** To direct the public to specific phases and/or subdivisions within the planned community. These are temporary signs. The City of Corona may also utilize these signs to provide public service information.
2. **LOCATION:** At intersections shown on Exhibit 9.SB.
3. **SIZE:**
Height = 16'
Width = 6'
Sign Face = 6'x14'
Sign Panels = 10" in height (18" for Mountain Gate name on top)
Total Number of Panels = 12
Total Sign Face = Maximum 90 square feet
4. **MATERIALS:** Materials for the sign base include concrete, stucco, decorative block or stucco stone. Materials for the sign panels include Medium Density Overlay, Sintra, or Plexiglas. A metal logo may also be mounted on the base. The design shall be approved by the Council Kiosk Sign Committee.
5. **ILLUMINATION:** None.
6. **NUMBER OF SIGNS:** As necessary.
7. **LONGEVITY:** Until 60 days following completion of the subject phase(s) and/or subdivision(s).

B. Secondary Directional Signs (Exhibit No. 9.5C)

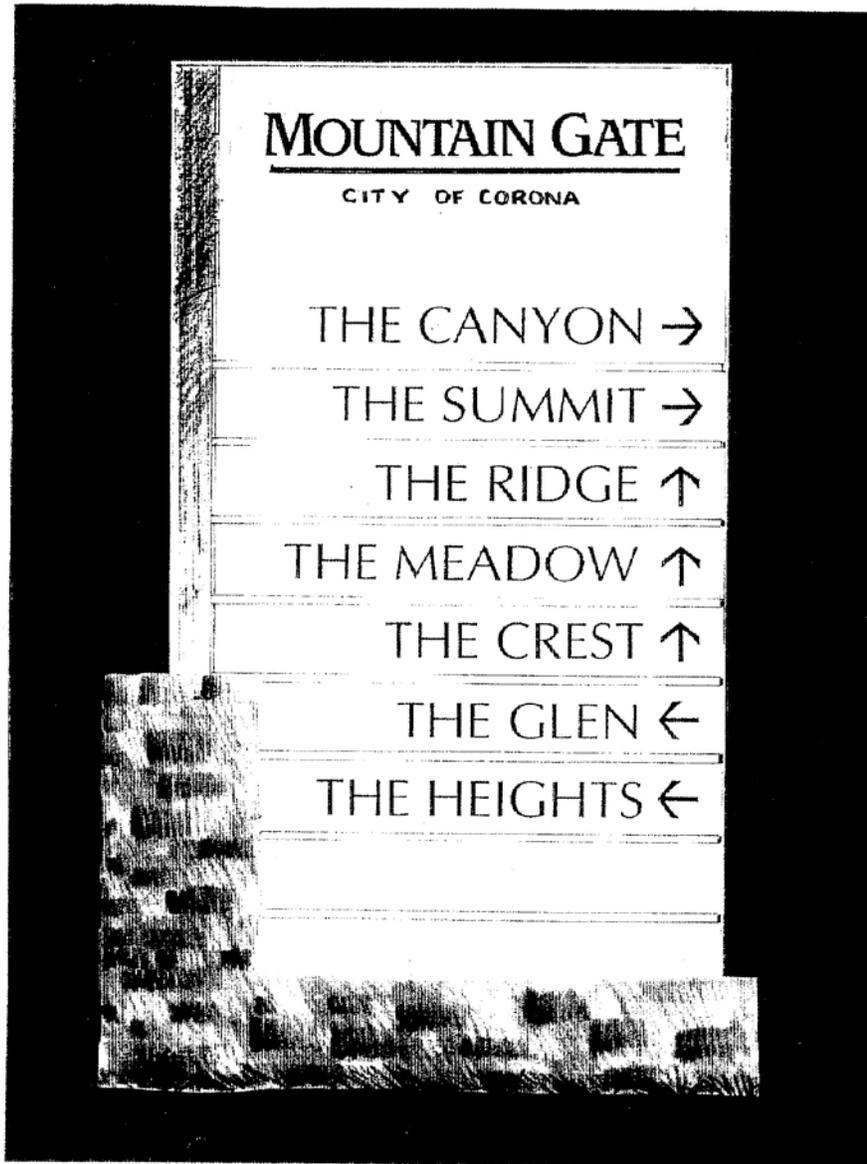
1. **PURPOSE:** To direct the public to specific phases and/or subdivisions within the planned community. These are temporary signs. The City of Corona may also utilize these to provide public service information.
2. **LOCATION:** At intersections throughout the planned community on Mountain Gate Drive, Main Street, Highgrove Street, Garretson and Fullerton Avenues, Cleveland Way, Orange Heights Lane, Upper Drive and Chase Drive.
3. **SIZE:**
Height = 9' (excluding sign base)
Width = 5'
Sign Face = 5' in height x 10' in width

Sign Panel = 8” in height (18” for Mountain Gate name on top).
Total Number of Panels = 10
Total Sign Face = Maximum 45 square feet

- 4. MATERIALS:** Materials for the sign base include concrete, stucco, stone, decorative block or stucco stone. Materials for the sign panels include Medium Density Overlay, Sintra or Plexiglas. A metal logo may also be mounted on the base.
- 5. ILLUMINATION:** None.
- 6. NUMBER OF SIGNS:** As necessary.
- 7. LONGEVITY:** Until 60 days following completion of subject phase(s) and/or subdivision(s).

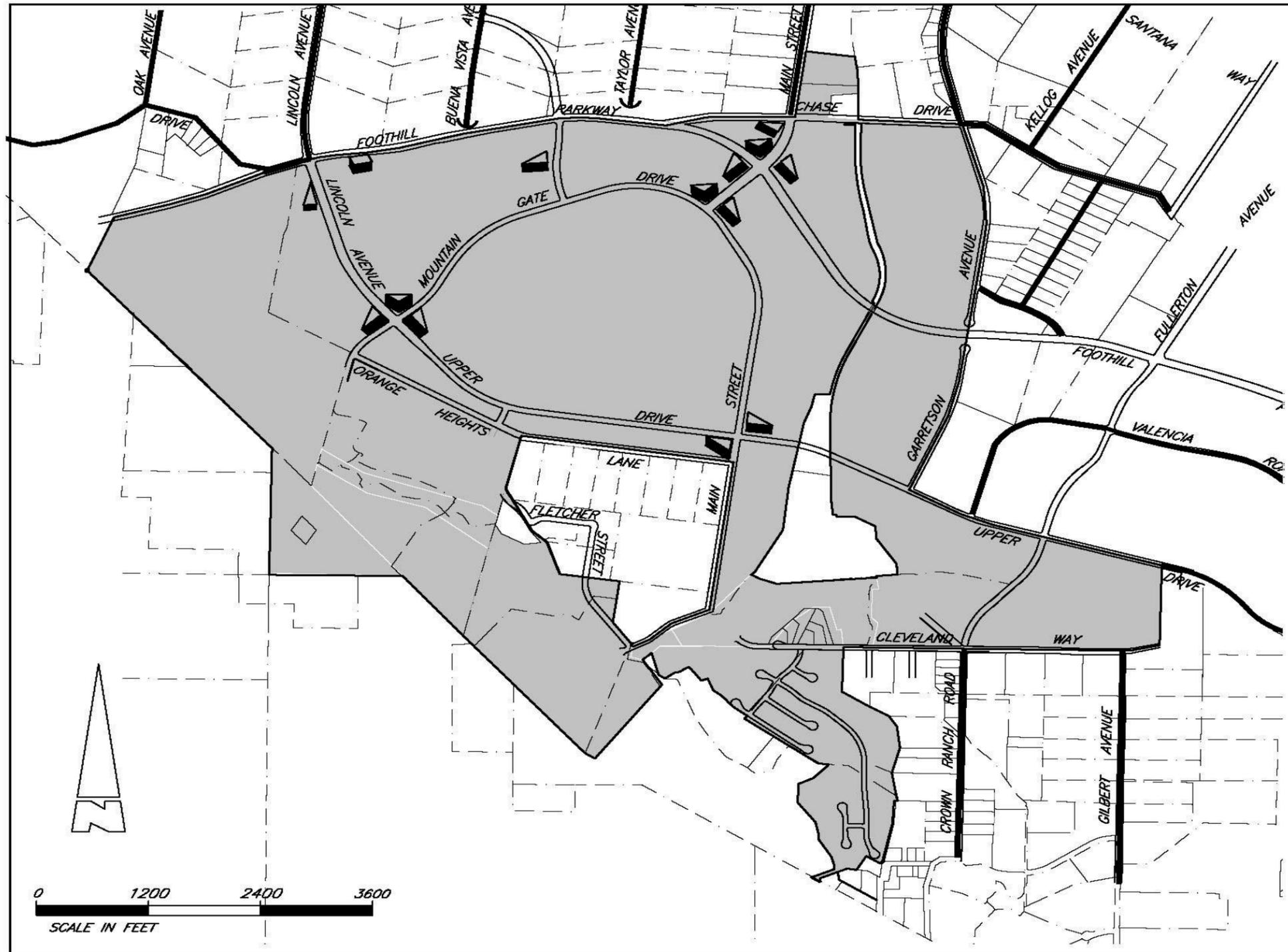
D. Removable Directional Signs

- 1. PURPOSE:** To provide directions to subdivisions where sales are taking place.
- 2. LOCATION:** Along roads leading to subdivision.
- 3. SIZE:** 2’ in height, 3’ in width.
- 4. MATERIALS:** Pressure-treated wood, Sintra, Plexiglas or Medium Density Overlay.
- 5. ILLUMINATION:** None.
- 6. LONGEVITY:** Temporary, portable signs to be removed at end of particular promotion or sales period for subject subdivision.



PRIMARY DIRECTIONAL
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5A



LEGEND

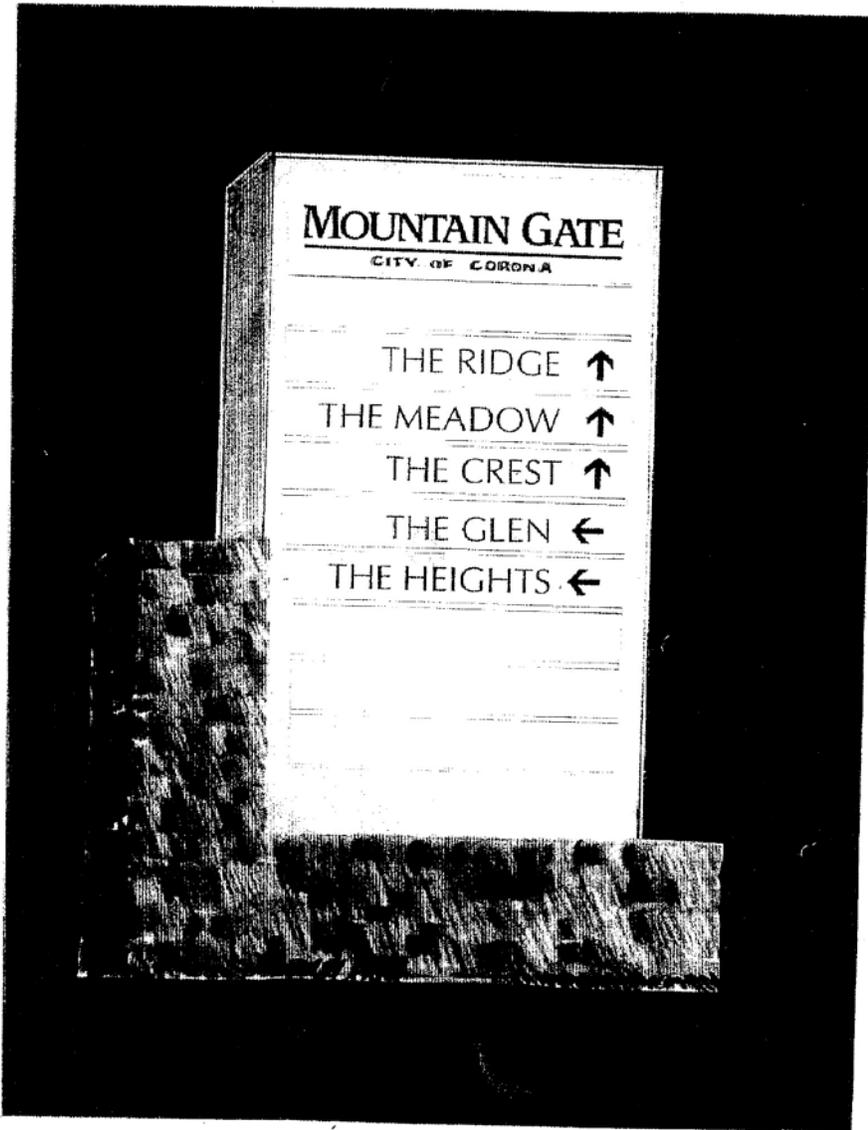


NO ACCESS TO THROUGH STREET



PRIMARY DIRECTIONAL SIGNAGE

**MOUNTAIN GATE
DIRECTIONAL
SIGNAGE
LOCATIONS
EXHIBIT: 9.5B**



SECONDARY DIRECTIONAL
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5C

9.5.8 IDENTIFICATION SIGNS

A. Community Identification Signs (Exhibit No. 9.5D)

1. **PURPOSE:** To identify the Mountain Gate Community at key entry points.
2. **LOCATIONS:** One at the intersection of Foothill Parkway and Lincoln Avenue and one at the intersection of Chase Drive and Garretson Avenue.
3. **SIZE:** Height = 16'
Sign Face = 8' in height, 16' in width
Sign Ground Clearance = 8' in height
Total Sign Face = Maximum 130 square feet
4. **MATERIALS:** Materials include pressure-treated wood, Sintra or Medium Density Overlay.
5. **ILLUMINATION:** Optional: Floodlights to be removed when signs are removed.
6. **NUMBER OF SIGNS:** Two double-faced signs.
7. **LONGEVITY:** Until 60 days after completion of 90 percent sales of lots within the overall development.

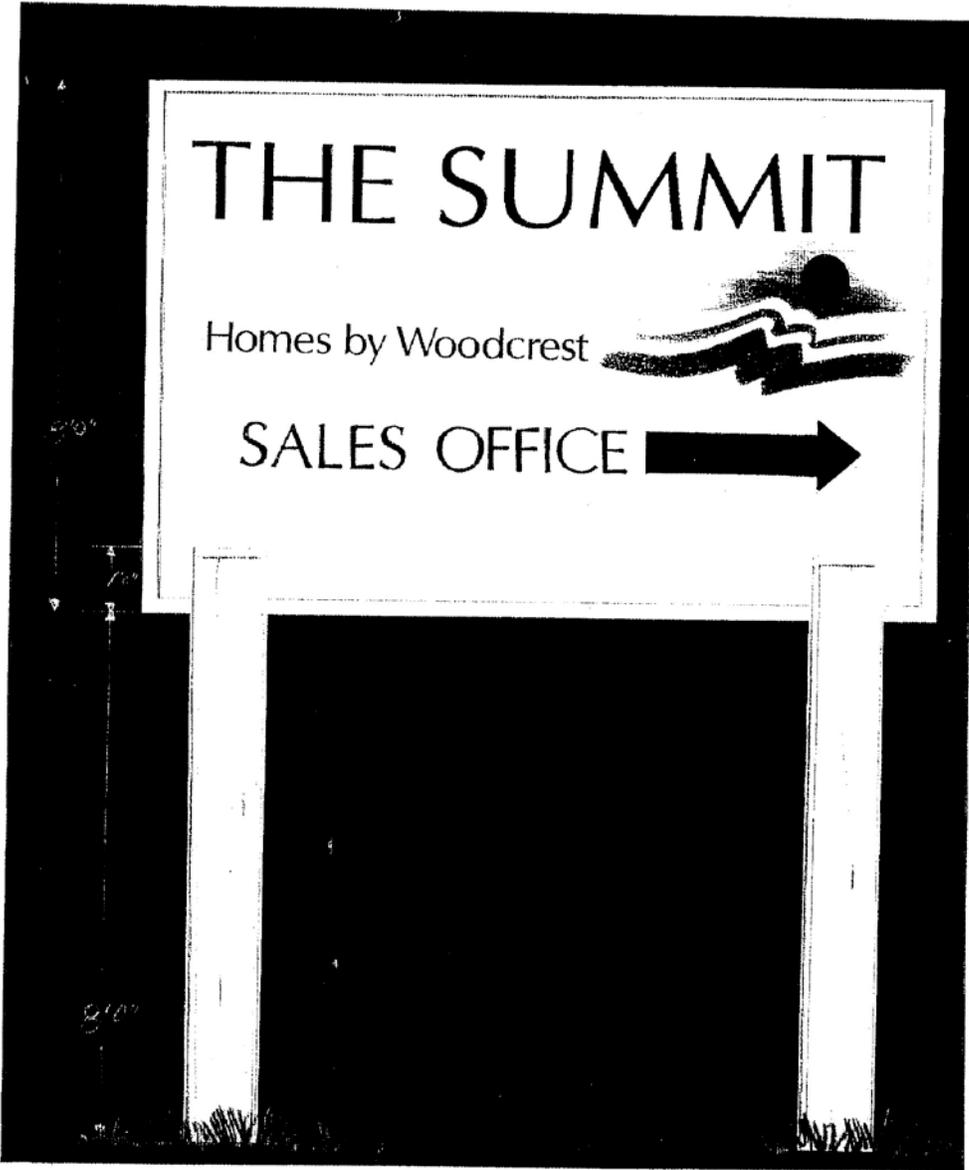
B. Subdivision Identification Signs (Exhibit No. 9.5E)

1. **PURPOSE:** To identify individual projects from arterials and major collectors.
2. **LOCATIONS:** One may be permitted for each new subdivision or phase.
3. **SIZE:** Height = 16'
Sign Face = 8' in height, 12' in width double faced.
Sign Ground Clearance = 8' in height
Total Double Faced Sign Face = Maximum 100 square feet.
4. **MATERIALS:** Materials include pressure-treated wood, Sintra, or Medium Density Overlay.
5. **ILLUMINATION:** Optional: Floodlights to be removed when signs are removed.
6. **NUMBER OF SIGNS:** One double-faced sign per subdivision.
7. **LONGEVITY:** Until 60 days after completion of sales period for subject subdivision or phase.



COMMUNITY
IDENTIFICATION SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

10/16
EXHIBIT 9.5D

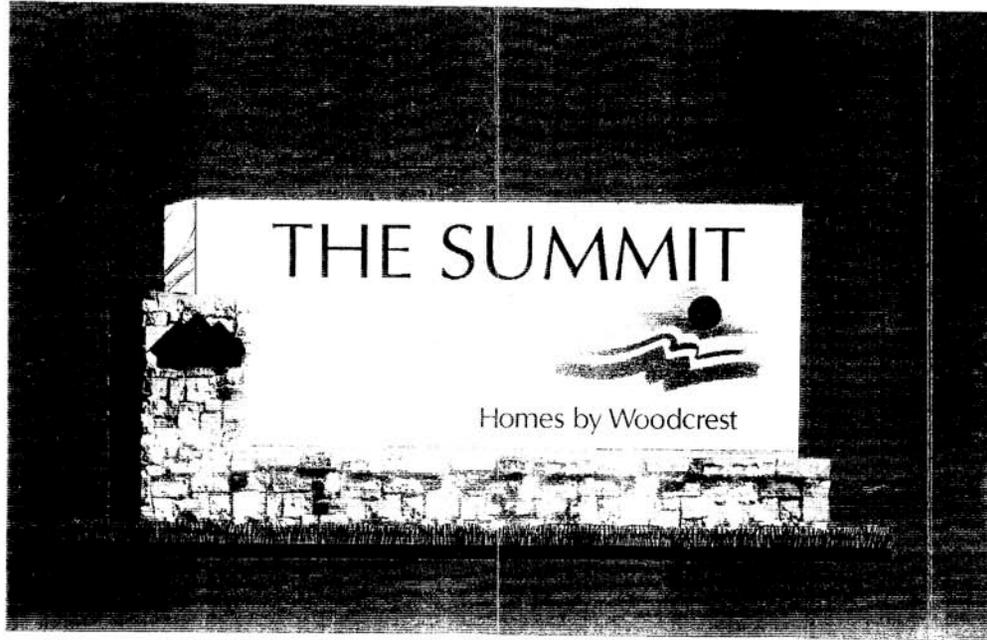


SUBDIVISION
IDENTIFICATION SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5E

C. Project Identification Signs (Exhibit No. 9.5F)

- 1. PURPOSE:** To identify a specific subdivision or phase of development from within that development.
- 2. LOCATIONS:** One may be permitted for each new subdivision or phase and shall be located at the primary entrance to the subdivision.
- 3. SIZE:** Height = 12'
Sign Face = 4' in height, 10' in width.
Total Double Faced Sign Face = Maximum 44 square feet.
- 4. MATERIALS:** Materials include stucco, stone, decorative block or stucco stone and Sintra, Medium Density Overlay or Plexiglas for the face. A metal logo may also be mounted on the base.
- 5. ILLUMINATION:** Optional: Floodlights to be removed when signs are removed.
- 6. NUMBER OF SIGNS:** One double faced sign per entrance.
- 7. LONGEVITY:** Until 60 days after completion of sales period for the subject subdivision or phase.



PROJECT
IDENTIFICATION SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

1317

EXHIBIT 9.5F

9.5.9 FLAGS

A. Community Flags

1. **PURPOSE:** To be used along the primary entrances to the planned community to identify a new development. An entryway statement which informs viewers of new models opening within a subdivision.
2. **LOCATIONS:** Along Foothill Parkway, Main and Chase Streets.
3. **SIZE:** Maximum 24 square feet.
4. **MATERIALS:** Nylon or Dacron.
5. **ILLUMINATION:** None.
6. **LONGEVITY:** Until 60 days after completion of sales period of the development.

B. Subdivision Flags

1. **PURPOSE:** To be used to identify a new subdivision or phase and/or along the entrances to a new community. An entryway statement which informs viewers of new models opening within a subdivision.
2. **LOCATIONS:** Along main streets leading to subdivision or phase being marketed.
3. **SIZE:** Maximum 12 square feet mounted, 21 feet high.
4. **MATERIALS:** Nylon or Dacron.
5. **ILLUMINATION:** None.
6. **LONGEVITY:** Until 60 days after completion of sales period of subdivision being marketed.
7. **NUMBER:** 30 poles maximum per subdivision.

9.5.10 SALES INFORMATION CENTER SIGNS

A. Sales Center Identification Sign (Exhibit No. 9.5G)

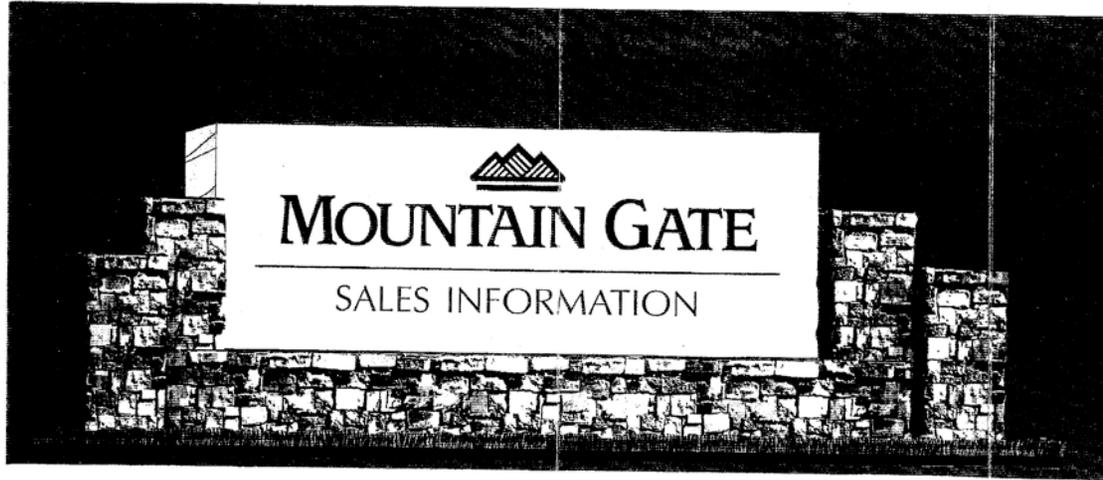
- 1. PURPOSE:** A monument sign to direct attention to the location of a sales office for a specific subdivision or phase.
- 2. LOCATIONS:** One sign located at each model complex
- 3. SIZE:** Height = 7' to top of sign
Sign Face = 4' in height, 12' in width
Total Sign Face = Maximum 60 square feet
- 4. MATERIALS:** Materials include concrete, stucco, decorative block or stucco stone. The sign face materials include Sintra, pressured-treated wood, Medium Density Overlay or Plexiglas. A metal logo may also be mounted on the base.
- 5. ILLUMINATION:** The sign shall be externally illuminated, with horizontal flood lights which are water and tamper-proof.
- 6. LONGEVITY:** Until 60 days after sales period is completed for subject subdivision or phase.

B. Parking Lot Signs (Exhibit No. 9.5H)

- 1. PURPOSE:** Located at the model complexes to direct the public to the models.
- 2. LOCATIONS:** Two signs located at each model complex
- 3. SIZE:** Height = 5' to top of sign
Sign Face = 2' in height, 3' in width
Total Sign Area = Maximum 8 square feet
- 4. MATERIALS:** Materials include pressure-treated wood, Sintra or Medium Density Overlay.
- 5. ILLUMINATION:** None.
- 6. NUMBER OF SIGNS:** Two for each model complex
- 7. LONGEVITY:** Until 60 days after sales period is completed for subject subdivision or phase.

C. Information Signs (Exhibit No. 9.5I)

- 1. PURPOSE:** Located at the model complexes to provide information about the subdivision and/or models.
- 2. LOCATIONS:** Two signs located at each model complex.
- 3. SIZE:** Height = 5' to top of sign.
Sign Face = 2' in height, 3' in width.
Total Sign Area = Maximum 8 square feet.
- 4. MATERIALS:** Materials include pressure-treated wood, Sintra or Medium Density Overlay.
- 5. ILLUMINATION:** None.
- 6. NUMBER OF SIGNS:** Two for each model complex.
- 7. LONGEVITY:** Until 60 days after sales period is completed for subject subdivision or phase.



SALES CENTER
IDENTIFICATION SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

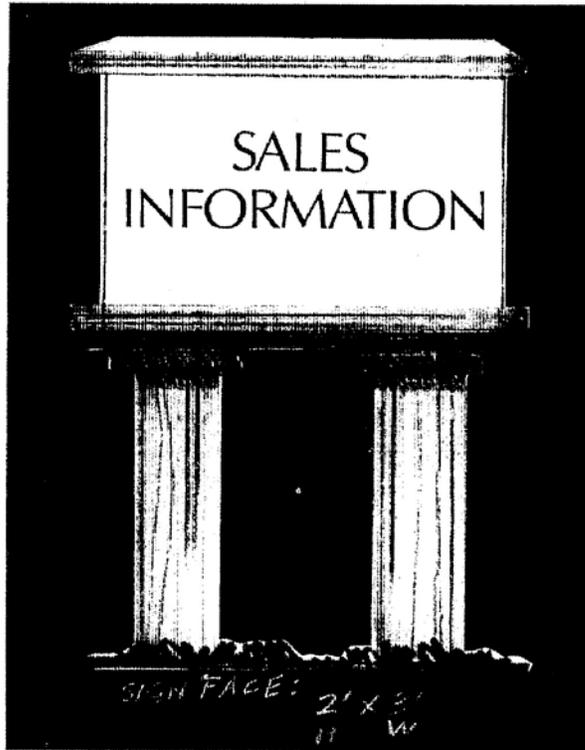
7020

EXHIBIT 9.5G



PARKING LOT SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5H

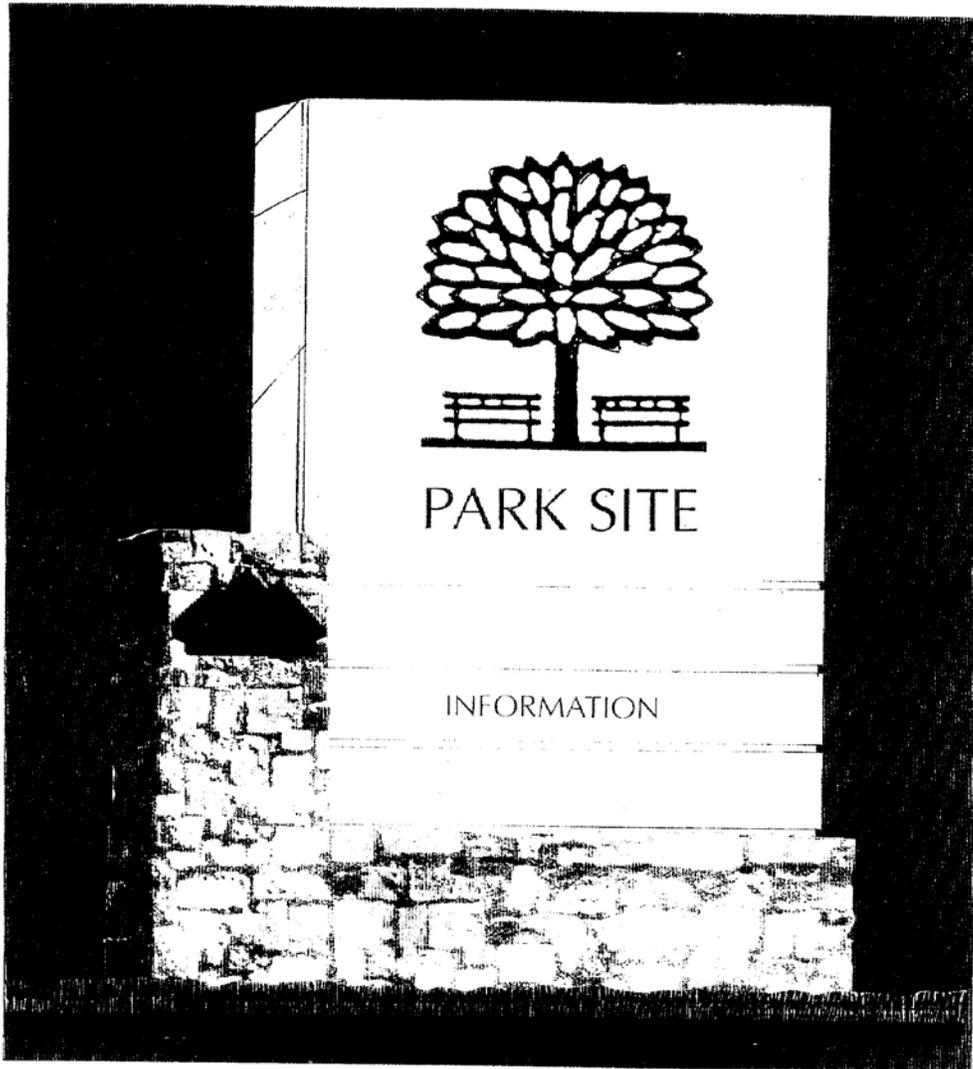


INFORMATION SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5I

9.5.11 FUTURE FACILITY SIGNS (Exhibit No. 9.5J)

- 1. PURPOSE:** A monument sign to identify the location of future facilities such as a school, park site, shopping center and the like.
- 2. LOCATIONS:** One sign located on each street frontage of the subject site within private property.
- 3. SIZE:** Height = 10' to top of sign.
Sign Face = 5' in height, 7' in width.
Total Sign Area = Maximum 40 square feet.
- 4. MATERIALS:** Materials include concrete, stucco, decorative block or stucco stone. Sign face may be Sintra, Plexiglas or pressure-treated wood.
- 5. ILLUMINATION:** None.
- 6. LONGEVITY:** Until 60 days after project is completed.



FUTURE FACILITY SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5J

9.5.12 APARTMENT SIGNS (Exhibit No. 9.5K)

- 1. PURPOSE:** To identify the name of an apartment complex and provide leasing information.
- 2. LOCATIONS:** One sign within each project, at the main entrance complex.
- 3. SIZE:** Height = 12' to top of sign
Sign Face = 5' in height, 10' in width
Total Sign Area = Maximum 60 square feet
- 4. MATERIALS:** Materials include concrete, stucco, decorative block or stucco stone. Sign face materials include Sintra, Medium Density Overlay, pressure-treated wood or Plexiglas.
- 5. ILLUMINATION:** The apartment identification sign shall be externally illuminated with horizontal flood lights which are water and tamper proof.



APARTMENT SIGN
MOUNTAIN GATE
LYON COMMUNITIES INC.

EXHIBIT 9.5K

9.5.13 CITY KIOSK SIGNS

- 1. PURPOSE:** These signs are to allow specific locations for the standard City-wide kiosk signs, which provide directions to other the development projects surrounding Mountain Gate from within Mountain Gate.
- 2. STANDARDS:** Same as City requirements found in Section 17.74.165 of the Corona Municipal Code.
- 3. LOCATIONS:**
- A. Southwest corner of Foothill Drive and Lincoln Avenue.
 - B. Southeast corner of Foothill Parkway and Main Street.
 - C. Southeast corner of Main Street and Upper Drive.
 - D. Off-site at project perimeter as permitted by the City program.

10.0 COMMUNITY DESIGN CRITERIA

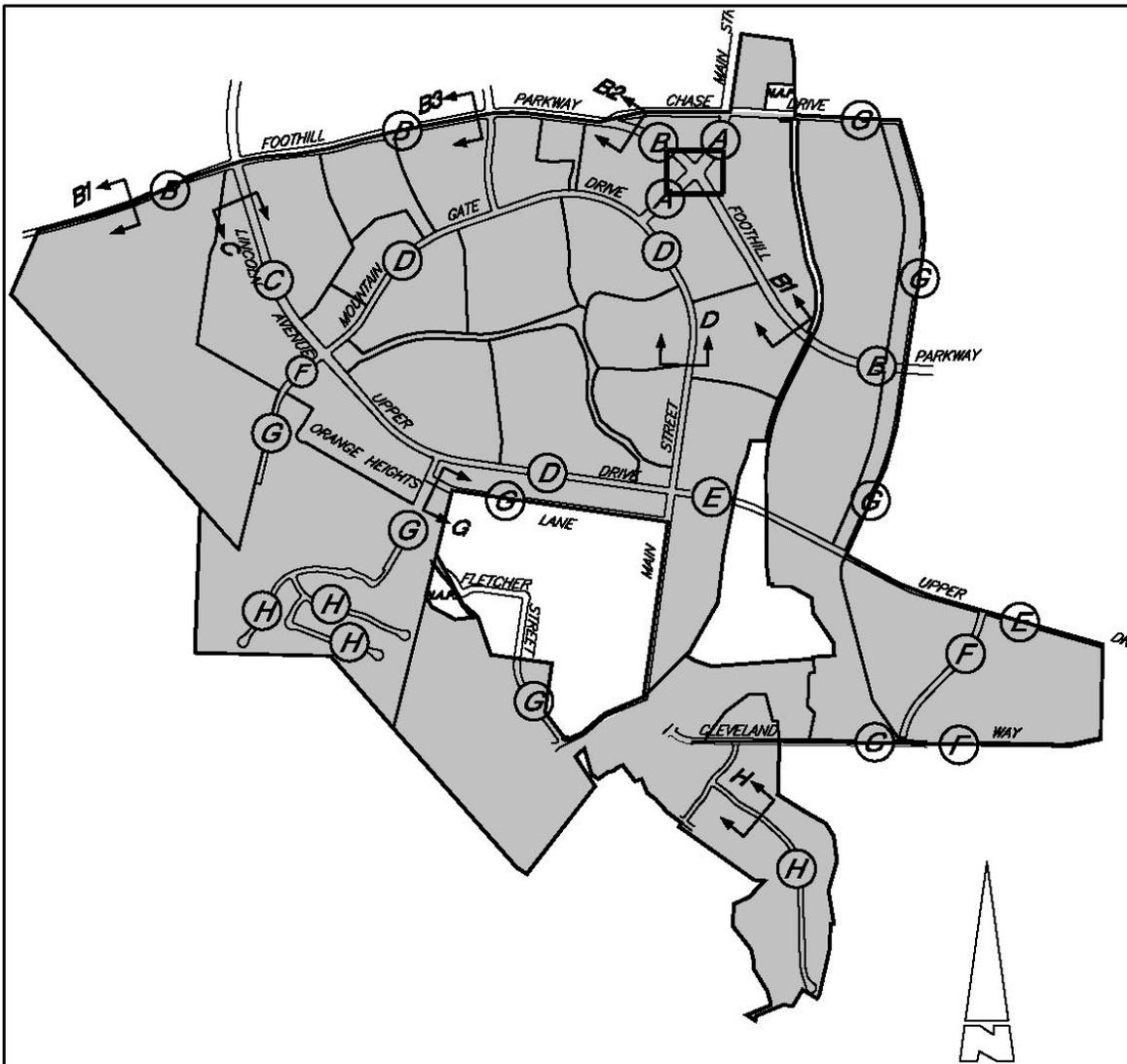
10.1 Community Design Elements

The Community Design section includes guidelines and standards to establish a strong design theme as a unifying element for the Mountain Gate planned community. The following community design elements are established in this section:

1. **Streetscapes.** Standards for landscaping of the arterial and collector streets, including plant materials, sidewalks, and additional landscape lots adjacent to the right-of-way to create an appealing boulevard character for the major streets.
2. **Project Entries and Theme Wall.** Standards for the design of project and subdivision entries and a theme wall to be constructed at selected locations along the arterial and collector streets.
3. **Trails.** Locations for off-street bicycle trails consistent with the Community Facilities Plan.
4. **Park and Greenbelt Improvement Program.** Specific standards and a facilities improvement program for the community park, the neighborhood park, and the greenbelt park.
5. **Special Landscape Standards.** Specific standards and guidelines addressing special landscaping situations.
6. **Architectural Guidelines.** Standards and guidelines for architectural design, to ensure the quality and diversity of architectural design in the residential neighborhoods.

10.2 Streetscape Design

The streetscape designs proposed for Mountain Gate are illustrated in plan and section on exhibits 10.2A through 10.2.G. Exhibit 10.1 is a streetscape design key exhibit which shows the locations of the streetscape plans and sections. Table 10.1 is a summary of the information included within the exhibits. Details of the streetscape designs may be modified subject to the approval of the Planning Director.

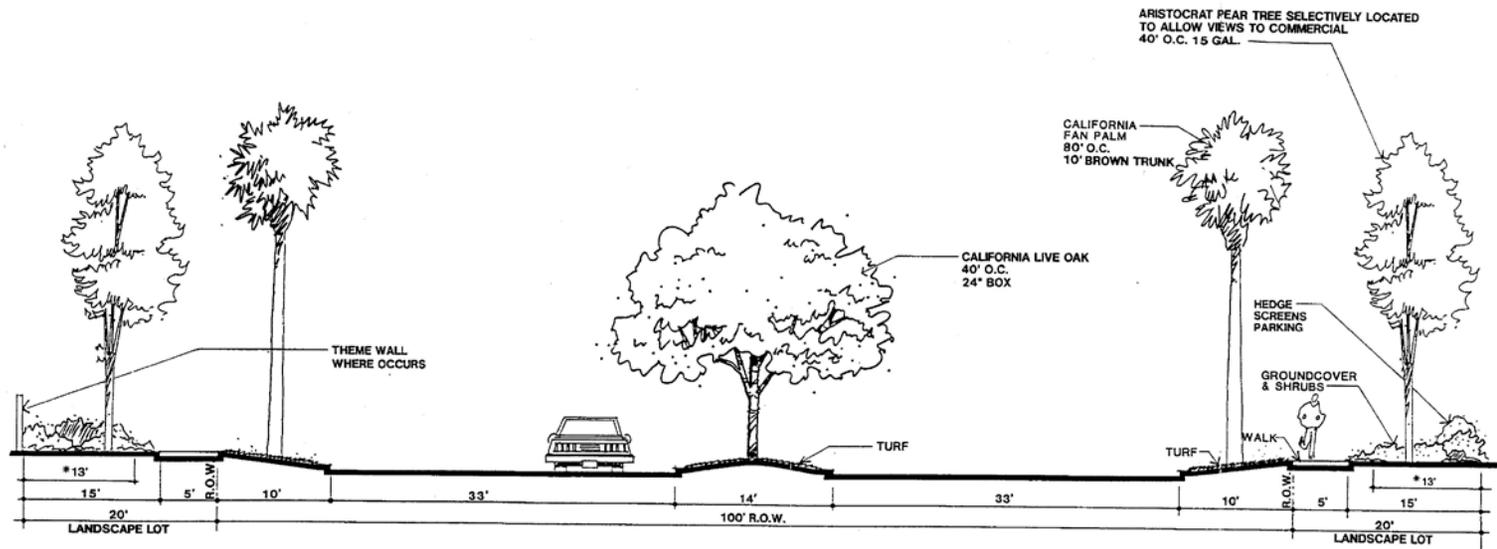


LEGEND

-  SECTION LINE
-  MAIN ENTRY
-  SECONDARY ENTRY

0 1500 3000 4500
SCALE IN FEET

**MOUNTAIN GATE
STREETSCAPE
KEY MAP
EXHIBIT: 10.1**

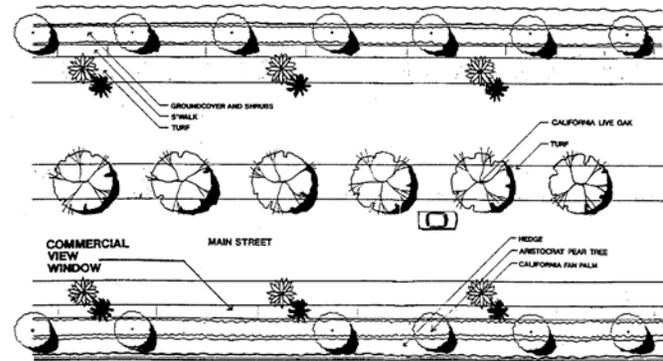


SECTION

*MINIMUM SLOPE SHALL BE 2%;
 MAXIMUM SLOPE SHALL BE 2:1.

MAIN STREET

**STREETSCAPES
 MOUNTAIN GATE
 LYON COMMUNITIES INC.**



PLAN

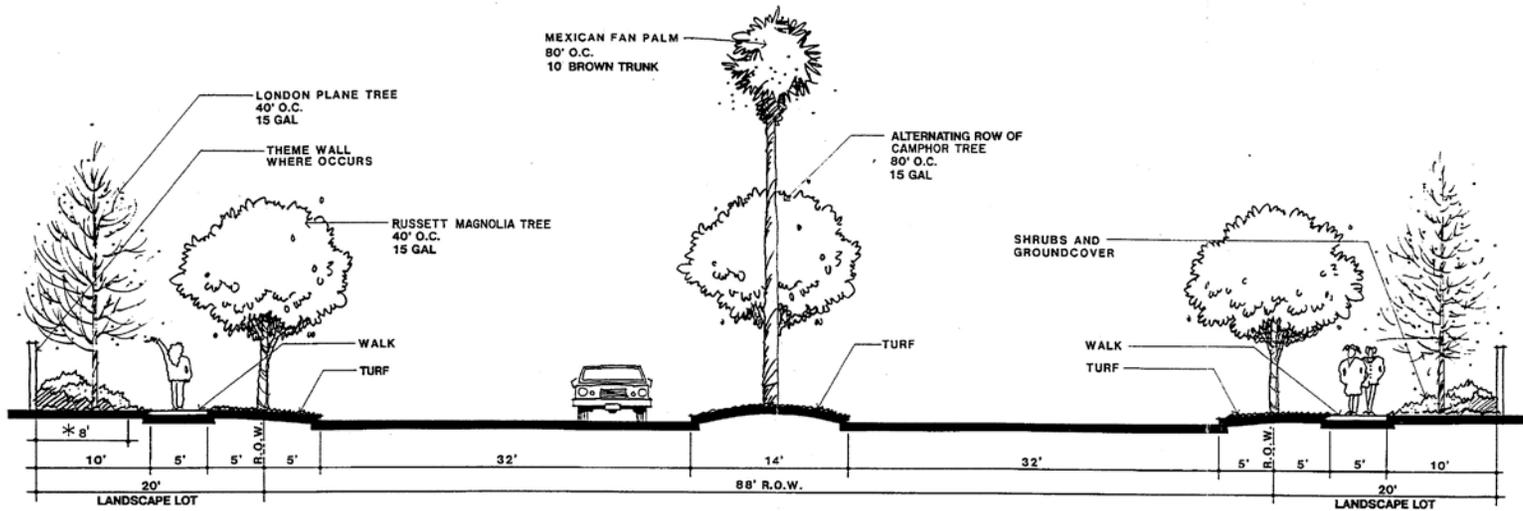
Table 10.1
Mountain Gate Streetscape Design

Street Name	Exhibit Number	Streetscape Elements
Main Street	10.2.A	100' right-of-way including a 14' raised median and a 10' parkway on each side. Parkway is enhanced with the addition of two 20' landscape lots, providing a total parkway width of 30' on each side. California Fan Palm is proposed as the street tree within a 10' wide turf area. 5' straight sidewalk on each side. Aristocrat Pear trees, evergreen Hibiscus shrubs, Nerium Oleander or Escallonia with a low Carissa g. 'boxwood beauty', Nandina, Pittosporum or Grevillea shrub form the backdrop for the streetscape. California Live Oak is proposed as the median tree. A modified section is provided along the commercial frontage to provide views of the commercial area.
Foothill Pkwy	10.2.B	88' right-of-way including a 14' raised median and a 5' parkway on each side. Parkway is enhanced with the addition of two 20' landscape lots, providing a total parkway width of 25' on each side. Russet Magnolia is proposed as the street tree within a 10' wide turf area. Standard 5' walk on each side. London Plane Tree, alternate plantings of Variegated Pittosporum, Thevetia or Variegated Euonymus shrubs with Carissa g. 'boxwood beauty' or Raphiolepis 'ballerina' and Euonymus, Ligustrum or Photima shrubs with Asparagus, Grevillea or Nandina in the foreground form the backdrop for the streetscape. Mexican Fan Palm is proposed to alternate with Camphor as the median tree. An alternative section includes a 10' wide off-street bicycle path where the road follows the Old Chase Drive alignment east of the MWD right-of-way. A modified section is proposed adjacent to the commercial frontage to provide views into the commercial area. Extending westerly from approximately 1,700 linear feet west of Lincoln Avenue, Foothill Parkway adjacent to the Specific Plan boundary will have a 102' right-of-way that includes a 14' raised median and a 12' parkway on each side. A 5' walk is located at the right-of-way, with 7' of parkway landscaping adjacent to the curb.
Lincoln Avenue	10.2.C	100' right-of-way including a striped median and a 10' parkway on each side. Parkway is enhanced with the addition of two 10' landscaped lots, providing a total parkway width of 20' on each side. Bloodgood London Plane is proposed as the street tree with a 10' wide turf area. Standard 5' walk on each side. Eucalyptus Rudis trees, Pittosporum Tobira, Euonymus or Escallonia shrubs with a low Lantana, Viburnum and/or Grevillea shrubs for the backdrop for the streetscape.

Chapter 10.0 Community Design

Street Name	Exhibit Number	Streetscape Elements
Mountain Gate Drive, Upper Drive (west of Main Street) and Main Street (between Upper Drive and Mountain Gate)	10.2D	68' right-of-way including a 12' parkway in each side. Enhancement adjacent to the parkway is provided by additional village core landscape lots on each side. On the village core side, the added landscape lot of 18' provides a total parkway width of 30'. On the village core interior side, the added landscape lot of 4' provides a total parkway width of 16'. Bloodgood London Plane is proposed as the street tree. Eucalyptus Rudis trees, Prunas Caroliniana 'bright and tight' Ligustrum or Carissa g. 'tuttle' shrubs with low Paxistima, Petite Pink Oleander and/or Raphiolepis 'clara' shrubs form the backdrop for the streetscape. Standard 5' walk on each side. (The off-street bicycle trail for the village core is provided in the greenbelt park).
Upper Drive (east of Main Street)	10.2.E	68' right-of-way including a 12' parkway on each side. Enhanced parkway is provided by additional landscape lot. Along the south side of Upper, adjacent to the Estate Residential Cluster area, the added landscape lot of 12.5' provides a total parkway width of 24.5', consistent with the Community Facilities Plan. On the north side of Upper, a 2.5' landscape lot provides a total parkway width of 14.5'. Pistache is proposed as the street tree. Eucalyptus Rudis trees, Pittosporum Tobira, Ligustrum or Carissa g. 'tuttle' shrubs with Asparagus, low Viburnum and/or Grevillea shrubs form the backdrop for the streetscape. Standard 5' walk on each side.
Fullerton Avenue, Highgrove Street, Cleveland Way (east of Fullerton) and Mountain Gate Drive (west of Lincoln to Orange Heights Lane)	10.2.F	68' right-of-way with standard 12' parkway on each side. Standard 5' walk located at the right-of-way with 7' of parkway landscaping adjacent to the curb. Thevetia, Escallonia, Carissa g. 'tuttle' shrubs with a low Paxistima, Petite pink and oleander, Raphiolepis 'clara' shrubs will form the backdrop of the streetscape. Chinese Pistache will be the street tree for Fullerton Ave. south of Cleveland Way. The street is Modesto Ash for Fullerton, north of Cleveland Way. The street tree is Bloodgood London Plane for Mountain Gate Drive. The Street tree is Silver Dollar for Cleveland Way, east of Fullerton Avenue. For Highgrove Street, between Foothill Parkway and Mountain Gate Drive, the Street tree is Bradford Pear within a 68' right-of-way with a 12' parkway on each side. A 6.5' walk is located adjacent to the curb with 5.5' of parkway landscaping adjacent to the right-of-way for Highgrove Street.
Local Street	10.2.G	64' right-of-way with standard 12' parkway on each side. Standard 4'-5' walk located at the right-of-way, with 7'-8' of parkway landscaping adjacent to the curb. Street tree to be selected from the approved plant list. The street tree is Bloodgood London Plane for Mountain Gate Drive south of Orange Heights Lane to Windy Ridge Drive.
Hillside Street	6. 1.H	54' right-of-way for double-loaded Streets, with 40' of pavement and 7' parkways on both sides. For single-loaded streets, 50' right-of-way is permitted, with 36' of pavement and 7' parkways. Sidewalks may be placed next to curb in either street Section, per the approval of the Public Works Director. Street trees are required, and may be faster growing or larger species than specified on the approved plant list if to be maintained by a homeowners association.

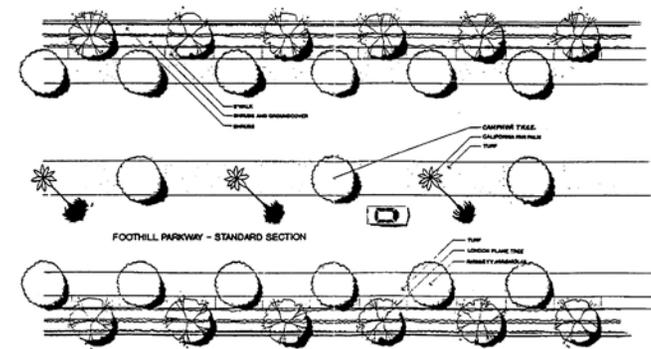
Street Name	Exhibit Number	Streetscape Elements
Hillside Street (PA 26)	6. 1.I	52' right-of-way for Street, with 36' of pavement and a 10' parkway on one side with a side walk and a 6' parkway with landscaping only on the other. Sidewalk may be placed next to curb, per the approval of the Public Works Director. Street trees are required, and may be faster growing or larger species than specified on the approved plant list if to be maintained by a homeowners association.



SECTION

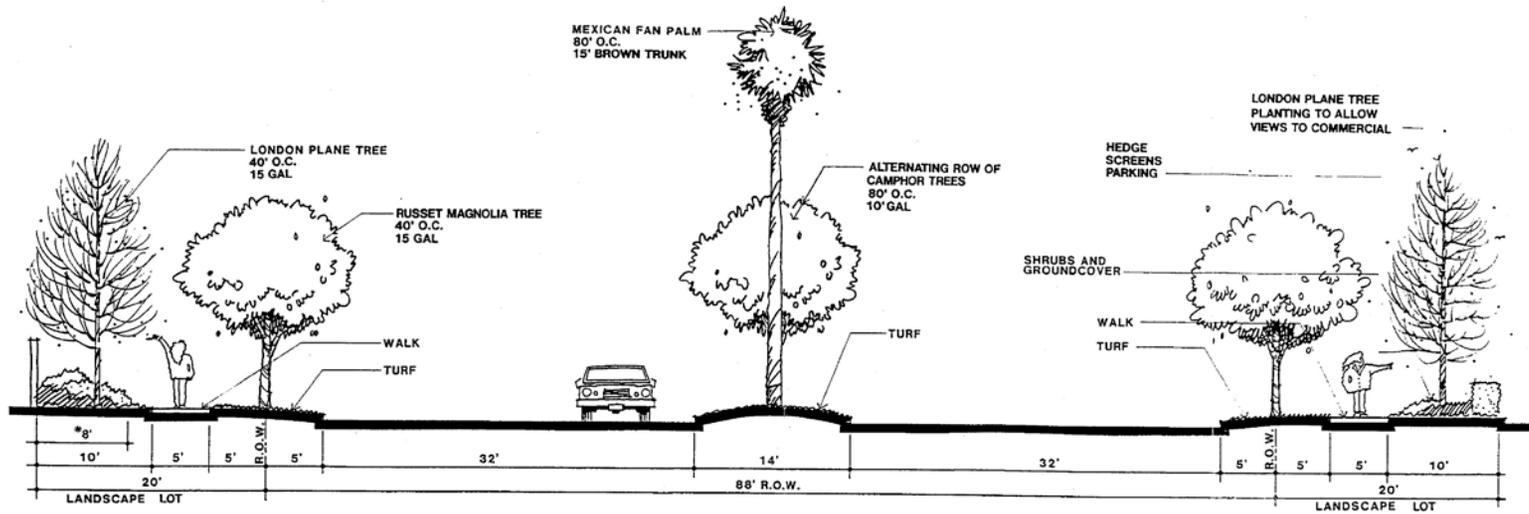
* MINIMUM SLOPE SHALL BE 2%
 MAXIMUM SLOPE SHALL BE 2:1.

FOOTHILL PARKWAY - STANDARD SECTION
STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.



PLAN

Full

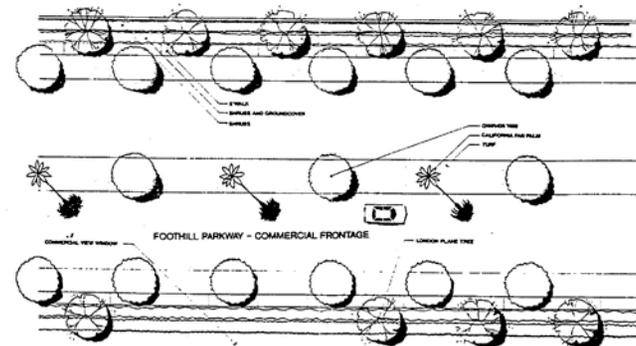


SECTION

*MINIMUM SLOPE SHALL BE 2%;
 MAXIMUM SLOPE SHALL BE 2:1.

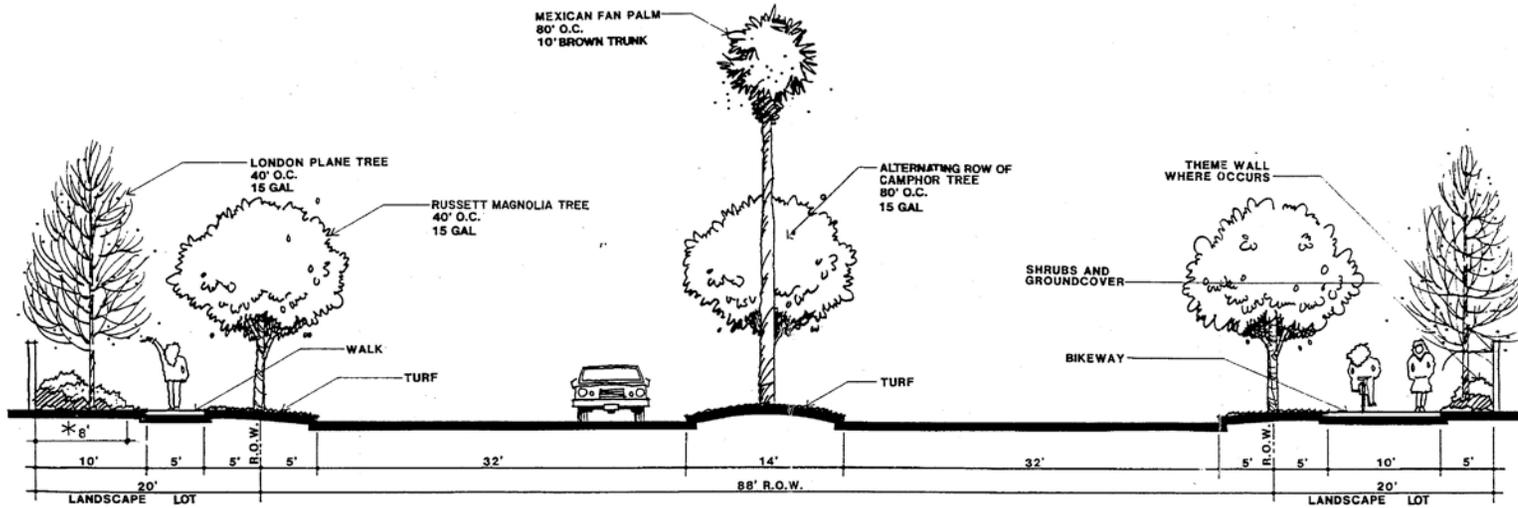
FOOTHILL PARKWAY - COMMERCIAL FRONTAGE

STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.



PLAN

*2/14/01
 10/2/01*

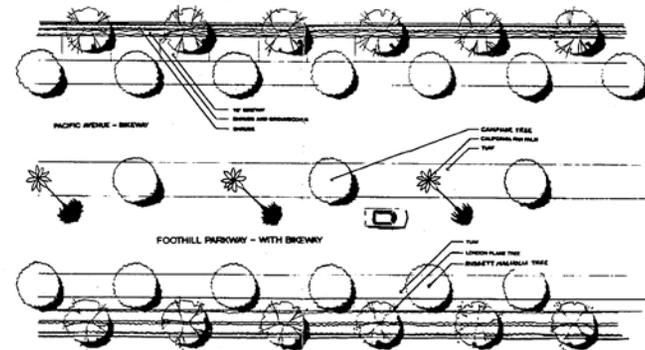


SECTION

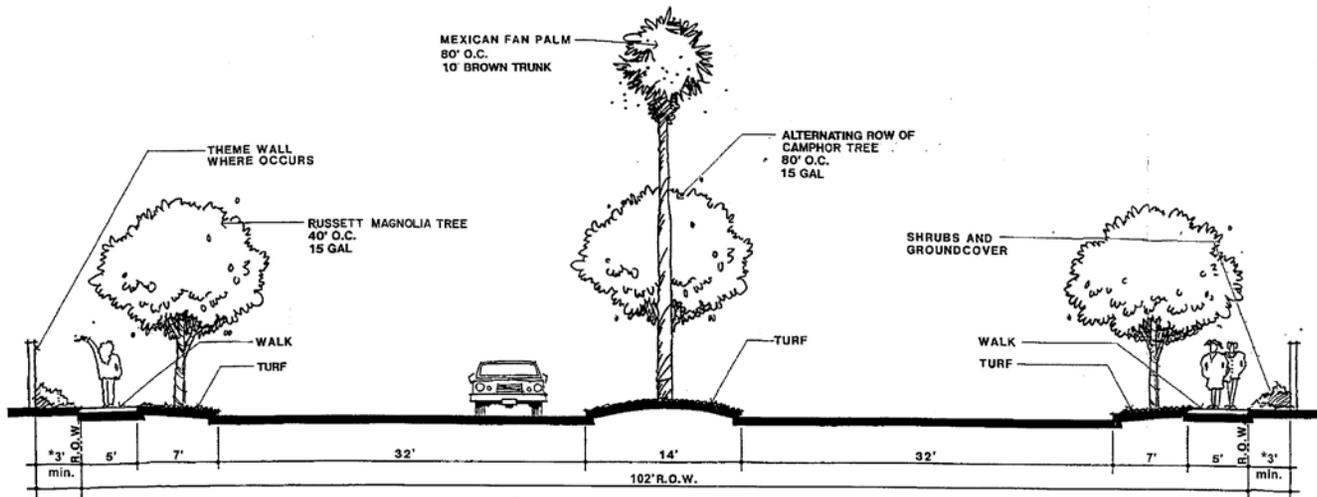
* MINIMUM SLOPE SHALL BE 2%;
 MAXIMUM SLOPE SHALL BE 2:1.

FOOTHILL PARKWAY - WITH BIKEWAY

STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

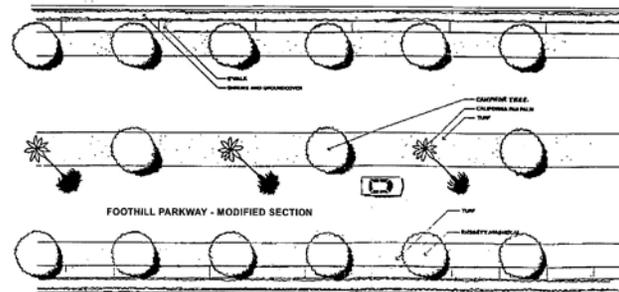


PLAN



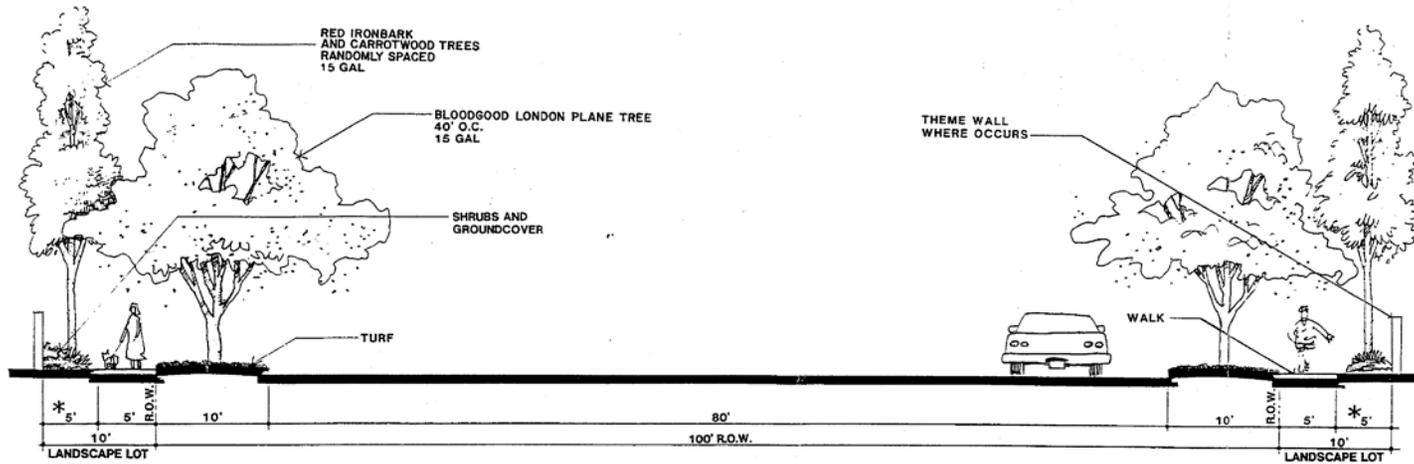
SECTION

* MINIMUM SLOPE SHALL BE 2%
 MAXIMUM SLOPE SHALL BE 2:1.
 CFD LANDSCAPE AREAS WILL VARY DEPENDING ON THE TOPOGRAPHY OF THE PROPERTY.



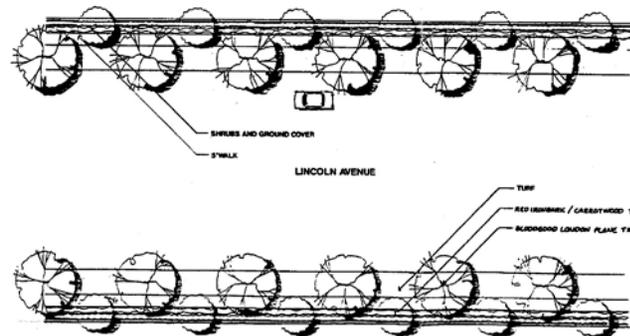
FOOTHILL PARKWAY - WEST OF LINCOLN AVENUE
STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

161253



SECTION

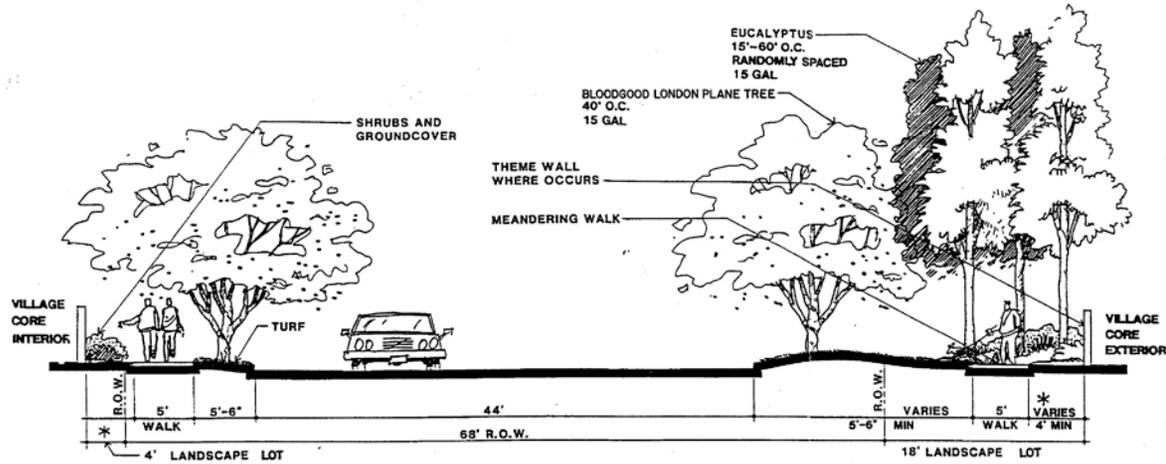
* MINIMUM SLOPE SHALL BE 2%;
 MAXIMUM SLOPE SHALL BE 2:1.



PLAN

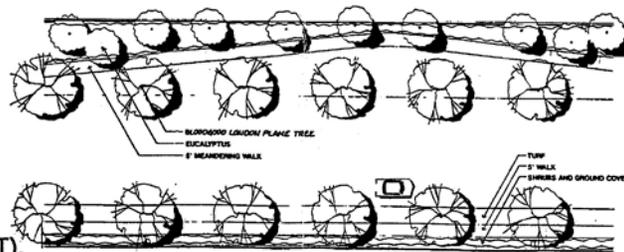
LINCOLN AVENUE
STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/25/11



SECTION

*MINIMUM SLOPE SHALL BE 2%;
 MAXIMUM SLOPE SHALL BE 2:1.

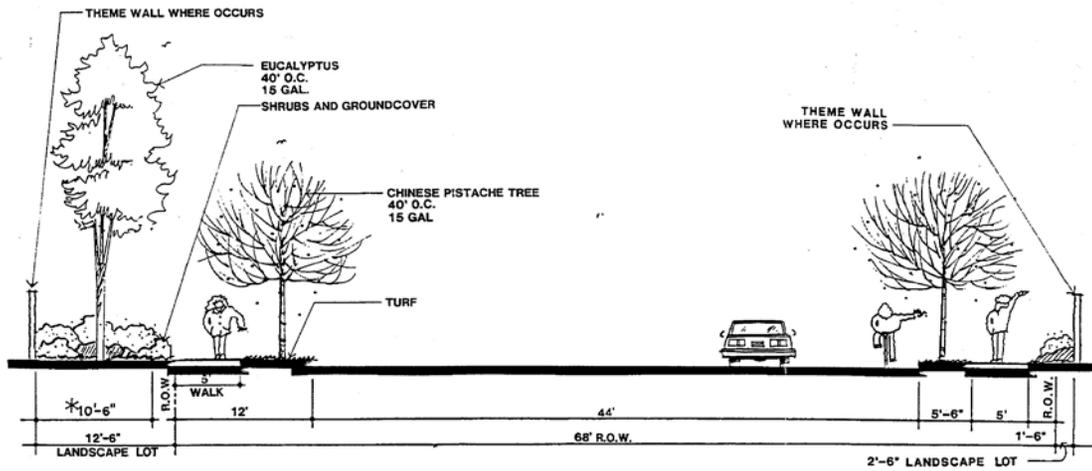


PLAN

MOUNTAIN GATE DRIVE,
 UPPER DRIVE (WEST OF MAIN STREET),
 MAIN STREET (BETWEEN MOUNTAIN
 GATE DRIVE AND UPPER DRIVE)

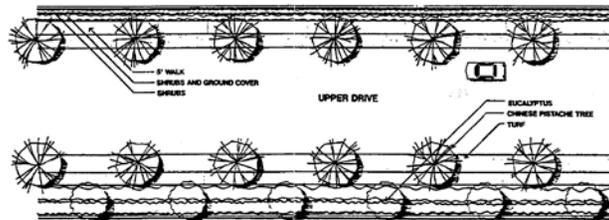
STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/12 C



SECTION

* MINIMUM SLOPE SHALL BE 2%
 MAXIMUM SLOPE SHALL BE 2:1.

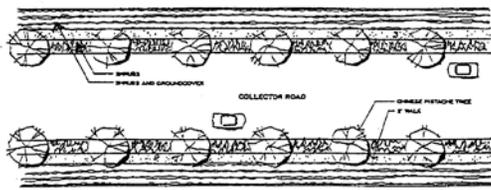


PLAN

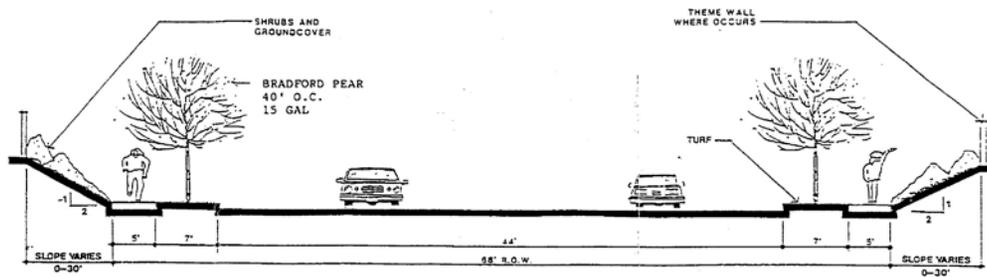
UPPER DRIVE
 (EAST OF MAIN STREET)

STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

0.2D

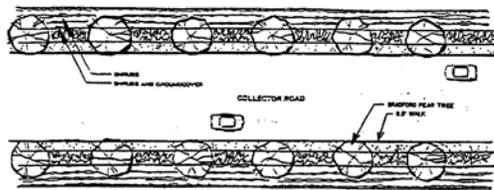


PLAN



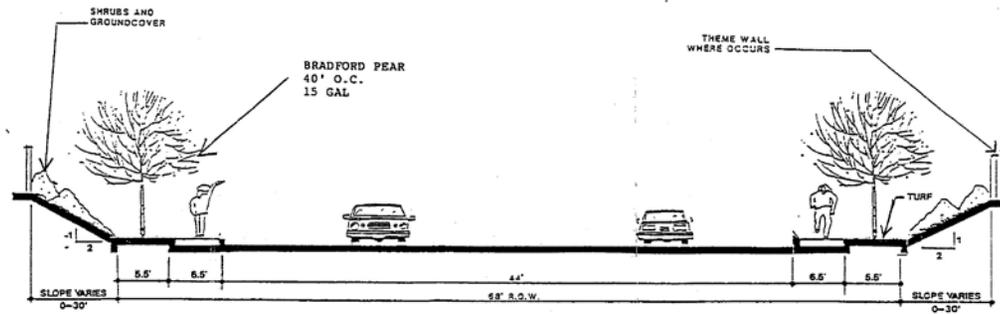
SECTION

COLLECTOR ROAD



PLAN

COLLECTOR ROAD

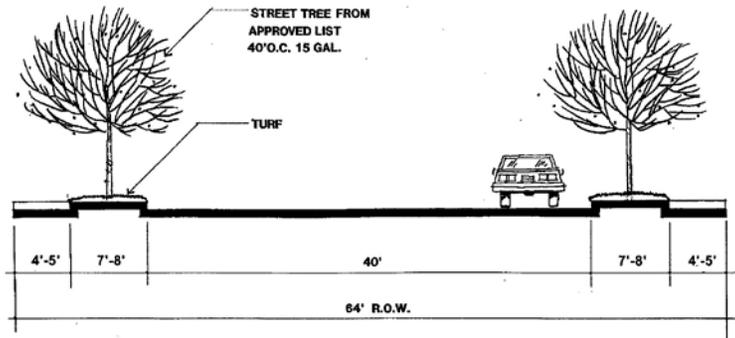


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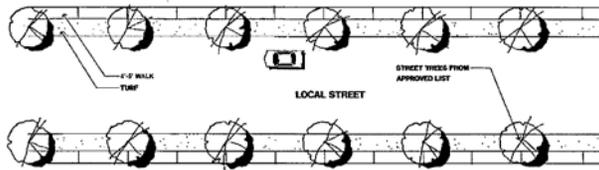
HIGHGROVE STREET

STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/25



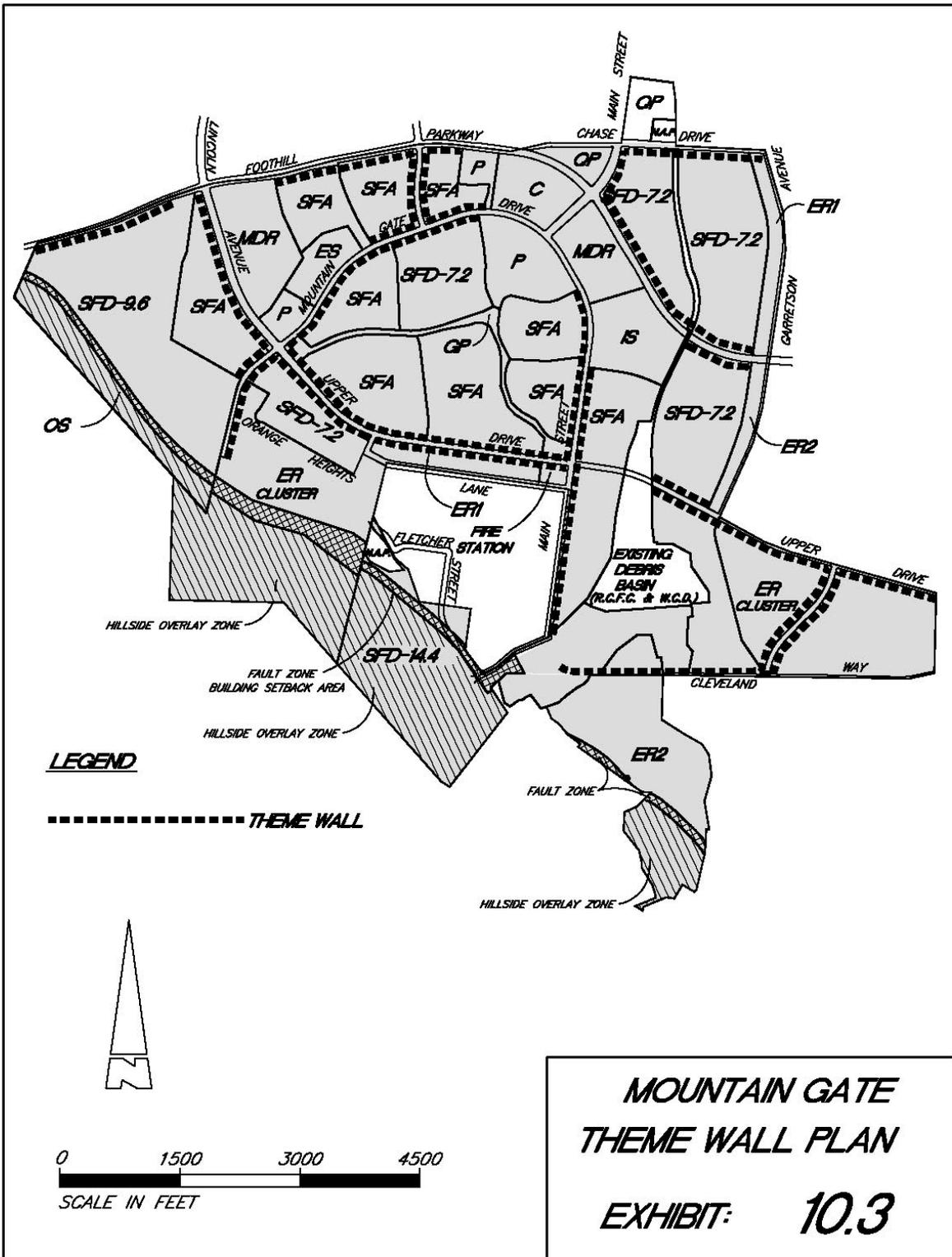
SECTION



PLAN

LOCAL STREET
STREETSCAPES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/1/07

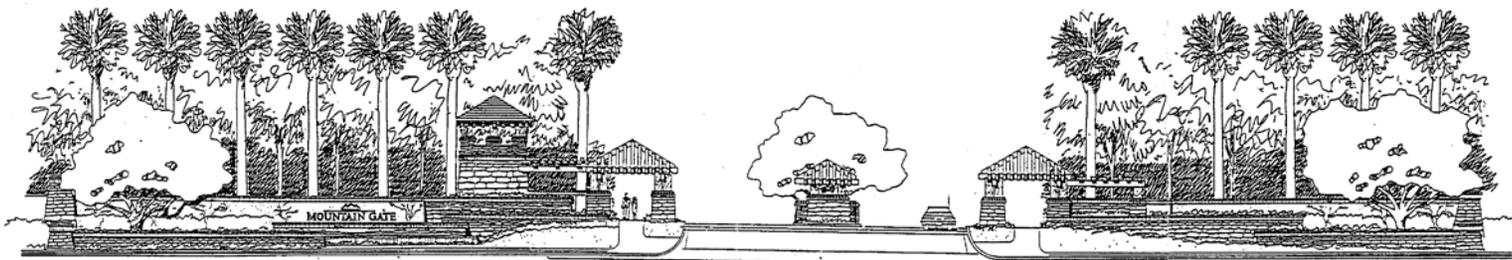


10.3 Project Entries and Theme Wall

A hierarchy of three types of project entries is proposed within the Mountain Gate Specific Plan: Main Entry, Secondary Entry and Subdivision Entry. Table 10.2 summarizes each of the three types of entries as illustrated in exhibits 10.4A through 10.4.C. Exhibit 10.3 shows the proposed locations of the hierarchy of project entries. Details of the project entries and theme wall designs, including manufacturer and color specifications shall be established during final design, and may be modified subject to the approval of the Planning Director.

Table 10.2
Community Entries

Entry Type	Exhibit Number	Character and Improvements
Main Entry	10.4A	<p>The main entry will be located at the intersection of Main Street and Foothill Parkway. This entry contains plant materials which frame and define specific areas as entry statements for the overall community. A stone and clinker brick monument sign is backed by orchard-like planting of Carolina laurel cherries or <i>Ficus Retusa</i> 'Nitida'. California Fan Palms and <i>Eucalyptus Rudis</i> row trees form a columnar backdrop that creates a large and open entry space.</p> <p>The foreground of the monument is contoured by layered planting. Directly adjacent to the monument is a larger shrub which may be selected from a palette of <i>Pittosporum</i>, <i>Variegated Pittosporum</i>, <i>Calliandra</i> and <i>Hibiscus Rosasinensis</i>.</p> <p>Complementing the shrubs is a groundcover planting selected from <i>Needlepoint Ivy</i>, <i>Verbena</i>, <i>Lantana</i> or <i>Potentilla</i>. The direct foreground is planted with annual color which is changed quarterly.</p>



ELEVATION

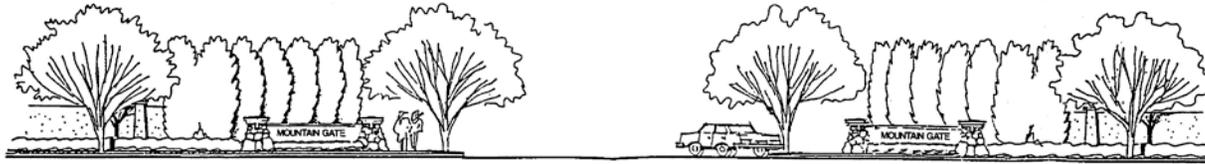
MAIN ENTRY

ENTRIES
MOUNTAIN GATE
LYON COMMUNITIES INC.

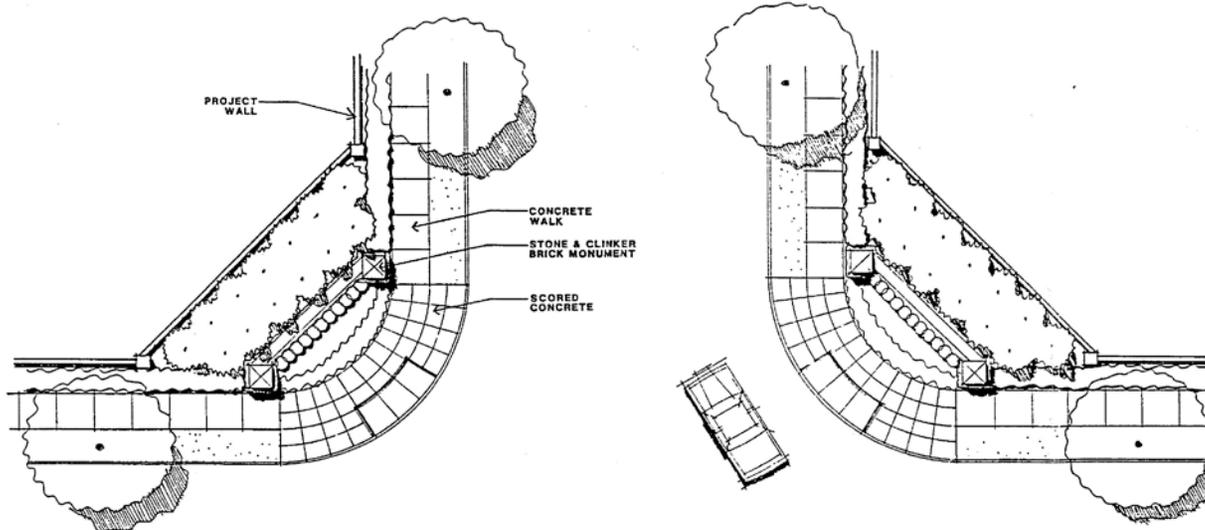
19127



EXHIBIT 10.4 A



SECTION



PROJECT WALL

CONCRETE WALK
STONE & CLINKER BRICK MONUMENT

SCORED CONCRETE

SECONDARY ENTRY

PLAN

ENTRIES
MOUNTAIN GATE
LION COMMUNITIES INC.

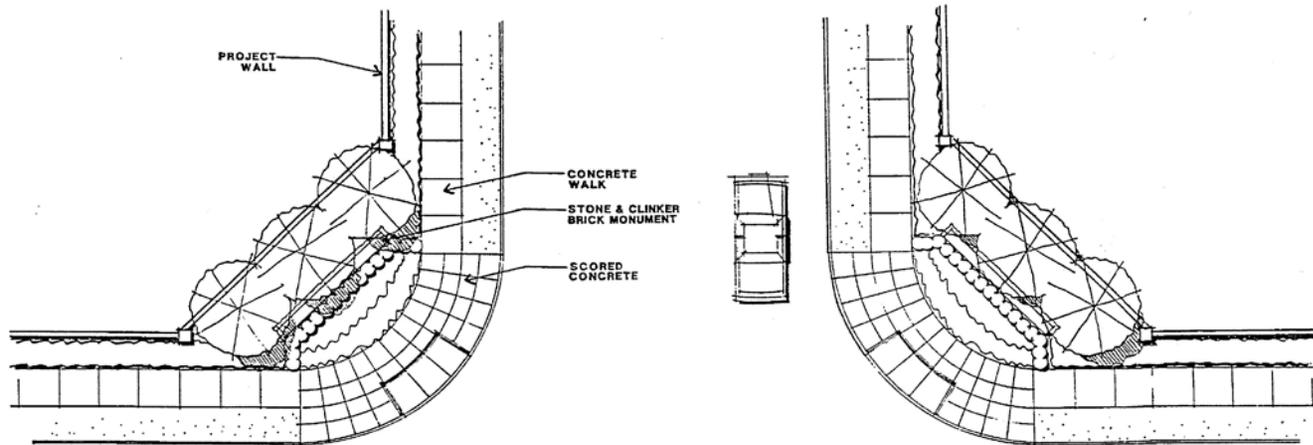
10.9/17



EXHIBIT 10.4 B



SECTION



PLAN

SUBDIVISION ENTRY
ENTRIES
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/1/13

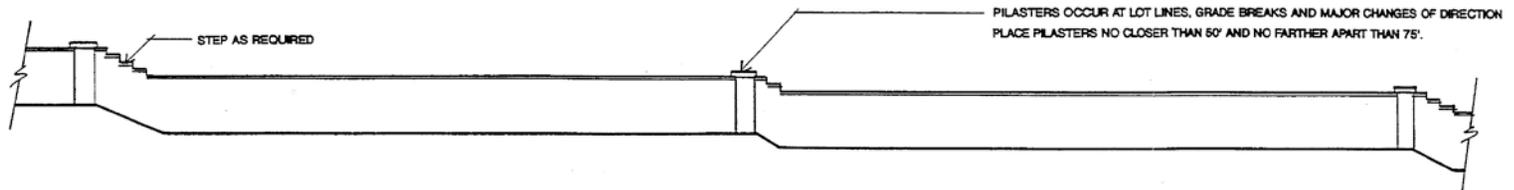
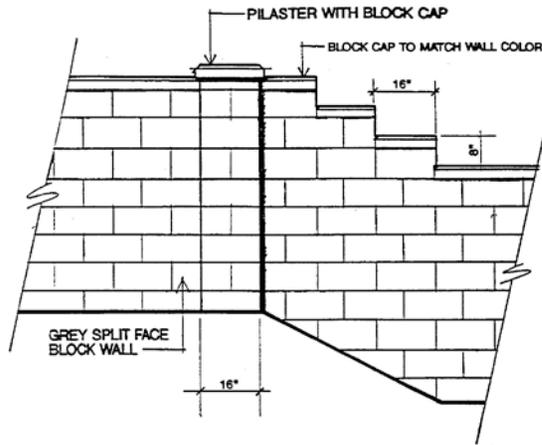
Table 10.2
Community Entries
(continued)

Entry Type	Exhibit Number	Character and Improvements
Secondary Entry	10.4B	Secondary entries also include stone and clinker brick project monumentation that is backed by dense columnar Carolina laurel cherry trees or Australian Brush cherry trees on each side of the entry street. Directly adjacent to the monument is a row of a larger shrub which may be selected from a palette of Nandina, Pittosporum, Raphiolepis or Grevillea. Complementing the shrub is a groundcover planting selected from Potentilla, Lantana, Geranium or Verbena. The direct foreground is planted with annual color which is changed quarterly.
Subdivision Entry	10.4C	Project entries are located at the entrance to individual subdivisions from arterial or collector streets. They are comprised of stone and clinker brick project monumentation backed by a row of flowering trees to fit the character of the subdivision. These trees may be selected from a palette of Albizia, Pyrus, Lagerstroemia or Jacaranda. Directly adjacent to the monument is a row of larger shrubs which may be selected from a palette of Pittosporum, Var egated Pittosporum, Raphiolepis and Viburnum. The direct foreground is planted with annual color which is changed quarterly.

A specific design for the project theme walls along the major streets has also been established. The theme wall design is shown in exhibit 10.5. The locations of the theme wall are shown in exhibit 10.3.

10.4 Trails

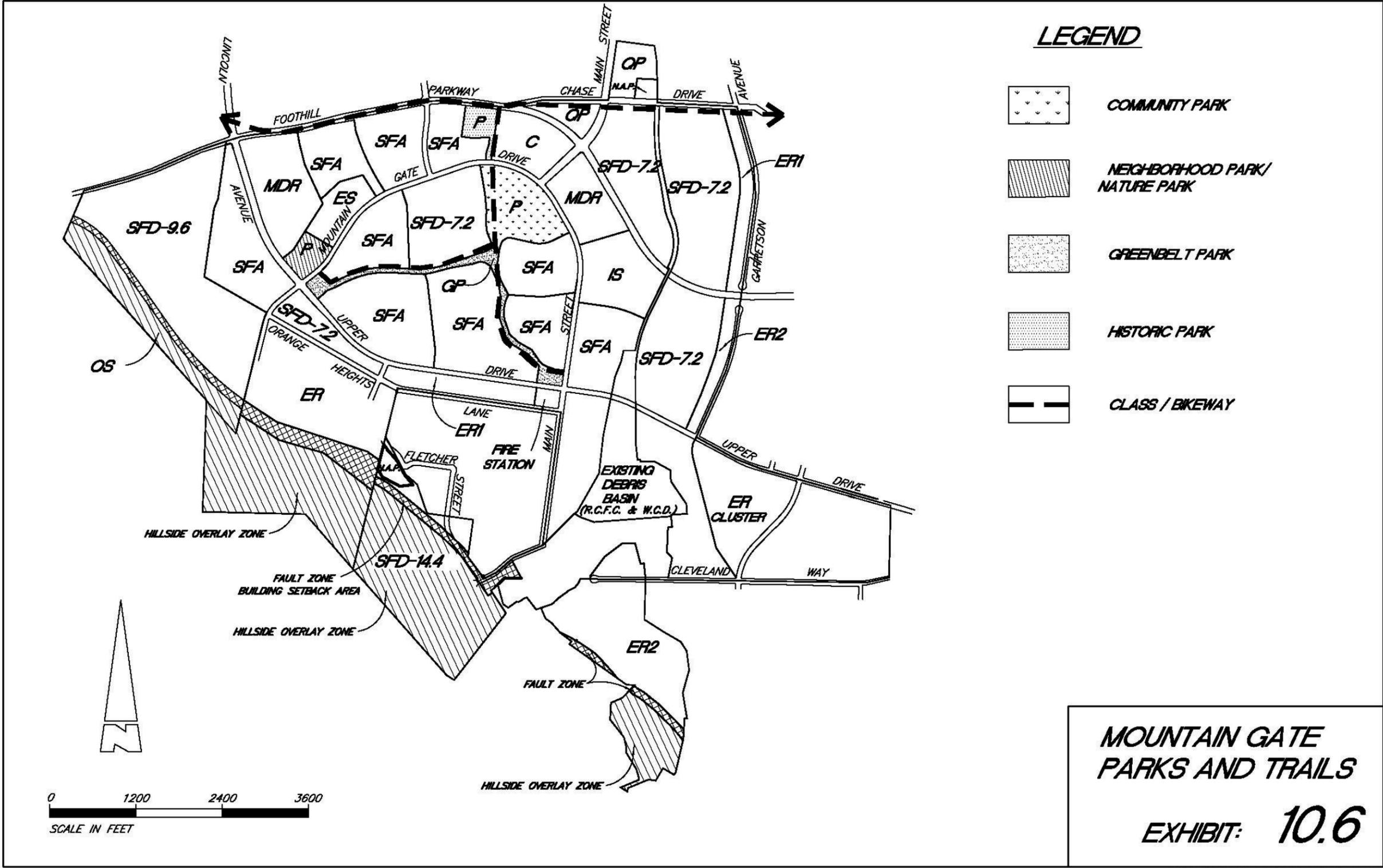
Exhibit 10.6A identifies the location of proposed off-street bicycle and pedestrian trails. Exhibit 10.6.B presents the Class I off-street bicycle path standard. Inter-village trail connections will be provided by an off-street trail located along the north edge of the property. This trail will follow the Chase Drive alignment from Garretson Avenue west to the westerly boundary of pl2nning area 9. From this point, the trail will follow the north side of the Foothill Parkway right-of-way to the MWD easement, where the trail will then run northwest along this easement.

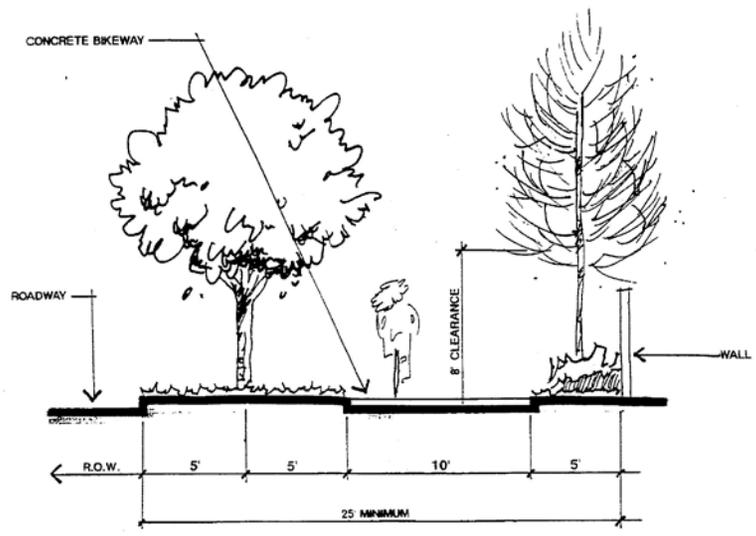


ELEVATION BLOCK WALL

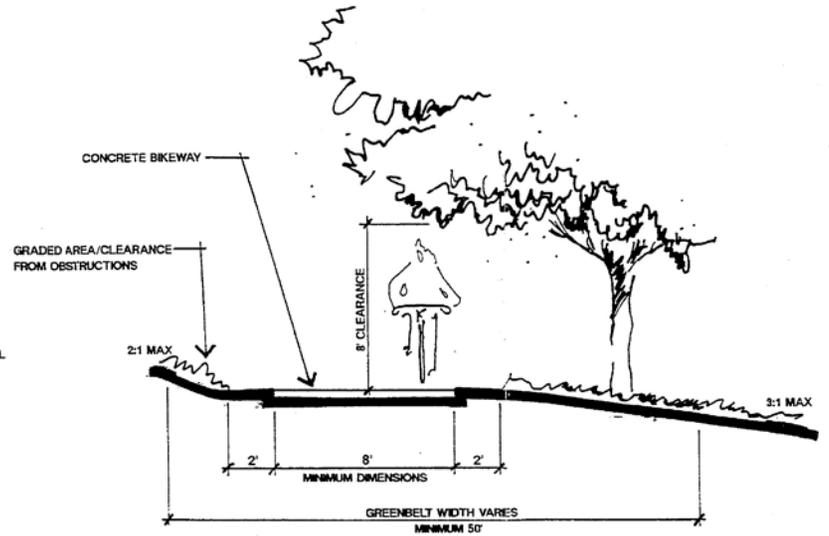
THEME WALL DESIGN
MOUNTAIN GATE
 FOOTHILL PROPERTIES

100-23





ADJACENT TO ROADWAY



THROUGH GREENBELT

- DESIGN SPEED -MINIMUM 20 MPH
- HORIZONTAL ALIGNMENT/SUPERELEVATIONS-PER CALTRANS DESIGN MANUAL
- STOPPING SIGHT DISTANCE-PER CALTRANS DESIGN MANUAL
- VERTICAL CURVES-PER CALTRANS DESIGN MANUAL
- 6' BIKEWAY ALONG ROADWAY IN CASES OF PHYSICAL LIMITATIONS SUCH AS A NARROW BRIDGE OR SEVERE TOPOGRAPHY

CLASS 1 BIKEWAY
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10.6A



Internal bicycle trail connections will be provided by a trail running along the length of the greenbelt park through the village core. This trail will link the residential neighborhoods in the village core with the community park, the neighborhood park, the greenbelt park recreation areas, and the proposed elementary school site.

The greenbelt park trail will be connected with the inter-village trail by a third trail extending south from Foothill Parkway, through the proposed historic park, then south along the westerly edge of the commercial center in planning area 10. At this point the connecting trail will cross the Mountain Gate Drive to join the off-street bicycle trail in the community park.

All of the arterial and collector streets in the Mountain Gate master plan are designed to include emergency parking lanes. As a general rule, on-street parking will be prohibited along these streets, with possible exceptions adjacent to the park sites and school site. With this design, the streets can accommodate exclusive on-street bicycle lanes in lieu of the emergency parking lanes. The City Council will determine whether on-street bicycle lanes will be included in the future.

10.5 Park and Greenbelt Improvement Program

The community park, the neighborhood park, the historic park and the greenbelt park are proposed as central features of the amenity-oriented development concept for Mountain Gate. Exhibit 10.6A highlights the locations of the proposed park sites together with the off-street bicycle and pedestrian trail network.

This specific plan proposes guidelines for the development of particular park and recreational improvements in each of the park sites. The elements of this park facilities program are specifically designed to respond to the facilities needs and priorities identified in the Corona parks and recreation user survey prepared as part of the new parks and recreation master plan.

A park development agreement shall be prepared and executed prior to recordation of Phase 1 of the master tentative tract map for the project. The agreement shall address the timing and degree of improvements to be provided in each of the park sites, the amount of Quimby Act credit to be received, and other items as agreed to by the City and the applicant.

The precise program of park improvements will be determined by the Parks and Recreation Commission and the City Council. The proposed park improvement program guidelines are as follows:

Community Park. 20 acres of land to include a community recreation center, a community pool, sports fields, court sports, play equipment, picnic areas, trails, off-street parking and restrooms.

Neighborhood Park. 5 acres of land developed as a joint use park/detention basin facility, to include sports fields, open play areas, trails, and picnic areas in the lower detention basin area of the site; and court sports, play equipment and restrooms on the higher part of the site above the detention area. The plan for the 5 acre joint use park/detention basin must be submitted to the Parks and Recreation Commission for their review and recommendation to City Council regarding park credit.

Greenbelt Park. Approximately 11 to 15 acres of land within the village core, to provide recreation areas linked by an off-street bicycle and pedestrian path that will join together the residential neighborhoods with the community and neighborhood parks and the elementary school. Enhanced landscaped areas to be provided at the intersections of Mountain Gate Drive with Lincoln Avenue and Upper Drive with Main Street. A series of recreation areas to be improved along the length of the greenbelt park with facilities including picnic areas, children's play equipment, and a volleyball court.

Historic Park. A proposed five-acre historic park site at the intersection of Foothill Parkway and Taylor Avenue. The historic park site anticipates the preservation and reuse of several of the existing structures for a museum, branch library or other civic purposes. Adjacent citrus groves could be preserved within the boundaries of this park.

A feasibility study shall be funded by the applicant regarding the Historic Park and commenced prior to recordation of Phase 1 of the Master Tentative Tract Map. The study shall include an examination of the potential facilities and services to be provided at the park, the costs of improving, staffing and maintaining the park facility, the timing for acquisition and improvements, and methods of funding the above noted items. In the event that the Council decided not to pursue the Historic Park, based on the findings in the study, then the Specific Plan land use for this planning area shall revert to the adjacent Single Family Attached (SPA) district.

Exhibits 10.8A through 10.8.H present typical sections along the greenbelt park to illustrate the character proposed for this open space and recreation corridor. Exhibit 10.7 shows the location of these sections. The sections illustrate the types of improvements proposed for the greenbelt park, as well as its relationship to adjacent residential neighborhoods.

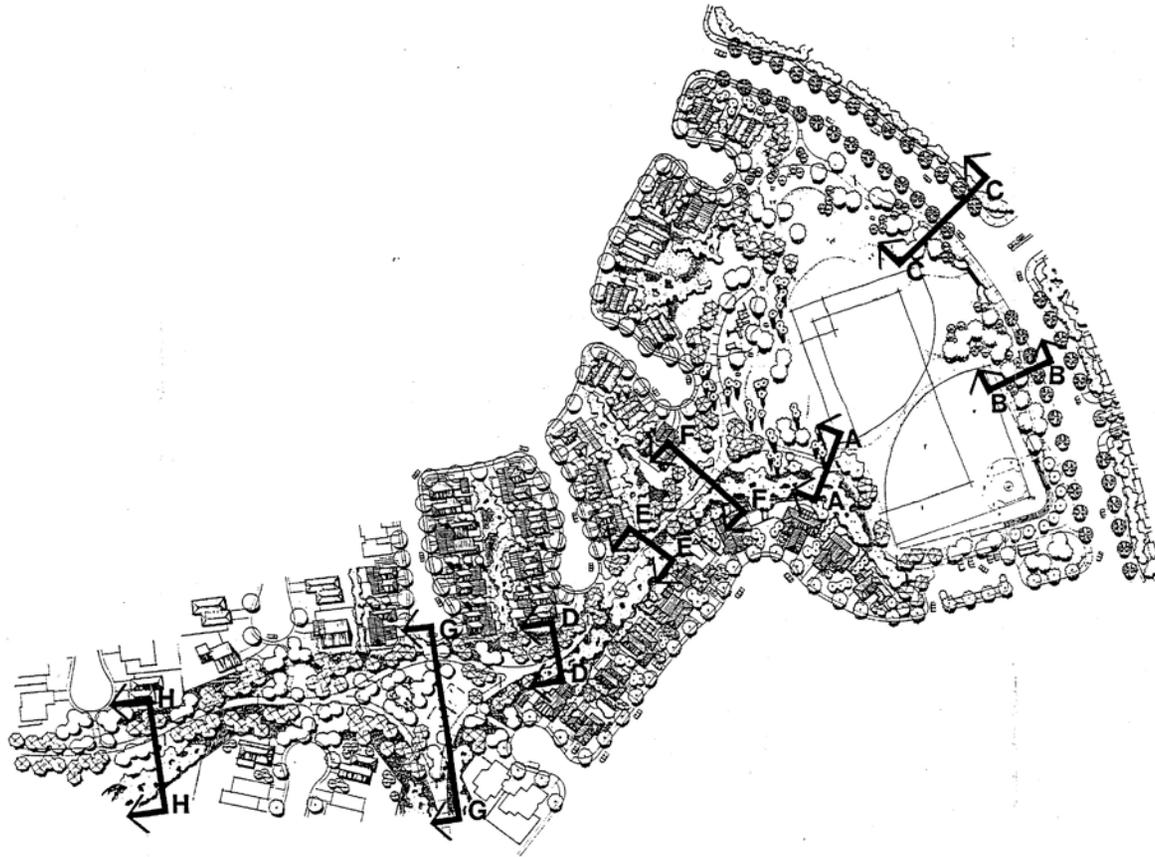
Greenbelt Design Guidelines

The design of the greenbelt park should consider the following design guidelines as provided by the Police Department:

1. Visual Access

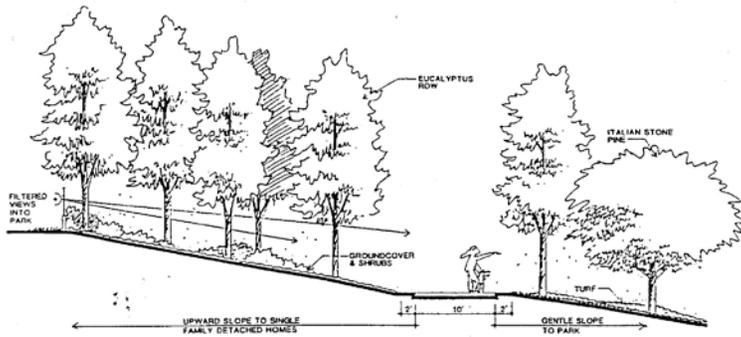
Visibility should be maximized along the length of the greenbelt by providing adequate width, line-of-sight, appropriate planting and fencing, and security lighting.

Six foot high, non-climbable wrought iron fencing should be used along the length of the greenbelt, particularly in slope conditions where visibility from both the residents' and users' perspectives can be enhanced. In at-grade conditions where privacy into the residences needs to be maintained, the alternatives will have to be proposed. Alternatives may include: a six foot high, non-climbable solid construction wall (or higher if allowed by the City as a further deterrent), a split wall with solid construction topped by wrought iron and security shrubbery, and a wrought-iron fence with security landscape screening. These at-grade areas should be kept to a minimum to further separate the private from public areas and avoid long stretches of solid walls. Regardless of the fencing material agreed upon, all pilasters should also be non-climbable, with no rock faces jutting out or crossbars to facilitate access.

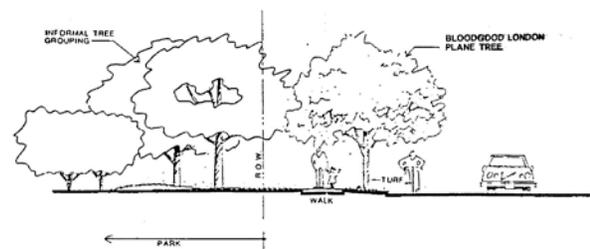


OPEN SPACE SECTION KEY MAP
MOUNTAIN GATE
LYON COMMUNITIES INC.

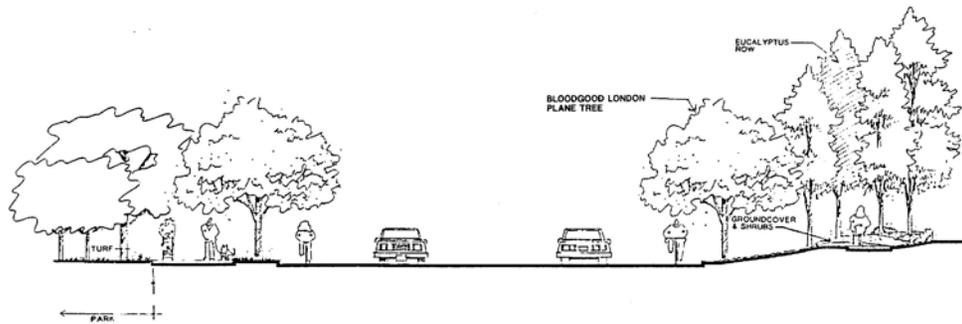
FOCO



SECTION A

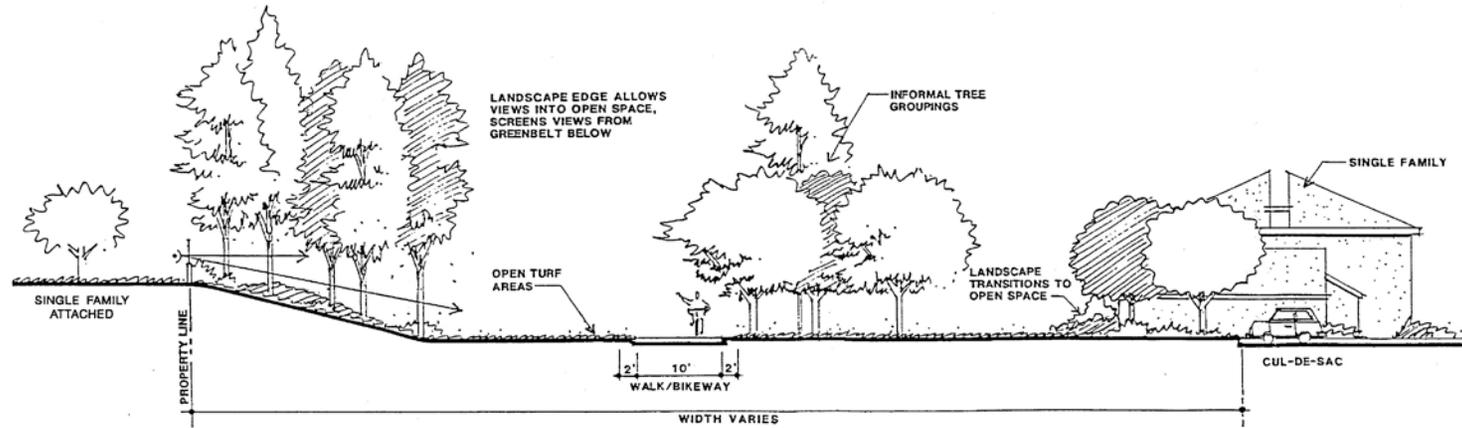
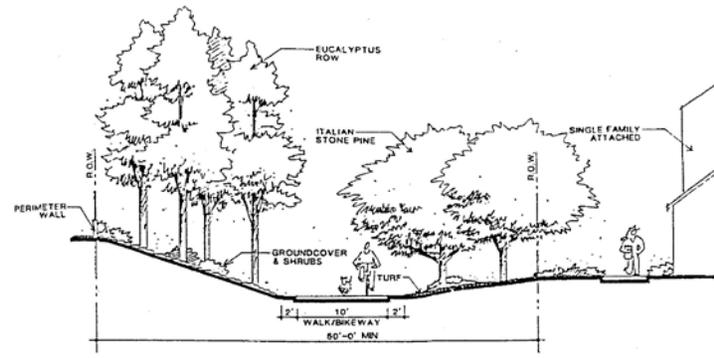
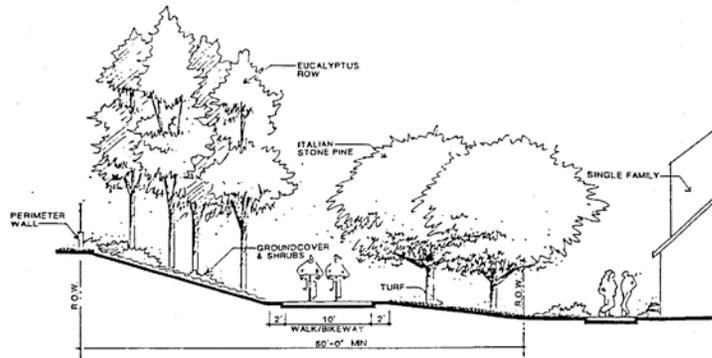


SECTION B



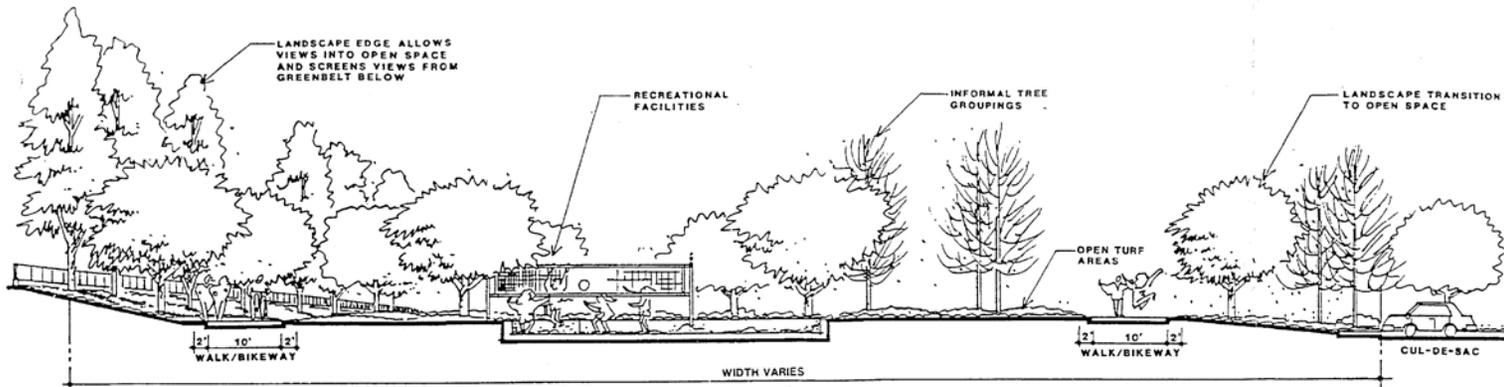
SECTION C

OPEN SPACE SECTIONS
MOUNTAIN GATE
 LYON COMMUNITIES INC.

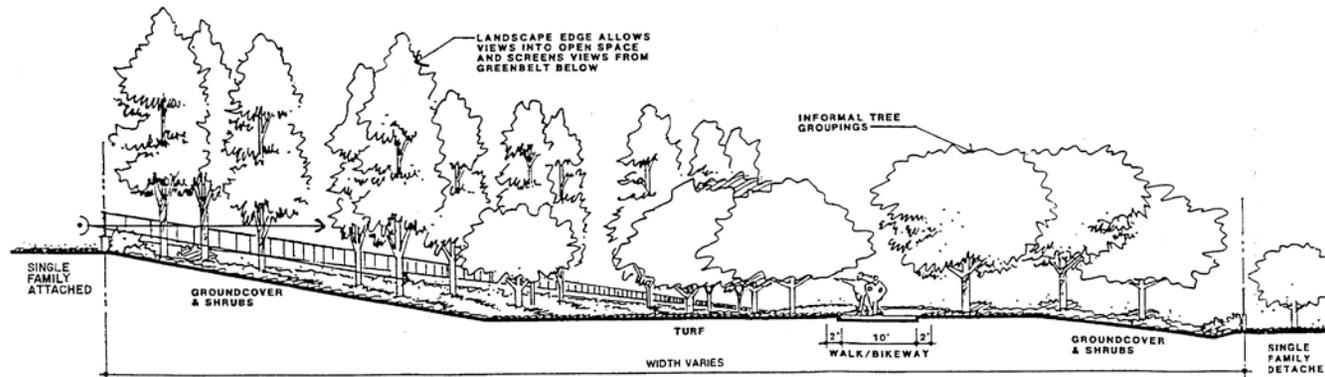


OPEN SPACE SECTIONS
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/17/18



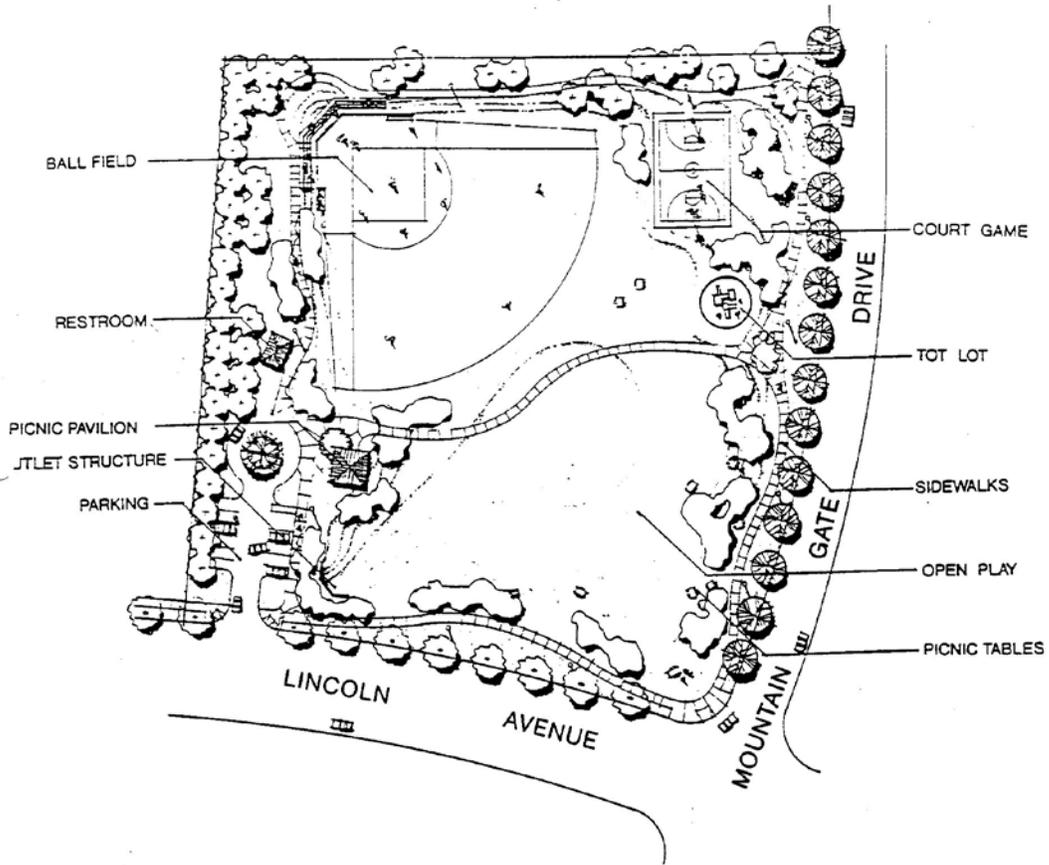
SECTION G



SECTION H

OPEN SPACE SECTIONS
MOUNTAIN GATE
 LYON COMMUNITIES INC.

10/10/05



CONCEPT PLAN FOR
 JOINT USE PARK
MOUNTAIN GATE
 LYON COMMUNITIES INC.



Provide direct line-of-sight as much as possible, both for the residents viewing the greenbelt and from adjacent streets. Views from adjacent streets and cul-de-sacs should provide maximum visibility in all directions seen along the greenbelt.

Parking should be prohibited along viewing areas on adjacent streets and ends of cul-de-sacs.

The visibility designed into the greenbelt should be maintained along its length by careful selection and location of plant material, location of structures and equipment, location of lighting, and maintenance of all the above.

2. Physical Access

Pedestrian access needs to be provided to make the greenbelt convenient for users; however, we recommend careful selection of pedestrian access points and consideration to limiting the number of access points. Our research and experience demonstrates significant problems in containing a perimeter at an incident in a greenbelt or linear park where there are unrestricted access points. Often, the subject of our pursuit/area search has more exit routes than we have officers on duty able to respond and contain a situation. This situation is only mitigated by providing main police vehicle access into the greenbelt and limiting the pedestrian access points between those main points.

Where vehicle access is to be provided we recommend:

Half-height curbing (4" high) along a six foot wide section of the curb at the street.

A strip of landscaping supported by grasscrete or turfblock behind the curbing, with the sidewalk behind the grass to create a symbolic barrier for private vehicles.

Provide an unobstructed transition curb radius from any street vehicle access to the greenbelt itself.

At main vehicle entry points, provide a six foot wide concrete drive ribbon with six feet of grasscrete or turfblock for a total driving width of 12 feet. We recommend the main vehicle entry points be meandered, with light standards and landscaping staggered to give the illusion it is not to be driven on. This can be accomplished while still maintaining good visibility from the street. As the drive ribbon extends into the entry and/or greenbelt itself it should be widened to an 8' concrete strip with 4' of grasscrete or turfblock to maintain the total 12' width and minimize maintenance to the landscape area.

We recommend grasscrete over turfblock as its one-piece construction is more durable over time. Wherever the sub-support is installed for driving, a delineating curb should parallel the edge to identify the area to be driven upon.

There should be no gates which open from the private residences onto the greenbelt.

3. Open Space Design

We strongly encourage promoting the use of the open space, particularly by adults and families. Our research and experience demonstrate higher demand for police services in parks which are

abandoned over time by these groups and used primarily as gathering points for juveniles and truants. This positive use can be enhanced by maintaining visibility and access to the greenbelt, which tends to create a greater sense of security for users.

The activity nodes proposed along the greenbelt should only be located in highly visible and accessible areas, both to residents along the greenbelt and from the street. Locating the nodes in these areas will help reduce the opportunity for vandalism and other crimes to occur.

We recognize the need to provide some form of communication along the length of the greenbelt, but recommend that public telephones rather than police call boxes be installed. They should be installed only at the highly visible activity nodes, however, to reduce the possibility of prank calls on the emergency 9-1-1 lines.

We agree with the applicant that all development along the greenbelt be encouraged to relate to its design in terms of enhancing visibility, surveillance, and use.

4. Landscaping

Provide plant arrangements and types which allow for easy security maintenance.

Use trees which provide views underneath and are of less dense foliage.

Avoid clumping trees and locate trees away from fencing and lighting to maximize visibility.

Plant security shrubs and clinging vines at walls to discourage climbing and graffiti or other vandalism.

Plant symbolic barriers at the greenbelt edges to delineate the public from private-use areas. These symbolic barriers typically encompass three phases: (1) The walkway and adjacent grass area, (2) the parallel ground cover and dense hedgerows, and (3) the fencing and plant material at fence lines.

Security shrubbery typically used would be varieties of natal plum and other dense thorny shrubs and bougainvillea or other clinging vines.

Provide for proper maintenance of the landscape to protect its design concepts for visibility and security.

Design and install the irrigation and sprinkler systems for the greenbelt to avoid conflicts with the driving area and increased repair costs due to broken sprinkler heads and other damage.

5. Lighting

Provide at least .25 foot candles of illumination / square foot, minimum maintained along the entire length of the greenbelt.

Eliminate areas of transitional lighting by spacing the light standards to provide constant coverage.

Utilize directional light fixtures and luminaries to avoid directing greenbelt lighting into adjacent residential areas.

Install light standards which are a minimum of 15' in height, with vandal-resistant covers.

Locate the light standards away from fencing structures, and benches which would provide access to them. Also carefully locate the lighting and landscaping to eliminate the possibility of growing plants and trees interfering with the illumination and coverage.

Utilize photo-electric cells or other automatic device to control the lighting during all hours of darkness.

10.6 Special Landscape Standards

1. A special landscape buffer area shall be established at the west end of planning area 9. This special landscape area shall be designed as a buffer to separate the existing home located north of the west end of planning area 9 from the commercial development proposed for this planning area.
2. A fuel modification plan shall be prepared and implemented for new development in planning areas 5, 26,33, and 34 which are located adjacent to wild land fire hazard areas, as defined by the Corona Fire Department. Said fuel modification plan shall include clearing or thinning or brush and the installation of irrigated landscaping within 100 feet of any structure in areas abutting native brush land. This fuel modification plan shall be submitted to the Fire Department for approval.
3. The permanent drainage detention basins to be constructed in the westerly neighborhood park site shall be designed and landscaped in accordance with standards approved by the Parks and Recreation Department. Maintenance responsibilities for the joint use detention basin/park site shall be allocated between the City of Corona and the Riverside County Flood Control and Water Conservation District according to an agreement to be developed between the two agencies.
4. Additional temporary detention basins which may be constructed to attenuate peak flows into the storm drain systems tributary to the Oak Street channel and to Taylor Avenue shall be fenced, and perimeter irrigated landscaping shall be installed to screen said basins from adjacent public streets. Plans for said fencing and perimeter landscaping shall be submitted to the City Planning and Public Works Departments for approval. Flood control improvements in the detention basin shall be maintained by the City of Corona. Perimeter fencing and landscaping shall be maintained through either a landscape maintenance district (LMD) or community facilities district (CFD). Low flow basins are to be maintained by a Homeowners' Association.
5. Landscape lots or landscaped slope easements adjacent to public street rights-of-way are encouraged to be graded to drain to the public street. In the event that said lots or easements are not graded in this manner, alternative drainage provisions acceptable to the City Engineer shall be provided. Except as otherwise provided in the City's landscape maintenance district guidelines, the maximum slope gradient permitted in said landscape lots shall be 2:1 in areas

planted in trees, shrubs and groundcover, and 5:1 in areas planted in turf. As an alternative, retaining walls may be incorporated into the landscape lot outside of the standard public right-of-way. Said retaining walls may not exceed a height of four feet and shall be of a design consistent with the project theme wall design.

6. Landscape lots or landscaped slope easements adjacent to public street rights-of-way shall be dedicated to the City, and a landscape maintenance district or community facilities district shall be established for the maintenance of said landscaped areas. The landscape maintenance district or community facilities district shall also be responsible for maintaining the structure and the exterior face of project theme walls identified in this specific plan.
7. All landscaping in parkways, landscape lots and landscape easements dedicated to the city shall be irrigated, and separate controllers, water meters and electrical meters shall be installed for said dedicated landscaped areas.
8. Existing significant stands of eucalyptus trees shall be evaluated for possible preservation during future detailed design of individual development projects. It is recognized that many eucalyptus trees cannot be preserved because of tree disease, road and utility improvement requirements, and aesthetic considerations.
9. Existing stands of mature palm trees shall either be preserved or be relocated for incorporation into the planned streetscapes and project entry landscaping.
10. Landscaping of the arterial and collector streets identified in this specific plan shall be done according to the following schedule. Landscape improvement plans for the entire parkway and for any adjacent landscape lot shall be prepared concurrently with the improvement plans for the adjacent street. Landscape improvements including the sidewalk, main and secondary entry monuments, and the parkway between the sidewalk and the curb shall be constructed concurrent with the construction of the adjacent arterial or collector street. Construction of landscape improvements behind the sidewalk, the project theme wall, and subdivision entry monuments may be deferred until the construction of the adjacent development.
11. All CFP designed Landscape Maintenance Districts or Community Facilities Districts must be submitted to, and approved by, the Park Planning Division.
12. All Landscape Maintenance Districts or Community Facilities Districts must be designed and installed in accordance with the LMD or CFD Design Guidelines.
13. The Fuel Modification Plan, if intended to be maintained by the Landscape Maintenance District or Community Facilities District, must be approved by the Parks Department.
14. The proposed greenbelt shall be included in the proposed Landscape Maintenance District or Community Facilities District.

10.7 ARCHITECTURAL DESIGN GUIDELINES

The purpose of these architectural design guidelines is to achieve a consistent high quality of design within the residential neighborhoods and the other areas within the Mountain Gate planned community. These guidelines are not intended to serve as absolute rules for design, but rather they are intended to encourage the building designer's creativity in a diversity of architectural styles.

10.7.1 General Requirements

Prior to the submittal to the City of Corona, building construction plans for plan check of single family projects within the ER, SFD and SFA Districts excluding single family detached condominiums, the builder of any building shall submit the following plans to the Community Development Director for advisory review.

- Preliminary floor plans.
- Preliminary elevations for each side of the building.
- Building footprints.
- Material and color samples for roofs and exterior walls.

The architectural design of the buildings submitted shall be reviewed by the Community Development Director according to these Mountain Gate architectural guidelines.

The architecture for development projects within the medium density residential district, single family condominiums, any product other than SFD in the estate residential cluster district, and commercial and quasi- public districts shall be submitted to the Design Review Board if required by City ordinance, or for Precise Plan review by the Planning Commission if the Design Review Board is not established.

Residential developments of less than five dwelling units and custom lot residential developments are exempt from the requirements established by these architectural design guidelines.

10.7.2 Commercial, Quasi-Public, and Public Architectural Design Guidelines

The architectural design of any building within the commercial and quasi-public land use districts and the design of any public building within any land use district, shall conform with the applicable architectural design standards contained within the South Corona Community Facilities Plan.

10.7.3 Residential Architectural Design Guidelines

These design guidelines include three distinctive architectural styles which provide variety to the overall development, and maintain integrity from one neighborhood to the others. The three architectural styles are:

- Ranch
- Mediterranean
- Traditional

Designers of residential buildings within the Mountain Gate planned community are encouraged to develop a design consistent with one or more of these styles. Other architectural styles may be permitted upon the designer's demonstration to the Community Development Director or to the Planning Commission/Design Review Board as required above that a high quality of design will be achieved. Such review shall be advisory.

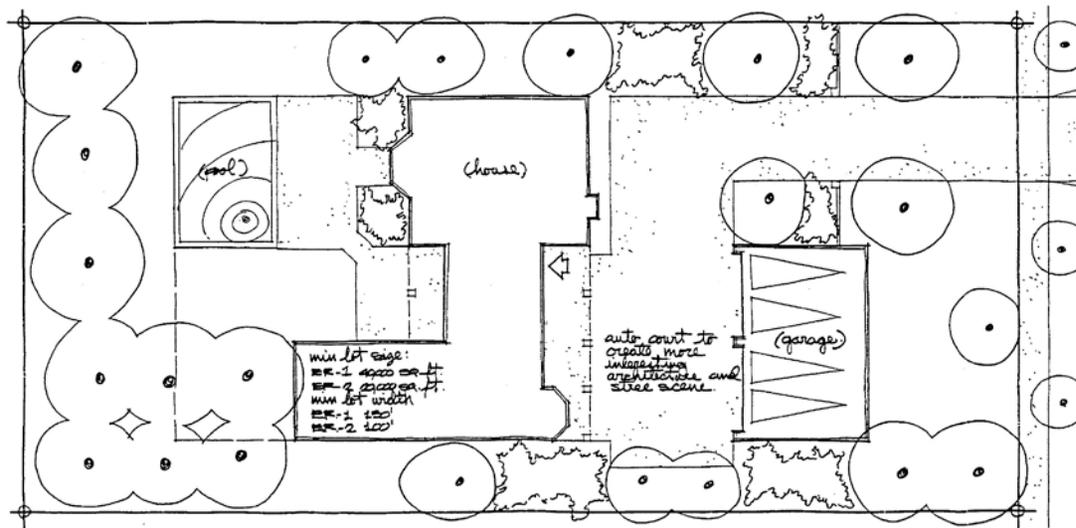
These residential design guidelines are presented in two parts. The first part of the residential design guidelines describes and illustrates the different materials, textures, colors and elements which are characteristic of each of the three architectural styles. Within each of the styles, architectural elements are defined as appropriate, discretionary or inappropriate for the particular style.

The second part presents a typical plot plan for each of the major residential density categories (estate, single family detached, duplex and town home). Each typical plot plan is accompanied by a street scene elevation which illustrates one of the three architectural styles identified above. These elevations are intended to show some of the variations possible when using different densities.



STREET SCENE ELEVATIONS

RANCH STYLE



these drawings are for illustrative purposes only, plot plan must conform with the setback and all other development standards in chapter 9 of this specific plan.

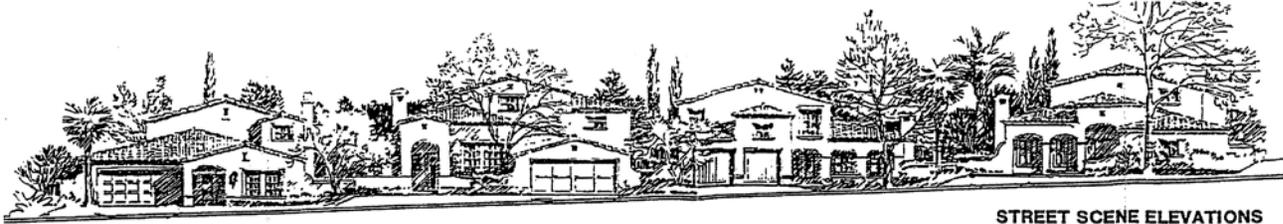
E.R. ESTATES

SITE PLAN

10126

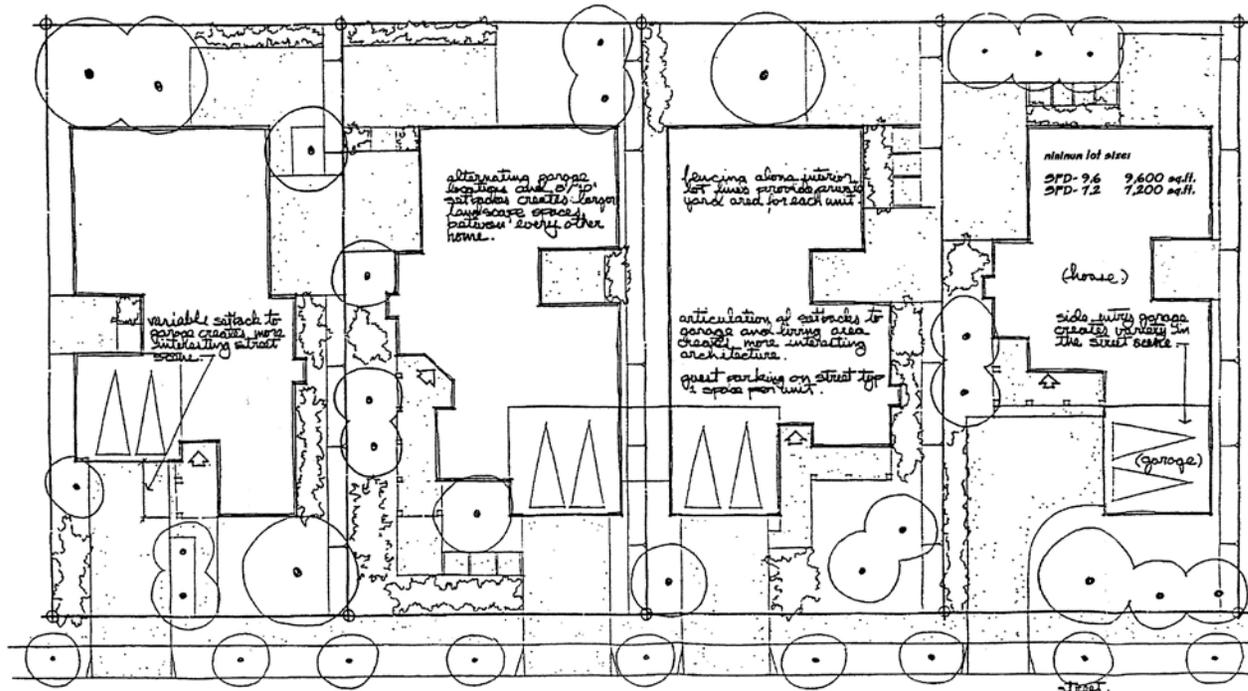
EXHIBIT 10.10A

CPRDA
 COLUMBIAN PUBLIC RIGHTS DEVELOPMENT ASSOCIATION
 Architects Planners



STREET SCENE ELEVATIONS

MEDITERRANEAN STYLE



S.F.D RESIDENTIAL

SITE PLAN

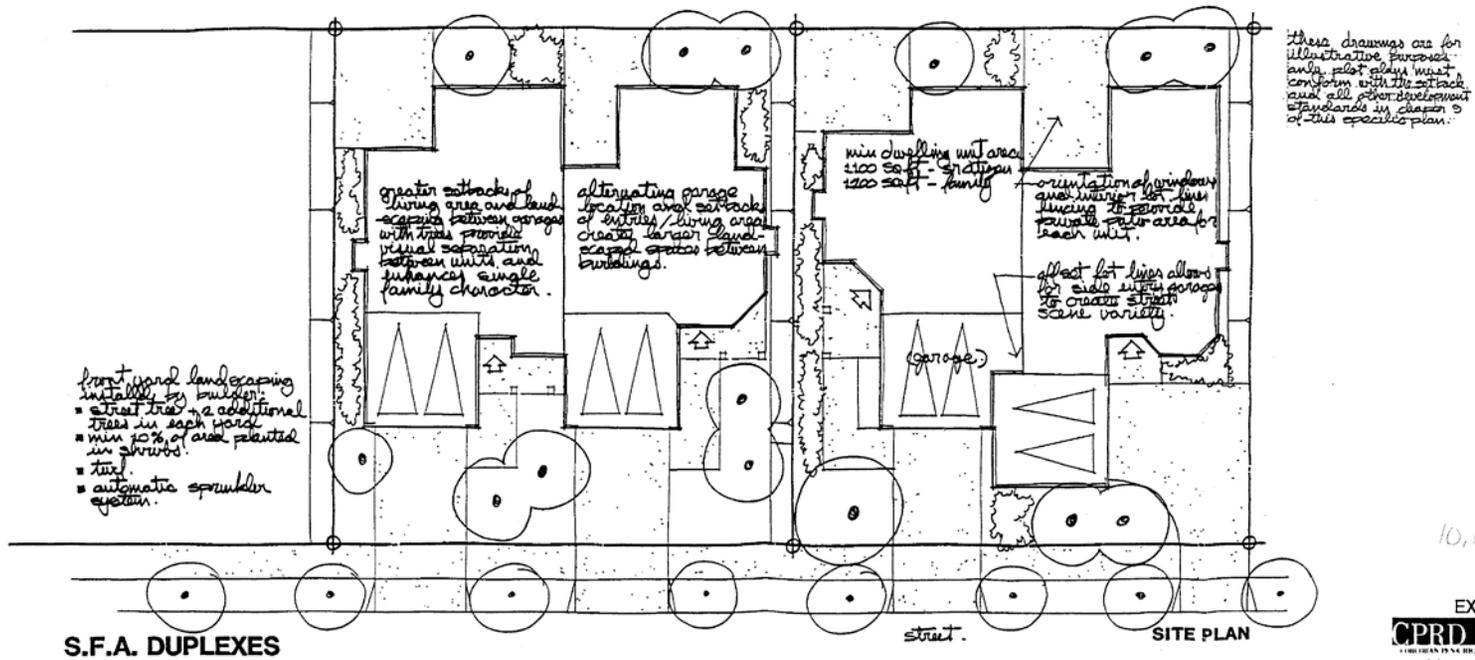
These drawings are for illustrative purposes only. All plans must conform with the setback and side front development regulations in Chapter 9 of this specific plan.

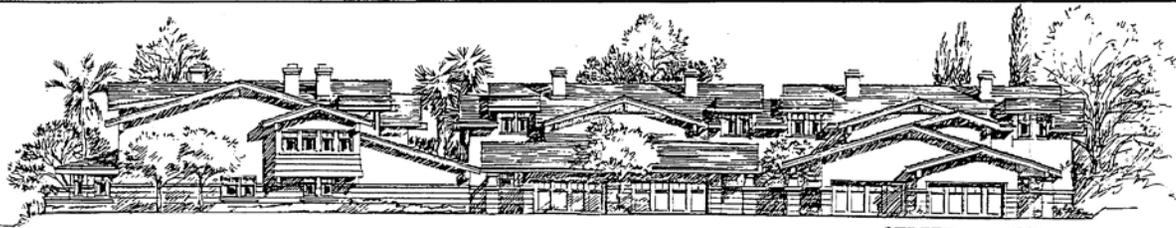
EDAW
EXHIBIT 10.10B



STREET SCENE ELEVATIONS

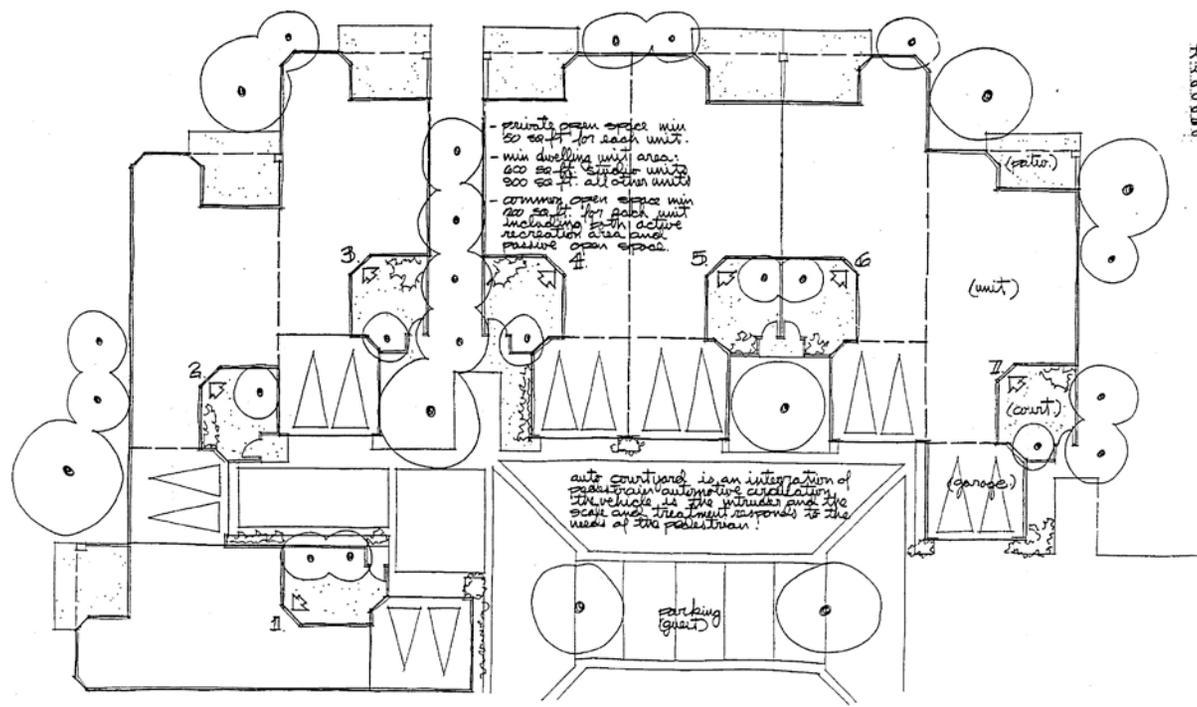
TRADITIONAL STYLE – VICTORIAN





STREET SCENE ELEVATIONS

TRADITIONAL STYLE – CRAFTSMAN



these drawings are for illustrative purposes only. plot plan must conform with the setback and all other development standards in chapter 9 of this specific plan.

M.D.R. TOWNHOMES

SITE PLAN

EXHIBIT 10.10D



11.0 IMPLEMENTATION PLAN

11.1 Precise Plans

A precise plan shall be prepared, submitted and approved for all single-family detached condominiums in the “SFA” Single-Family Attached Residential district, and for all town home and multi-family development in any district, and for all development within the “C Commercial and the “QP” Quasi-Public districts. The precise plan shall conform to the General Plan and Specific Plan in terms of general location, number of dwelling units proposed, density and general standards of development.

The precise plan application shall be consistent with the City Planning Director’s current application submittal and review requirements. Prior to filing the precise plan application, the developer shall submit plans for Development Plan Review following the provisions of CMC Section 17.102 and Section 10.7.1 of this Specific Plan.

The precise plan project shall be developed subject to all conditions imposed as part of the approval of the precise plan. The City Planning Director may approve or conditionally approve adjustments which in his/her opinion are minor in nature and are in substantial compliance with the conditions of approval.

In the event that Design Review supersedes the precise plan requirements, by City ordinance, these projects which are hereby made subject to precise plan approval shall then be subject to Design Review instead.

11.2 Density Transfers

A transfer of residential dwelling units may be made among planning areas under the following conditions:

1. The total number of dwelling units permitted within the Mountain Gate Specific Plan shall not be exceeded.
2. The maximum transfer shall not increase the dwelling unit range high in a planning area (see Table 5.2, Allocation of Dwelling Units among Planning Areas) by more than ten (10) percent of said range high; nor shall the transfer exceed the density range established by the Community Facilities Plan.
3. Property development standards for the residential category shall be in compliance with the Mountain Gate Specific Plan.
4. A proposed density transfer shall be made in writing to the Planning Director for review and approval, together with a revised table outlining the proposed transfer.

11.3 Specific Plan Amendments

During the course of project development and construction, Specific Plan Amendments may become necessary. It should be understood that while this document attempts to be comprehensive, not all things can be envisioned in its preparation. Therefore, it should be understood that Specific Plan Amendments are anticipated and are useful to keep the plan current with changes in the housing and construction market. Applications for Specific Plan Amendment shall be made to the Planning Director based upon the current application requirements. Applications shall be considered by the Planning Commission and City Council. The following criteria shall be considered for amendments to the Specific Plan:

- Consistency with the City of Corona General Plan and South Corona Community Facilities Plan.
- Consistency with the intent of the Mountain Gate Specific Plan.
- Consistency with the village concept is achieved.
- Adequate circulation to and from and within the area is maintained.
- Public service levels capable of adequately serving the Specific Plan area are provided.
- The public and private open space system is neither disrupted nor depleted.

11.4 Variances

Variance applications to the standards contained herein shall be permitted in compliance with CMC Sections 17.96 for major variances and 17.98.020.B for minor variances.

11.5 Home Buyer Notification

The application shall prepare a home buyer notification package which contains a description and accompanying maps showing the adopted land use plans for the Mountain Gate and South Corona CFP. Each homeowner will be required to affirm, by signature, that they are aware of the future development in South Corona. The contents of the notification package shall be reviewed and approved by the Community Development Director prior to recordation of the Phase 1 final map.

11.6 Fee Ordinance

All development within the Mountain Gate Specific Plan shall be subject to the South Corona Funding Program and Master Plan Fee Ordinance.

12.0 SUBDIVISION STANDARDS

The provisions of this section shall be the standards for subdivisions for the Foothill Ranch Standards which are not addressed in this section shall be governed by Title 16 (Subdivision Ordinance) of the Corona Municipal Code.

12.1 Subdivision Standards

12.1.1 Local Streets

- A. Local through streets shall be constructed in accordance with City of Corona standards, except that a minimum street centerline radius of 300 feet may be approved by the City Engineer.
- B. Local cul-de-sac streets shall be constructed in accordance with City of Corona standards, except as specifically amended below:
 - 1. 36 foot curb to curb roadway width in a 60 foot right-of-way.
 - 2. A minimum street centerline radius of 300 feet may be approved by the City Engineer.
 - 3. A maximum cul-de-sac length (measured from the centerline intersection of the through street to the center of the cul-de-sac bulb) of over 550 feet will be allowed, as long as the following criteria are satisfied:
 - No more than 20 residences may take access from the cul-de-sac; and
 - The water main in the cul-de-sac will be looped to provide two sources for water.
- C. All lots fronting on a cul-de-sac, knuckle or curved street shall have a minimum 32 foot distance at the curb line between property line prolongations except that for attached housing or condominiums, the minimum distance shall be 30 feet, subject to the demonstration of adequate on-street parking for guests.

12.1.2 Lots and Blocks

Residential blocks shall be two lots deep in width, excepting there from one lot deep blocks adjacent to collector, secondary and major arterial streets.

12.2 Modifications

Modifications to these standards may be approved pursuant to Chapter 16.32 of the Corona Municipal Code. Modification requests shall be made in writing and shall contain a statement of justification for the modification based upon submitted background data and information. The Planning Commission and City Council shall make findings consistent with Section 16.32 of the Corona Municipal Code.